



Star Hill to Sun Pier

Supplementary Planning Document

2024

About this document

Medway Council is committed to supporting Intra to become a great place in which to live, work and visit by ensuring that its existing historic fabric is retained and restored whilst new development is contextual, place-specific and successfully integrated into the historic environment.

To guide development within Intra, the Council has produced a Supplementary Planning Document (SPD) which sets out the type, form, scale, density and character of development expected within the framework area which celebrates its layered history.

For the purposes of this Supplementary Planning Document, Intra is defined as the area within the Star Hill to Sun Pier Conservation Area covering a section of Chatham High Street and its hinterland on the south bank of the River Medway, east of the town centre. Development of Intra is an important element in securing the Council's vision for the wider city.

Public consultation, undertaken between 2019 and 2023, has helped to shape the SPD prior to consideration and adoption.

If you would like any further information on the SPD please contact the Council's Planning Policy Team at:



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Chapter Overview 1.0

1.1 The Supplementary Planning Document (SPD)

1.1.1 Introduction

Research by Historic England demonstrates a positive correlation between the number and nature of heritage assets in places and their image and appeal as destinations. Many places that are popular as day visit destinations have mainstream retail and leisure activity in historic environments. People are willing to travel further to visit areas with distinct character and history, and it has been found that:

“ areas with large concentrations of heritage assets are often predominantly occupied by independent businesses. These may be very different in nature: London’s Clerkenwell, for example, has an extraordinary concentration of architects, Nottingham’s Lace Market has a concentration of professionals, Cheltenham’s Montpellier is full of shops that specialise in interiors. They are, however, alike in that they have economic activity that is different from other parts of the town/city they are located in and is dominated by occupiers that are not commonly seen elsewhere.

Impact of Historic Regeneration, English Heritage 2010

A historic environment also provides the opportunity for differentiation from more generic places, offering a more varied, independent and distinctive character. Intra is one such location with great potential, representing a fascinating succession of development steeped in cultural interest with a high concentration of historic buildings, in a riverside location.

Whilst much of the area has survived, many assets are in poor condition and are at risk of being damaged or lost. Intra needs significant investment to prevent the loss of its heritage and the erosion of its intrinsic historical and cultural value. This investment must retain and reinforce the qualities that differentiate Intra from other high streets around the country, and support its businesses and creative industries.

This significance-led framework recognises the increasing pressure that new development can place upon the existing built fabric and the consequences on heritage value and character.

The framework provides guidance which supplements the Council’s existing and emerging planning policies to achieve a well-designed contextual place, which balances the legacy of the past with the needs of a modern society. It should be read in conjunction with the ‘Star Hill to Sun Pier Conservation Area Appraisal and Management Plan’.

This framework is ‘significance-led’, referring to heritage significance which has been investigated in detail and used to shape the guidance and content contained in this document to be specific to the historic environment in Intra. The guidance contained within the SPD is intended to ensure that the value of Intra’s heritage assets are reflected in the design, layout and appearance of new development which comes forward.

1.1.2 Status

An SPD is a document which adds further site-specific detail to the policies within the Local Plan, explaining how planning policies and requirements can be complied with to help developers make successful planning applications, add certainty and assist the Local Planning Authority to make clear and consistent decisions.

The Local Plan should be the first port of call for guidance and policies relevant to development in Intra. This SPD will build upon these and in particular how development proposals should comply with Strategic Policies S1, S2, and S4 of the Medway Local Plan, which identify the need for high quality development and design standards in redeveloping parts of Medway to create a new ‘city’ centre, which includes Intra, to improve the use of underutilised land and the surrounding environment.

Strategic Policy S1 refers to the development strategy for Medway being to prioritise re-investment in the urban fabric. It also sets out that long term protection will be afforded to the historic built environment, recognising its quality and character. Strategic Policy S2 sets out how this development strategy will be implemented

Strategic Policy S4 refers to delivering a high quality built environment which responds to its context and reflects its distinct local character. Particular emphasis is placed on promoting Medways’ built heritage making reference to the wealth of historic buildings and conservation areas including Star Hill/Sun Pier.

Paragraph 8.5.5.1 goes on to further explain that the Star Hill/Sun Pier area is an important centre in townscape terms which contains a mix of retail uses with other commercial and residential uses. The ambition of these policies and associated guidance will also be reflected in the new Medway Local Plan which will direct future growth and development in Medway.

The strategic aspirations and aims of the new plan relate to sustainable development and providing the land needed for new homes, jobs, infrastructure and services whilst protecting and enhancing the environment and heritage. It will also plan to reduce health, social and economic inequality and manage change to benefit the people of Medway.

Similar to Strategic Policy S1, the new Local Plan will emphasise how renewal and redevelopment is a foundation for growth and how this should be heritage led. This document therefore speaks to the aspirations and objectives of both the adopted or emerging Local Plans and seeks to ensure that development helps the Council, landowners and developers achieve these shared goals for Intra.

Given that the SPD site covers a Conservation Area (Star Hill to Sun Pier), Local Plan policy BNE12 is particularly relevant. It requires special attention to be paid to the preservation and enhancement of the character and appearance of Conservation Areas.

1.2

Benefits of the SPD

1.2.1 Engagement with Stakeholders

Purpose of the document

This Significance-led Development Framework is part of the government-funded High Streets Heritage Action Zone programme (HSHAZ), which is being delivered by Historic England, to unlock the potential of high streets across England, fuelling economic, social and cultural recovery. It supports the policies within Medway's Development Plan by putting high-quality, inclusive design and sustainability at the forefront of a heritage-led revitalisation of Intra.

Intra is strategically located between Chatham and Rochester and has excellent links with the wider area and the South-East. It is seeing population growth, which is leading to greater demand for more and better-quality places to live, work, and enjoy.

Whilst development has continued to come forward, there needs to be a vision for Intra and guidance for both the public and private sector on how it will be achieved. Conserving the local heritage and giving it a role in placemaking is key to ensuring continuation of Intra's historic legacy.

This SPD is therefore intended for landowners, developers, house-builders, self-builders, local communities, politicians, planners, architects, designers and the full range of built environment professionals. It explains the Council's aspirations for placemaking and housing design and sets out how the planning and design processes should work together to achieve this aim.

Development of the SPD

The SPD has developed through an iterative process of consultation and engagement with a range of stakeholders. Information was gathered on their views of the area as a whole, any specific constraints or challenges as well as aspects they like and value about Intra, and their aspirations for future development.

Input was also sought from experts and Council officers across a wide variety of fields, and from specialist consultants and built environment professionals. Technical information was gained on the constraints of the site, such as flood risk, and how these could be planned around as a part of new development which would unify this area between Rochester and Chatham.

The process also involved engagement with the public and community groups. This took place online and through in-person presentations of the SPD proposals to key groups including the Cultural Consortium and Intra PACT. The findings of this consultation were used to help guide the production of this SPD.

These stakeholders have included:

- *Landowners*
- *All relevant departments within Medway Council, covering design, housing, drainage, transport and highways, economic development, heritage and conservation, planning and policy, among others*
- *Historic England*
- *The Environment Agency*
- *Peel Ports*
- *Marine Management Organisation (MMO)*
- *Ward Councillors/Planning Committee Members*
- *The Cultural Consortium and other community groups;*
- *Local residents and businesses*
- *University of Kent School of Architecture & Planning*

1.3 Applying the Guidance

1.3.1 Interpreting the SPD

The pages within the SPD are intended to be used follows:

01 Numbering

Each section is numbered for easy reference in reports

02 Alignment with National Design Guide

This shows how each piece of guidance relates back to the National Design Guide.

03 Vision Principles

Each section also relates back to a key objective as part of the Vision which is set out in Chapter 3. The Vision Principles capture the themes of both the adopted and emerging Local Plan strategic policies.

04 Design Principles

Key guidance is provided in coloured boxes under each objective, explaining the steps needed to achieve the objective. This guidance sets out benchmarks for planning applications to follow. Where it is not strictly followed, applications will need to robustly justify an alternative approach to achieving a similar standard or quality development.

05 Supporting Detail

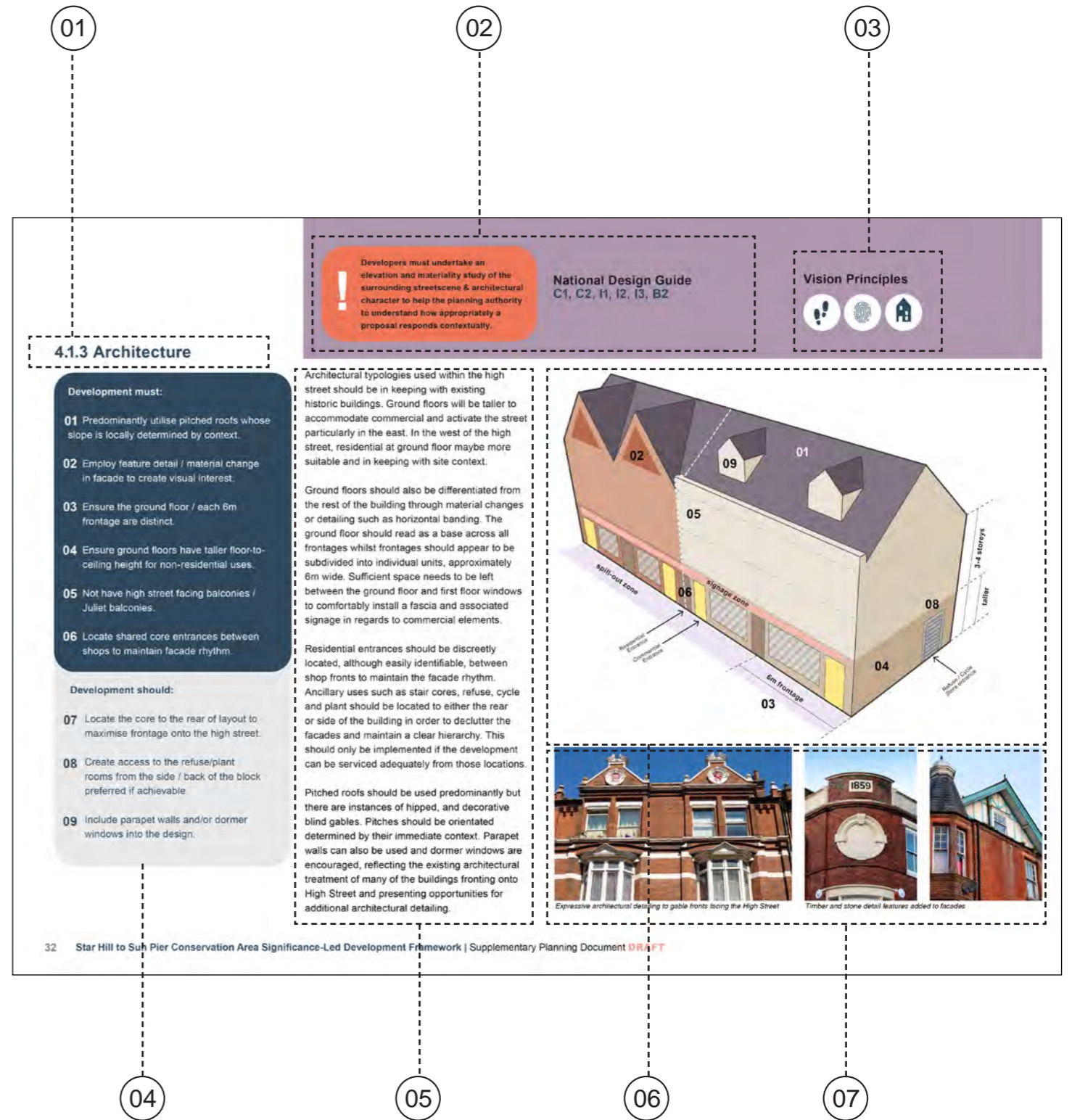
Additional detail is provided which expands on the objectives and guidance, explaining the reasons for the objective and providing detailed information on how to achieve an acceptable development proposal. This detail is not mandatory, but sets out how the objectives and design guidance can be complied with.

06 Illustrative Diagrams

In many sections of the SPD, illustrations and diagrams are provided to show how the development should be designed, alongside a key where appropriate. These relate to the objectives and guidance on the same page and should be complied with.

07 Precedents and Examples

In some cases the SPD highlights examples which achieve the objectives and guidance set out, to assist in demonstrating how they can be successfully achieved. These examples are accompanied by descriptions where appropriate to explain why they are good examples to be followed, or bad examples to be avoided, and are intended to be “real world examples” to explain to designers the outcomes following of the guidance.





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Chapter
Local Context 2.0

2.1 Site Context

2.1.1 Wider Context

Intra sits between the town centres of Rochester to the west and Chatham to the east. It forms part of an almost continuous high street that runs from Strood, across the River Medway through Rochester, Intra and onto Chatham.

Located on the outside bend of a meander in the river, Intra is distinct from these other high streets because of its connection to the River Medway and its rich industrial hinterland of lanes, warehouses and wharves adjacent to the water.

The site benefits from being situated between two train stations on the main line to London and Kent (Rochester and Chatham) and two town centres, all of which are within easy walking distance from the site.

With connections to both green and blue spaces, Intra makes for an attractive offer and several large schemes are in development today.



- A, represents Strood High Street.
- B represents Rochester High Street.
- C represents Intra High Street.
- D represents Chatham High Street.
- A thick pink line represents Corporation Street.
- A light orange line represents New Road.
- A thick orange line represents primary connector.
- A thin orange line represents major connections.
- A green line represents river walk.
- A horizontal line with vertical lines going through it represents railway.
- Grey shading represents town centres.
- Diagonal shading represents areas with masterplan proposals.



Strategic regional connections

- | | | | | | |
|---------------------------|-------------------------|--------------------|-------------------|-------------|---------------------------------|
| (A) Strood High Street | (C) Intra High Street | Corporation Street | Primary connector | River Walk | Town centres |
| (B) Rochester High Street | (D) Chatham High Street | New Road | Major connectors | HHH Railway | Areas with masterplan proposals |

2.1.2 The SPD site

The framework area, known as Intra, is defined by the boundaries of the **Star Hill to Sun Pier Conservation Area**. It can broadly be divided into three areas:

The **High Street** runs from Star Hill Junction in the west to Chatham town in the east. It occupies both sides of the street and the land between the high street and the railway to the west of where the railway crosses the High Street.

To the north of the high street, lies the **Wharves and Lanes**, where a series of narrow lanes lead to an industrial hinterland adjacent to the River Medway.

The **River Edge** consists of walkways, piers, jetties and sections of flood defence walls.



Intra site boundaries and character areas



A red line represents SPD site boundary.
 Purple shading represents high street.
 Brown shading represents wharves and lanes.
 Blue shading represents river edge.
 A thick black line represents primary roads.
 A thin black line represents secondary roads.
 A dashed line represents pedestrian.

- SPD Site Boundary
- High Street
- Wharves and Lanes
- River Edge
- Primary Roads
- Secondary Roads
- Pedestrian

2.2 Site Character

2.2.1 Conservation Area

The Star Hill to Sun Pier Conservation Area exists as a unique combination of former maritime industrial hinterland, leisure and community uses, commercial high street, and residential area, that retains much of its historic 18th to early 20th century character and charm.

Located on the historic riverside route between Chatham and Rochester, development within the area was spurred on by the economic stimulus created by the establishment of the Royal Navy Dockyard and associated military garrison from the 16th century. Throughout this period the area thrived, and became home to a range of industries and activities such as the navy victualling yard, chandleries, shipbuilding and repair, brewing, importation, retail; as well as a range of leisure activities such as theatres, music halls, and pubs.

Evidence suggest that a modest Jewish community settled in the 18th century, with the Memorial Synagogue being built in 1869 on the site of an earlier synagogue dating from around 1750. Opposite the synagogue is the Ship Inn, which is thought to be one of the oldest LGBTQ+ pubs in the country and would have been a valuable space for a community which has been part of Intra;s fabric since its beginnings as a dockyard.

An important series of small lanes lead down from the high street towards the river where a range of functional buildings were built as and when they were required, giving the area a rather jumbled and ad-hoc character. This part of the riverside area is fronted by the more formal character of a mixed commercial and residential nature to the high street frontage. The overall character is split between the

now calm and often tranquil open riverside area; in contrast to the busy, often noisy, and enclosed High Street – but historically being co-existent and co-dependent.

To the east, the high street frontage mainly consists of three-story shops often with extravagant 19th and early 20th century decoration. Historically, the western part of the high street and St Margaret's Banks included a range of uses, but is now predominantly residential, comprising a range of small-scale historic buildings of various types, many of which date from the 18th and 19th century. Of particular importance to the character of the Conservation Area is the prevailing small-scale nature of development; its layout, form, fine grain, built fabric, and legible palimpsest that displays centuries of life, use, and culture.

Views into, out of, and across the Conservation Area are also especially relevant, providing a wider context to its significance. Some of the principal views include the repeated glimpses of the river and beyond from the high street, views of the high street and out to the higher ground beyond from the lanes to the south, and the views into and across the area from along the river and the surrounding high ground.

The intangible heritage is of notable importance too, with a long and established connection between the area and a range of diverse small independent businesses, creatives, leisure activities, and a diverse community passionate about protecting its unique sense of place.

Summary of significance

- A historic and characterful development pattern of narrow lanes, paths, and High Street built along the historic route between Chatham and Rochester.
- A townscape that readily reflects its historic maritime, leisure, community, commercial and residential uses - clearly definable into separate character areas.
- A range of small-scale historic buildings of varying ages and styles, creating an eclectic and jumbled character.
- It's important military and civilian supporting role to the Royal Navy dockyard at Chatham.
- Dramatic views into, out of, and across the Conservation Area that provide a wider context of its significance.
- It's intangible heritage of being home to a diverse community, small businesses, creatives, and leisure uses.

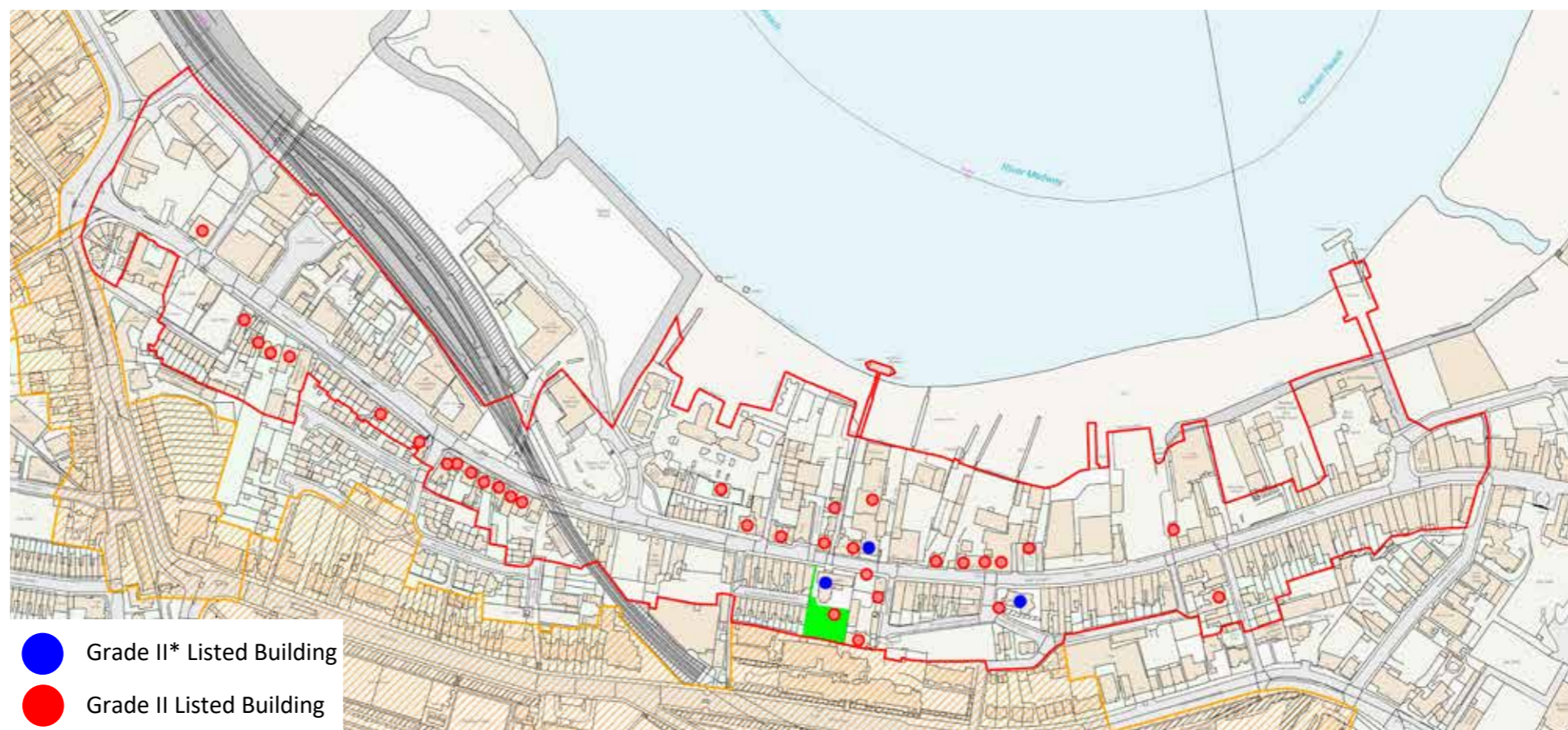
The Development Framework reflects the extents of the Star Hill to Sun Pier Conservation Area. Several other Conservation Areas are located nearby, including the Historic Rochester Conservation Area to the west, Brompton Lines to the east, and Star Hill and New Road Rochester Conservation Areas to the south – both of which adjoin the Star Hill to Sun Pier Conservation Area in which there are;

- **3 Grade II* Listed Buildings** (shown as Blue Circles on the site plan);
- **33 Grade II Listed Buildings** (shown as Red Circles on the site plan);
- **1 Grade II Registered Park and Garden** (shown as a Green Circle on the site plan).

Alongside the designated heritage assets such as Listed Buildings, the Conservation Area also contains an abundance of Non-Designated Heritage Assets that contribute to its character. The map shows the Non-Designated Heritage Assets within the Conservation Area that have been identified to date, however it may be the case that others are identified at a later time, such as through the Development Management process.“

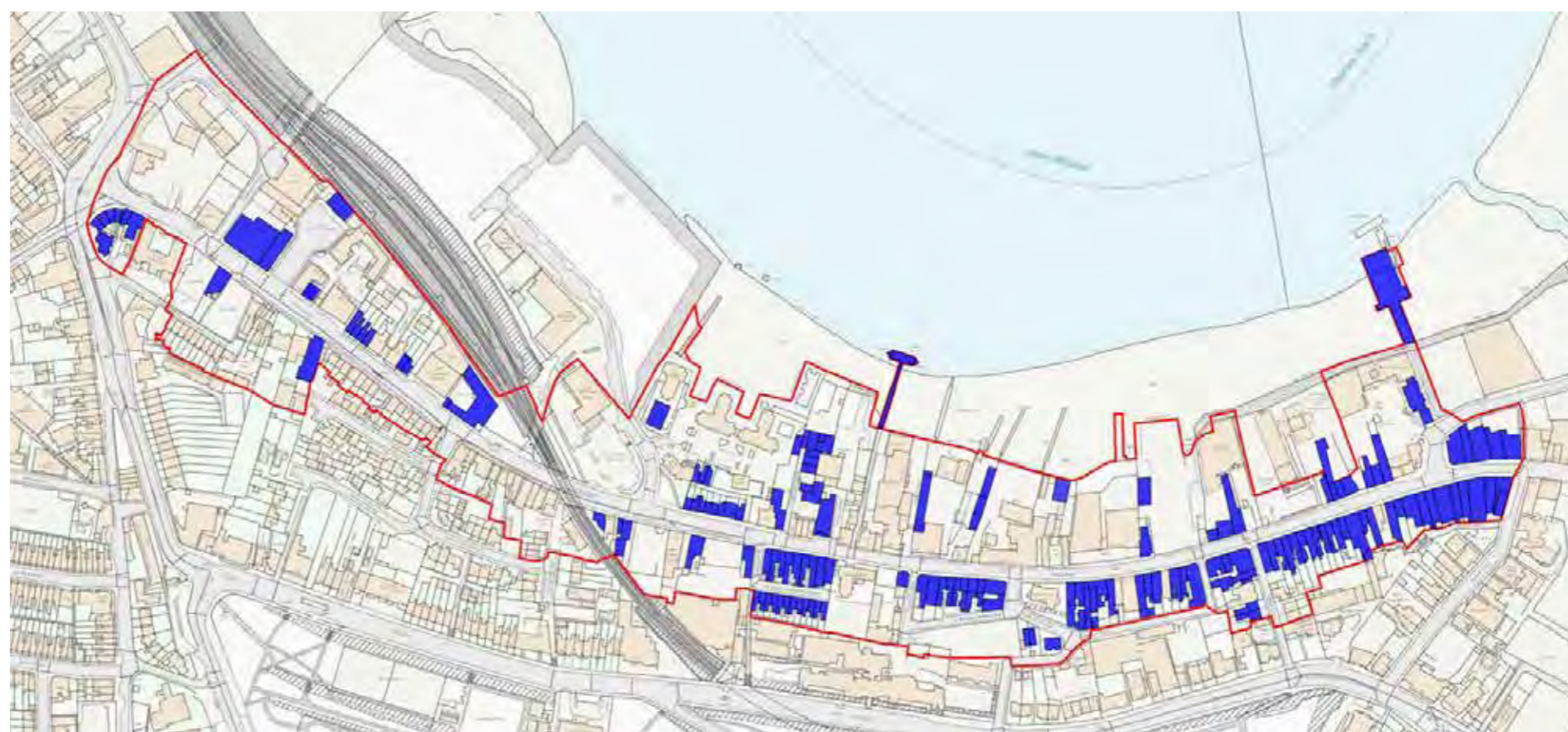
The NPPF sets out the Government’s approach to conserving and enhancing the historic environment, noting that heritage assets are an irreplaceable resource. Strategic Policy S1 of the Medway Local Plan identifies that long-term protection will be afforded to the historic built environment, and this includes the Star Hill to Sun Pier Conservation Area alongside other nearby heritage assets. This area therefore requires special consideration and a sensitive approach in terms of development which could impact the significance of these assets.

It is imperative that future development strives to enhance the character of the Conservation Area through retaining and repairing architectural features, or taking note of details or characteristics that may assist in new development integrating into the historic townscape.



● Grade II* Listed Building
● Grade II Listed Building

Designated assets within Intra



Non-designated assets within Intra

2.3 Areas within the SPD

2.3.1 High Street

Intra is defined by its narrow high street linking Chatham to Rochester from which a series of historic lanes lead down to the river on its northern side. On its southern side, streets zigzag uphill toward New Road.

The Western section extends from Star Hill Junction to Ship Lane. On the southern side, raised footways run along St. Margaret's Banks retained by historic brick walls. This section is primarily residential but also includes a mix of restaurants, pubs, shops, a college and a theatre. Along The Banks are several listed and locally significant buildings.

There is a well-preserved historic centre between St Bartholomew's Chapel opposite Boundary Wharf and Chatham Memorial Synagogue opposite Foundry Wharf. Here, there are some fine examples of historic buildings.

The eastern section, leading to Chatham town centre has a retail, food and beverage focus with a range of architectural styles.

The urban grain is largely intact along the southern side of the street with the exception of two parking lots to the west. To the north, it has been eroded over time and there are a number of large open plots which are vacant or used for car parking.

Along the length of High Street, there are also number of setbacks in the building line where more recent development has been aligned to accommodate potential road widening, exposing a number of gables.

The junction at Star Hill, where the High street is bisected by the A2, has 6 lanes at its widest, creating a barrier to pedestrian movement and severance between the high streets.

The High Street itself has a narrow carriageway which means that it is difficult for large vehicles to pass and easily gives rise to congestion. Combined with narrow footways, this creates a poor and dangerous pedestrian environment.



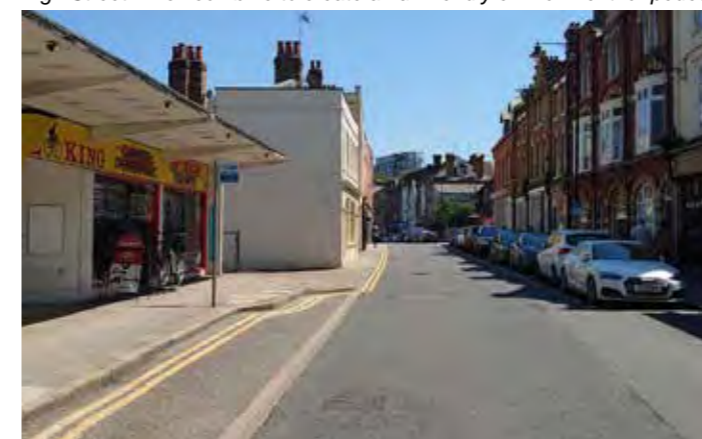
Star Hill Junction looking towards Rochester High St from Intra. The width of carriageway and the impediment to pedestrian movement is clear.



Height difference across the High Street which creates issues with permeability and access.



High Street - the narrowness of both the carriageway and the footpaths are very apparent. Parking also adds to the narrow feeling of this section of the High Street which combine to create an unfriendly environment for pedestrians and discourages footfall and sustainable transport.



Building setbacks and subsequent gable ends form part of the enduring street character along the High Street within Intra.



Shop fronts - these vary in layout, colour and style but display a uniformity in proportions that add character to the street with space for fascia boards.

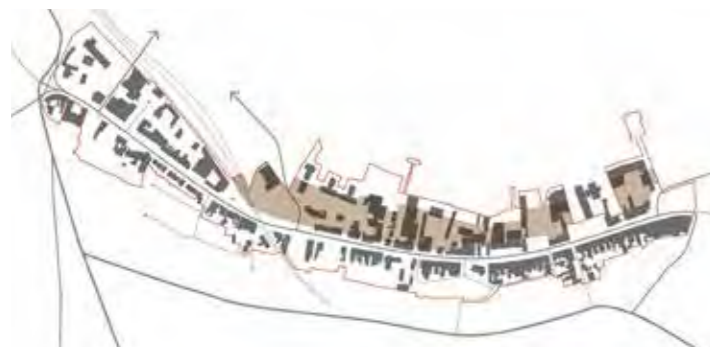
2.3.2 Wharves & Lanes

Situated between the high street and riverfront, the Wharves and Lanes area is characterised by an assemblage of warehouses, workshops and industrial buildings. The wharves have evolved from continuous industrial use and ad-hoc development in response to commercial need. They retain much of their historic character with several notable listed buildings including the Lion Brewery complex.

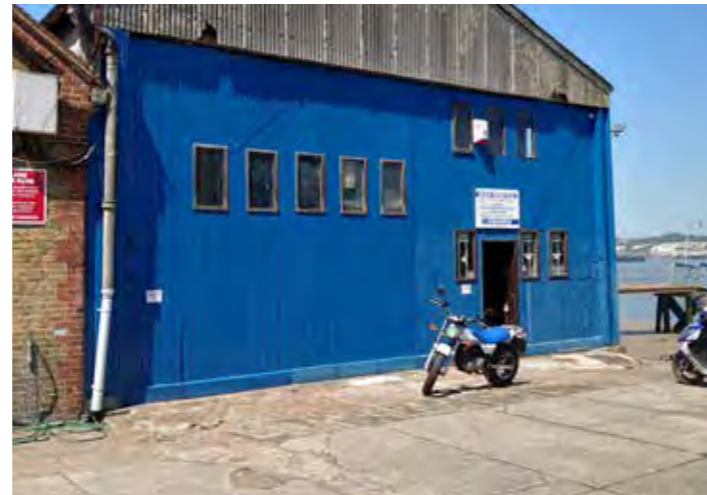
Long low-rise workshops and buildings typically ran perpendicular to the river along narrow lanes which then open out into yards. These buildings are the key components of the area's former industrial character and a number have been removed so that the lanes are less well defined and the yards enlarged.

The lanes themselves provide glimpses of the water and views beyond. They contain remnants of historic cobbles and granite kerb stones which allude to their age and historic character.

To the east and west peripheries of the Conservation Area, the historic significance and grain has been eroded through contemporary development. Development in these areas will need to form a sympathetic transition that integrates the new with the character and scale of the historic centre.



Former industrial buildings - part of the Lion Brewery complex



Shed / warehouse typologies that typify the wharves and lanes



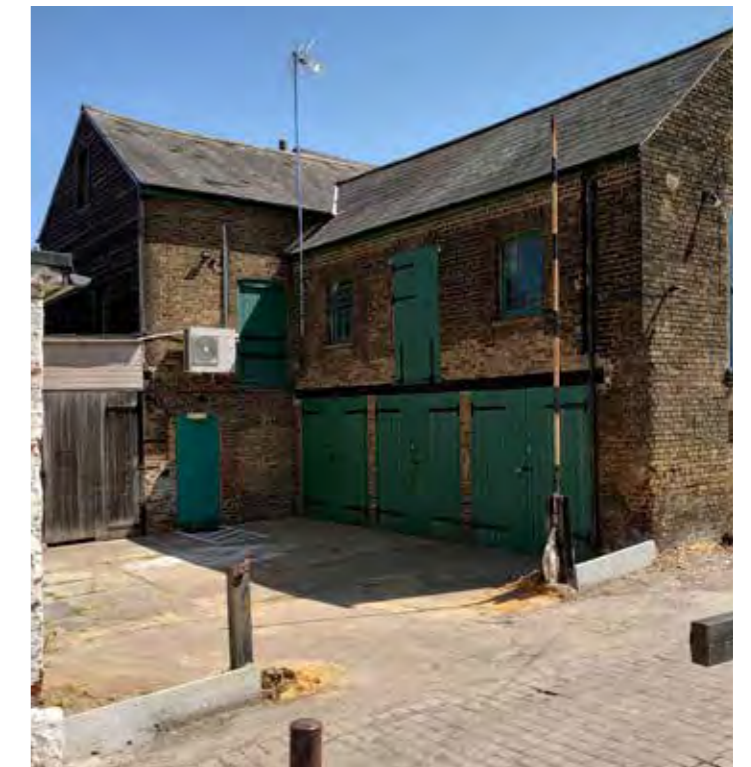
Narrow lane with warehouse typology buildings around a yard



Warehouse typologies in a yard formation - brick, corrugated steel and industrial materials typify the character of this area.



Narrow, brick lane with cobbles leading down to the riverfront.



Small brick buildings arranged to create yard spaces.

2.3.3 River Edge

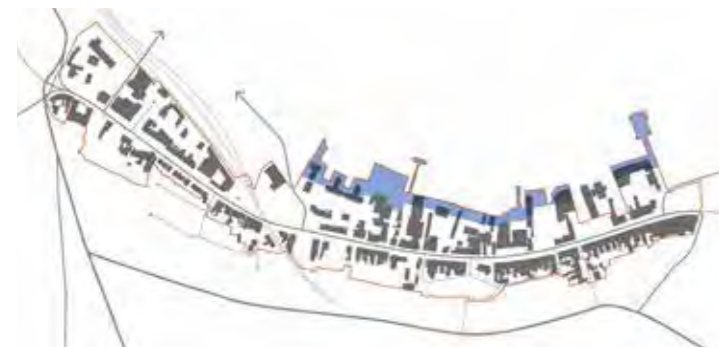
At Intra, the River Medway is over 300m wide, with a daily variation of over 4m and is navigable by large vessels for some 13 miles from Sheerness to Rochester.

In its heyday, the riverside would have been full of activity and spectacle, servicing an impressive array of vessels from ships of war to those transporting goods through to fishing boats, tugs, pleasure craft and ferries.

Today, it is much diminished with a number of resident houses boats, and facility to dock boats at Sun Pier. Many of the wharves and jetties are in a poor condition, derelict and dangerous, the river wall is in poor condition and the river shore is silting up.

Historically, east-west access along the waterfront was prevented by land ownership, although riverside access has been implemented to both the eastern and western peripheries of the Conservation Area as part of 20th Century development. Currently, these do not connect through the central section of Intra.

The river edge has the potential to act as a major attraction for Intra and to contribute to its revitalisation.



Sun Pier jutting out into the water



River Walk in front of Sun Pier House



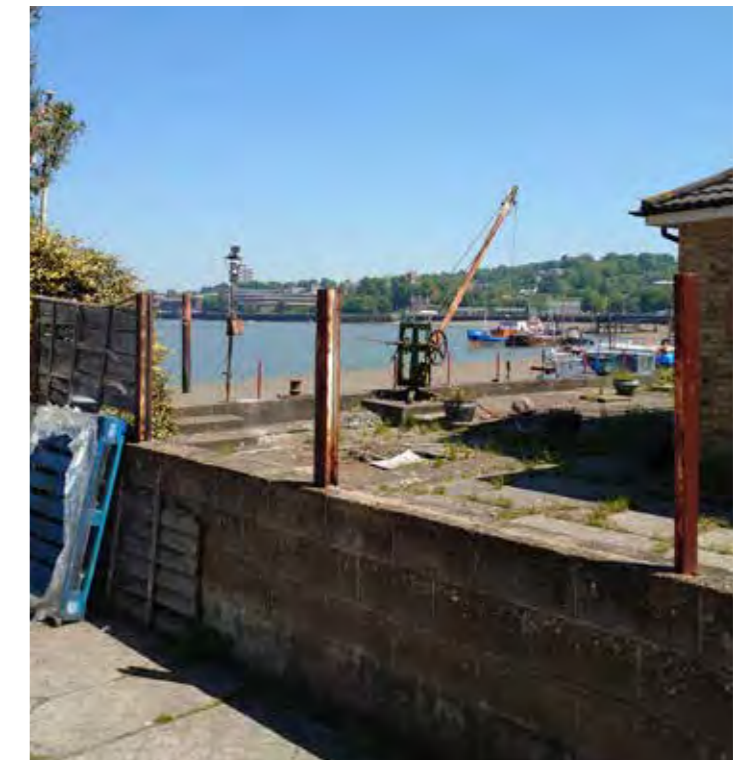
Salt marsh habitat establishing within the muddy banks



Existing jetties and piers within the central area which can / could be used for house boats and commercial activities.



The hard concrete edge where the lanes meet the river.



Remnants of former river activities litter the river edge.

2.4 Constraints & Opportunities

2.4.1 Constraints

Intra has a number of development constraints:

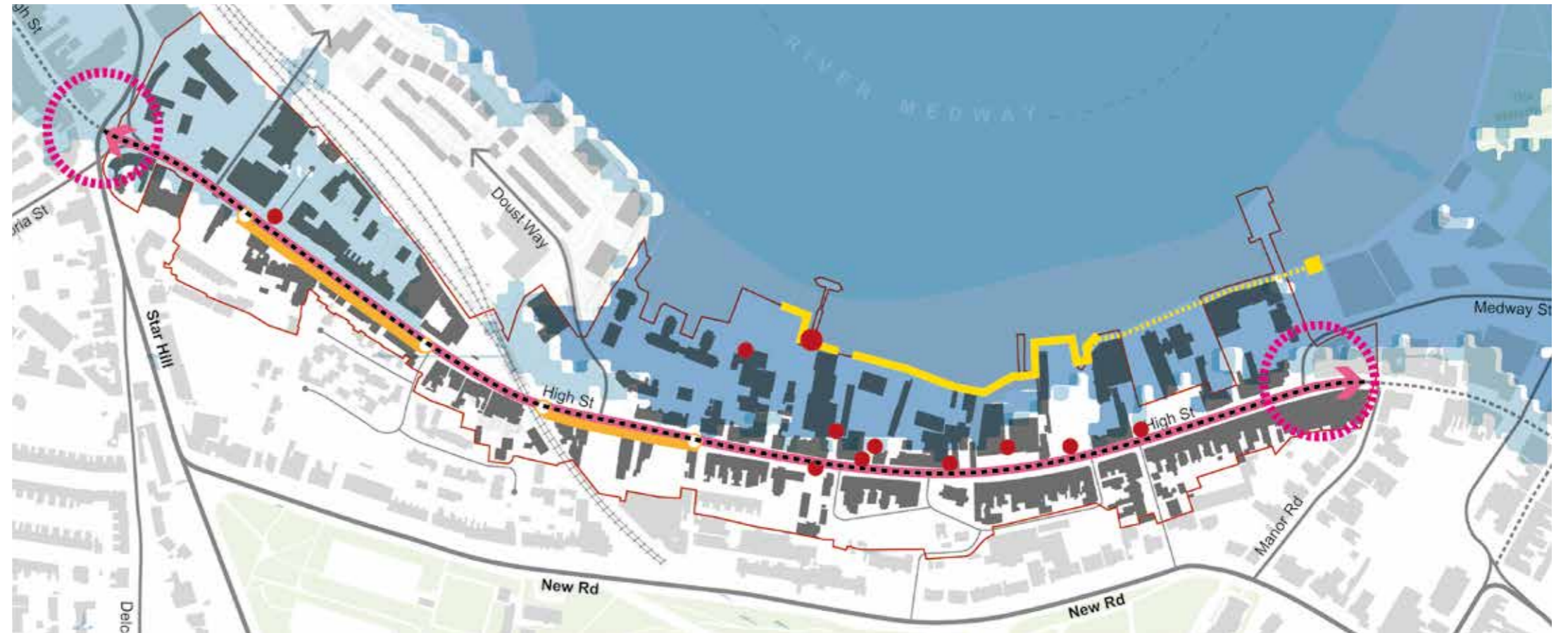
Flooding

Proximity to the river gives rise to the potential for flooding within the Wharves and Lanes which are located in Flood Zone 3 (see section 5). There are flood defences either side of Intra, at Rochester Riverside to the west, and at Sun Pier House and Chatham Waterfront to the east. There are currently no defences within the central section of the site.

Highways

The scale and crossing facilities at Star Hill Junction are a deterrent to pedestrian movement. The High Street has a relatively narrow carriageway giving rise to congestion. Narrow footways provide limited space for pedestrians, an issue which is further exacerbated through on-street parking. This arrangement provides no opportunities for spill-out spaces in the public realm or the inclusion of street trees to provide a range of ecosystem service and social benefits.

- Light blue shading represents flooding (zone 2).
- Blue shading represents flooding (zone 3).
- A pink dashed circle represents busy junction.
- A black dashed line represents high street traffic.
- A pink line represents narrow high street.
- An orange line represents elevated existing high street walls.
- Grey shading represents existing intra buildings.
- A red dot represents private access.
- A yellow line represents no riverside walk.
- A yellow dashed line represents hard landscaped riverside walk.



Constraints plan of Intra

- | | | | | | | | | | |
|--------------------|-------------------|--------------------|---------------------|-------------|----------------------------|--------------|--------------------------|--------------------|--------------------------------|
| Light blue shading | Flooding (Zone 2) | Pink dashed circle | Busy Junction | Pink line | Narrow High Street | Grey shading | Existing Intra buildings | Yellow line | No riverside walk |
| Blue shading | Flooding (Zone 3) | Black dashed line | High Street traffic | Orange line | Elevated High Street walls | Red dot | Private Access | Yellow dashed line | Hard landscaped riverside walk |

2.4.2 Additional Considerations

Historic Environment

The site itself encompasses the Star Hill to Sun Pier Conservation Area. Several other conservation areas are nearby, including the Historic Rochester Conservation Area to the west, and Star Hill and New Road Rochester Conservation Areas to the south. Each of these conservation areas have different characteristics, examples of built form, and spaces which the Council is seeking to protect and enhance.

Within the site and the Star Hill to Sun Pier Conservation Area, there are over 35 statutorily Grade II and II* listed buildings and historic structures. These are broadly found along the southern half of the High Street to the west, and within the more central section of the High Street. There is also a Grade II listed park and garden, comprising the Jewish Burial Ground associated with Chatham Memorial Synagogue.

Views

Set within the river corridor, the topography of Chatham gives a composition of historic significance and a series of views that enables the origins of modern Medway to be understood and appreciated. The intervisibility between Fort Amherst and Fort Pitt was fundamental to the defence of the River Medway and Chatham Dockyard and these views with their green skyline contribute to the understanding of Medway's historic strategic function. The historic wharves remain highly visible

from the river, and the views from High Street to the river connect the commercial centre to the water, differentiating it from the other high streets in the area. This connection and the visual link to historic dockyard beyond contribute to an understanding of the area's historic significance. Any future development within Intra will need to respect these existing views and viewing corridors, within their immediate context and further afield. Heights and massing must not impact upon historic views.

2.4.3 Opportunities

Heritage

The number and quality of Intra's heritage assets create the potential for a truly distinctive neighbourhood that is differentiated from its surroundings by the quality of its built environment.

Riverside Location

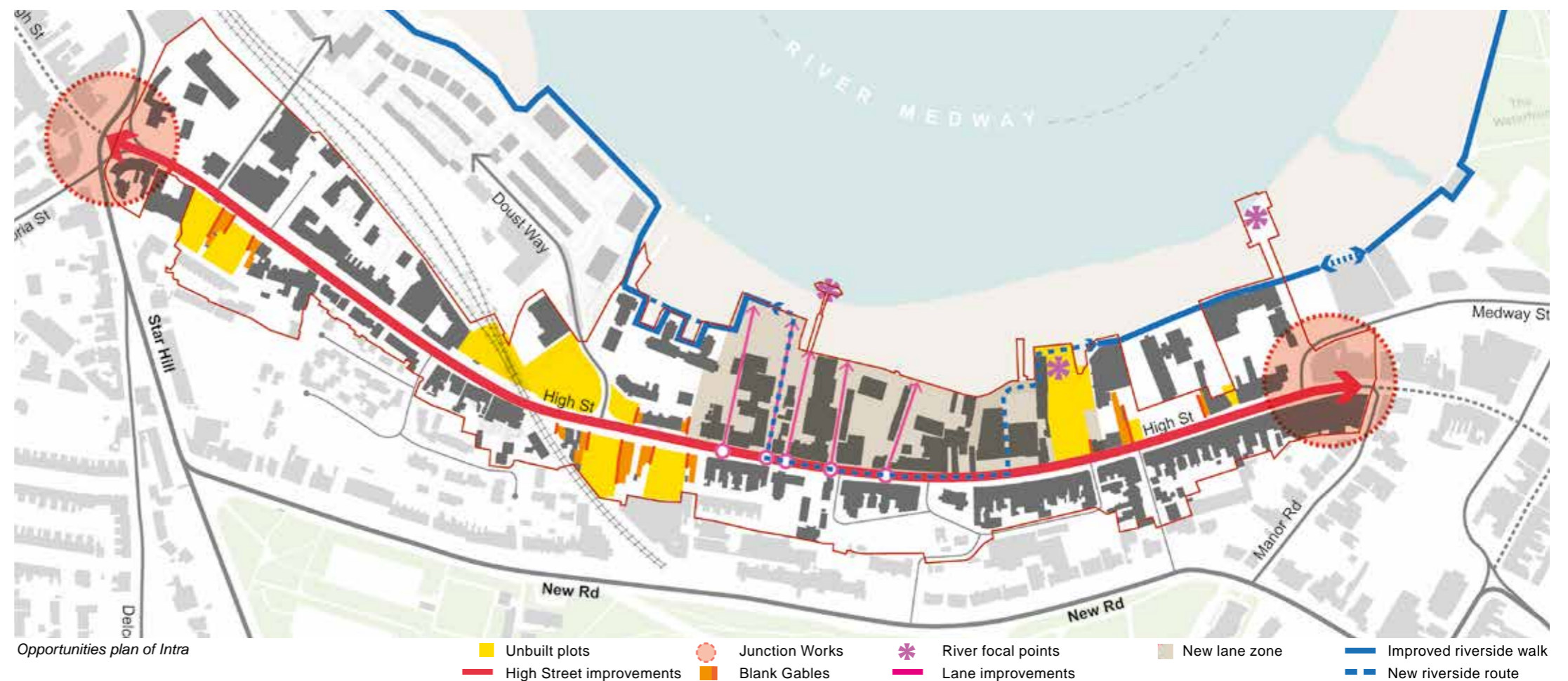
The benefit of these assets is reinforced by activating the waterfront with access routes/walkways also support healthier lifestyles, which is one of the key objectives for the Local Plan. The River Medway, providing attractive views over the water, creates potential for destinations within the Wharves and Lanes. Piers and jetties provide opportunities for increasing access to the river for recreation and leisure.

Creative and Retail Environment

Intra is known for its independent shops, creative industries and artist community which form a strong basis for its revitalisation. The promotion and inclusion of other retail opportunities such as eco-friendly, refill and reuse and repurposing should be highly encouraged to support sustainability.

Opportunity Sites

There are several unbuilt plots, some of which are vacant and others that function as car parks, that are early opportunity sites for development as they involve no demolition and can supply the quickest repairs to the high street. In other areas, there is an opportunity to infill sites and make repairs to the high street frontage as buildings have been lost over the passage of time.



Public Realm Enhancements

Junction improvement works at Star Hill, traffic calming and footway widening would make High Street safer and more attractive, increasing footfall and dwell times, leading to economic improvements and commercial opportunities.

Other elements along High Street provide improvement opportunities - setbacks in the building line can be retained and transformed into spaces for shops and people to use or for urban greening. Blank gables could be utilised as part of art

strategies, enhancing the local identity of Intra.

The lanes contribute to the character of Intra and there is an opportunity to reinforce their character, make them more publicly accessible and to reinstate routes that have been lost.

Urban Greening and Climate Adaptation.

There is the potential to provide a number of new open spaces within Intra – creating spaces for people and wildlife. New wetland habitats could be integrated into the river's edge, promoting wildlife and ecology benefits. Tree planting and

urban greening can be used to provide a range of ecosystem service and social benefits. For example, trees help with the response to climate change, reduce the urban heat island effect, contribute to sustainable urban drainage, provide habitats for wildlife and enhance air quality under certain circumstances. Trees and urban greening also improve human health and wellbeing, providing opportunities for people to connect with and enjoy nature.

2.5 Environmental Factors

2.5.1 Flood Risk

Development must:

- ▶ Submit a Flood Risk Assessment (FRA) with Planning Applications. It will inform how the mechanisms, design solutions and effects of flooding will be mitigated.
- ▶ Demonstrate Sustainable Urban Drainage Systems (SuDS) which work as standalone proposals and contribute to the reduction of flood risk as part of a joined-up strategy across a wider area.
- ▶ Be designed to accommodate flood risks without compromising urban design principles.

▶ Flood risk mitigation measures should be prepared in accordance with the Medway Estuary and Swale Strategy (MEAS) and the following Local Plan 2003 policies.

- Policy CF 13: Tidal Flood Risk Areas
- Policy CF12: Water Supply
- Medway Strategic Flood Risk Assessment
- Medway Local Flood Risk Management Strategy
- Medway Surface Water Management Plan
- Medway SuDs Water. People. Places. A guide for masterplanning sustainable drainage into developments

! The Medway Estuary and Swale Strategy (MEAS) will provide developers with detail on the required defence heights for this frontage. Financial contributions may be sought from developers to raise or maintain flood defences.

National Design Guide C1, N2, R3

Vision Principles



Development proposals must demonstrate how they would contribute to reduced flood risk and avoid exposing new residents to unacceptable flood risks. Parts of the Site are at significant risk of flooding from the River. The Environment Agency Flood Map shows that the area between the high street and River Medway is classified as Flood Zone 3 and a 1 in 200 or greater annual probability of river flooding.

The sites adjoining Intra have responded to this condition through the implementation of a river wall at Sun Pier or increasing land levels at Rochester Riverside. The assumption should initially be that developers need to raise flood defences, in accordance with Medway Estuary and Swale Strategy (MEAS), which could then be supported by a combination of site-specific flood mitigation measures.

However, flood defence measures will need to be sensitively integrated into the public realm and not adversely impact on the character of Intra's built heritage. Applicants should liaise with the Environment Agency, Medway Council and Historic England to review the approach to flood defences to determine the most sensitive approach to safeguarding the historic environment.

In addition, flood defences should not cut off views and access to the river from within the area and should not isolate historic buildings.

There are several potential measures that can be incorporated as a means of mitigating the effect of floods: the first is to locate buildings and spaces away from areas of the highest flood risk, and then to contribute to appropriate mitigation and a joined-up approach to sustainable drainage.



Flood Zone mapping of Intra

Light blue shading represents flood zone 2. Blue shading represents flood zone 3. A blue line represents flood defences.

It may be possible to incorporate flood defences into the fabric of buildings to provide property line defence. Alternatively internal spaces below flood level could be made more resilient to the effect of flooding by the use of impervious materials. There is also potential to incorporate floodable voids in the ground floor of buildings or areas of external amenity.

In addition, non-residential space could be provided at ground floor with any residential accommodation placed on the upper levels of a building to provide an active frontage. Access and egress to sites and buildings will need to be considered in all cases. Safe dry access should be provided to account for residual flood risk. Buildings should still be provided with step-free access to entrances and positively engage with the street scene. Where buildings are raised above the flood level, this should be done

sensitively and should not compromise their design or the surrounding public realm.

A licence is required from the Environment Agency for permission to build within 15m of the river front. However, in order to provide the robust and varied character of the area, it will be appropriate to have new development forward of this line, subject to appropriate mitigation measures and agreement of the Environment Agency.

Light blue shading represents flood zone 2. Blue shading represents flood zone 3. A blue line represents flood defences.



2.5.2 Adaptation to Climate Change

Development must:

- ▶ Be designed from the outset to adapt to climate change and avoid overheating.
- ▶ Ensure that passive cooling is used rather than mechanical ventilation or cooling where possible.
- ▶ Use high quality insulation and prevent thermal bridging.
- ▶ Involve energy specialists and integrate sustainability considerations into the design as early as possible.
- ▶ Minimise the consumption of water.
- ▶ Check and comply with Medway Council's vision and climate targets.

Development should:

- ▶ Ensure designs are future-proofed to allow for the inclusion and upgrade of technologies as they emerge.
- ▶ Ensure that the most up-to-date guidance is followed, met and where possible exceeded to ensure longevity and robustness within the built environment.

Medway Council declared a Climate Emergency in April 2019 and passed a motion committing to reduce its carbon footprint and provide the local community with a clean, green future and be a place people want to work and live, which has a sustainable future. All development proposals must incorporate measures to adapt to climate change and avoid overheating within developments.

All buildings should follow a hierarchy in design in order to create the most sustainable developments possible. This starts with incorporating passive design, maximising the thermal performance of the envelope then implementing efficient fossil-fuel free services.

Adopt Passive Design Measures

Buildings must be designed to allow natural ventilation, avoid overheating, and the need for mechanical cooling. Buildings should be designed to take advantage of sunshine in the winter months and to provide shading and passive cooling during the summer.

Residential dwellings should also be dual aspect. Homes which are single aspect must have ventilated corridors, to avoid corridors acting as heat sinks and preventing cooling.

Optimise Insulation

The building fabric used should optimise insulation, alleviate thermal bridging and be as airtight as possible to prevent the loss of internal heat during winter and prevent heat from entering during the summer. Energy specialists should be involved early in the design process to ensure that proposals have

captured all opportunities to enhance sustainability and energy performance.

Limit Water Usage

Buildings should incorporate measures to encourage lower water consumption and be accompanied by a SUDS strategy that which promotes reduced water consumption.

Reducing the carbon footprint

Development proposals must seek to reduce their carbon footprint as much as possible and proposals must comply with the latest guidance on sustainability measures set out by both local and national guidance. Designs must seek to incorporate Renewable Energy where possible and consider circular economies when choosing material, in addition to using future-proof design to allow the use of new technologies as they emerge.

2.5.3 Air Quality

Road traffic emissions are a significant contributor to poor air quality and its associated health problems in Medway. The Central Medway Air Quality Management Area (AQMA) and the Air Quality Annual Status Report 2017 confirmed that nitrogen dioxide concentrations within the AQMA continued to exceed annual mean air quality objectives. A number of strategies are put forward within the SPD including:

- Traffic calming and management to reduce vehicles using Intra as a through route.
- Parking provision limited in new developments to commercial, car club and blue badge only.
- Provision of electrical car charging points.
- Introduction of urban greening.



Extract from the Central Medway AQMA showing the affected area

2.5.4 Biodiversity and Sustainability

Development must:

- ▶ Achieve Biodiversity Net Gain.
- ▶ Increase tree canopy cover and urban greening and as integrated part of the design.
- ▶ Incorporate Sustainable Urban Drainage.
- ▶ Provide open space and habitat that is part of a considered green infrastructure strategy.

Development should:

- ▶ Check and comply with Medway Council's vision and most recent sustainability targets.
- ▶ Ensure that the most up-to-date guidance is followed.
- ▶ Work with an ecologist to incorporate biodiversity enhancements

Developers will be required to implement a number of measures as part of their proposals that contribute to creation of an attractive biodiverse environment with well-integrated sustainable drainage, ecology, shading, improved air quality and outdoor space that form part of the wider green infrastructure and ecosystem services network:

Biodiversity Net Gain

Under schedule 12 of the Environment Act a 10% Biodiversity Net Gain (BNG) will be mandatory. The Council may set targets higher than 10% and in all instances will encourage and support development which exceeds the expected 10% BNG requirement.

Urban Greening Factor

The Green Infrastructure Planning and Design Guide, seeks an uplift in the greening of residential urban neighbourhoods to at least 40% average green cover. This might not be achievable when taking account of SPD area constraints. New development, however, should seek to implement interventions that aid biodiversity, including the use of bio-diverse (green/blue/brown) roofs, living walls etc.

Tree Canopy Cover

Developers will be required to increase Urban Tree Canopy Cover as part of their applications. It is important that the right trees are planted in the right places for the right reasons, with space for them to grow and conditions that aid their establishment.

Sustainable Urban Drainage

Sustainable Urban Drainage systems (SuDS) shall be incorporated where possible to manage the surface water runoff from streets and development areas, reducing the risk of flood and pollution and contributing to environmental enhancement, biodiversity and placemaking. A number of approaches can be implemented from increased soft landscape, rain gardens – designed to attenuate and allow water to infiltrate more slowly, to permeable paving.

Roosting and Nesting Sites

Provide bird and bat boxes and utilise flat roofs where appropriate to create brown and green habitats for invertebrates and birds.

Environmental Health

In line with the Council's planning policies, environmental health matters including noise, vibration, and land contamination will be considered on a case-by-case basis. Developers and applicants should ensure that good design practice with regard to these matters is adopted, including following relevant guidelines and British Standards.

This is particularly important where there can be conflicts between considerations such as noise mitigation and conservation requirements. This can usually be overcome by considering noise impacts and mitigation early in the design stage alongside any heritage and conservation matters.

Strategies

A number of strategies are put forward within the SPD. These strategies must provide a contextual and place specific response that adhere to best practice and complement Intra's historic character including:

- Introduction of tree planting, rain gardens and Sustainable Urban Drainage within High Street.
- Introduction of tree planting, greening and Sustainable Urban Drainage within the Wharves and Lanes.
- Enhancement of riverine habitats within the River Medway as part of the wider ecosystem services network.
- Enhancement of the environmental performance of historic buildings and design of sustainable new ones.



Chapter **3.0**
Vision & Objectives

3.1 Shaping Intra

Intra is celebrated for its rich heritage, including its historic high street and complex of lanes, wharves, and riverfront spaces, making it an attractive place for people to live and work. Future growth, however, must be harnessed in a sustainable way and be led by the significance of Intra's unique heritage.

New development will be designed to integrate with the historic environment, seamlessly bringing together the old and new, set within a series of revitalised spaces to enhance the special qualities of Intra.

Sustainable modes of transport will be prioritised to make the high street, lanes, and the riverfront more inviting, accessible, and safe. New trees and public spaces will lead to increased footfall, improved air quality and greater biodiversity.

Together, and led by the conservation and enhancement of its heritage, Intra will become a destination renowned for its history, local and independent creative industries, and the arts.





River Medway

ROCHESTER RIVERSIDE

FORT PITT GRAMMAR SCHOOL

UNIVERSITY FOR THE CREATIVE ARTS

JACKSONS SKATEPARK

JACKSON'S FIELD

PLAY!

VICTORIA GARDENS

NEW ROAD

HIGH STREET

CELESTIAL CHURCH OF CHRIST

CHATHAM MEMORIAL SYNAGOGUE

RESTAURANT

3.2 Aims & Objectives

3.2.1 Design Principles for Chatham Intra

These principles have been aligned with the overarching themes of strategic planning policy in the adopted and emerging local plans.

PRINCIPLES	Celebrating Heritage & Local Identity 	Creating great new homes 	Improve Intra Permeability 	Stimulating Economic Vitality 
OBJECTIVES	<ul style="list-style-type: none"> Reinforcing Intra's identity by drawing on its rich history and raising its profile. Securing the retention, restoration and adaptive reuse of historic buildings. Ensuring new development is contextual and place specific. Protecting Intra's skyline and important views from overdevelopment. 	<ul style="list-style-type: none"> Delivering homes with a rich mix of dwelling types, styles, sizes and tenures to support the local community. Creating integrated communities that support the local economy and reduce inequality. Designing to a high standard. Serving a wide range of services and facilities to support existing and new residents alongside required infrastructure. 	<ul style="list-style-type: none"> Creating a more pedestrian and cycle focused environment. Improving connectivity between Rochester and Intra High Streets. Reinstating lost historic lanes between the high street and the river. Connecting the riverwalk back into the wider area and improving access. 	<ul style="list-style-type: none"> Creating an environment that supports a sustainable mixed-use economy. Supporting local independent businesses, creative industries and further / higher education providers. Encouraging retail / commercial ventures that supplement existing town centres. Supporting reinvestment in urban fabric.
LOCAL PLAN AMBITION	<ul style="list-style-type: none"> Enhancing special historic and architectural interest Fostering a strong local identity, sense of place, and community Encouraging development which reflects local character and ambitions. Using existing heritage assets to drive urban renewal 	<ul style="list-style-type: none"> Addressing local housing need and the demand for homes Securing high quality housing and built environment design. Encouraging healthy lifestyles and improved wellbeing Safeguarding the amenity and character of existing neighbourhoods 	<ul style="list-style-type: none"> Improving accessibility for all Maximising access to local services, facilities, and opportunities Encouraging sustainable modes of transport 	<ul style="list-style-type: none"> Promoting development through reinvestment in the urban fabric Supporting the competitiveness of the local economy Maintaining and enhancing local centres and high streets

PRINCIPLES

Creating a Thriving High Street



Reimagining the Wharves & Lanes



Increasing Use of the River Medway



Promote Biodiversity & Sustainability



OBJECTIVES

- Creating an attractive destination with a variety of suitable uses that people want to visit and spend time in.
- Enhancing the historic high street character by infilling gaps with sensitive development.
- Improving the built environment to make places for people and a safer, cleaner and more attractive space, and which pedestrians and cyclists find more welcoming.

- Protecting existing industrial heritage and character and referencing their materials and typologies as a template for new development.
- Encouraging public access, accessibility and activity.
- Ensuring flood defences are integrated into the public realm in a non-obtrusive manner as part of measures to mitigate flooding more widely.

- Creating riverside dwelling spaces to allow for sitting and reflecting.
- Improving public access along the river including connecting the river walk.
- Improving infrastructure for boat and river vessels.

- Contributing to a strong green infrastructure network.
- Retaining, restoring and reinstating historic open spaces.
- Creating opportunities for urban greening and intertidal habitats.
- Addressing the challenge of climate change by improving building performance, air quality, urban cooling and sustainable urban drainage.

LOCAL PLAN AMBITION

- Encouraging economic recovery, diversity, and resilience
- Improving safety by prioritising pedestrians and cyclists
- Reducing congestion and pollution
- Creating a pleasant, vibrant, and green high street environment

- Safeguarding and improving the Wharves and Lanes
- Creating high-quality public spaces
- Bringing people to the riverfront

- Encouraging greater water-based uses
- Supporting river-based industries
- Improving connectivity to, and along, the River Medway

- Improving the environment and variety of life
- Protecting and improving open spaces
- Fostering social, environmental, and economic equity
- Responding to the climate emergency

3.3 SPD Vision Focus Areas

3.3.1 The High Street

High Street shall continue to be a concentration of Intra's commercial activity with emphasis placed upon supporting small, independent and local businesses. Retention, conservation and reuse of historic buildings will be prioritised, whilst encouraging high quality new buildings to infill gaps in the high street. These shall contribute to the streetscene, relating to the surrounding historic context in terms of scale, massing, materiality and detailing.

Public realm enhancements shall improve pedestrian accessibility and safety, encourage footfall and provide spaces for people to dwell and spend time. Alternative, sustainable modes of movement will be prioritised to reduce car dependency for local journeys and discourage use of the high street in Intra as a through route.

Tree planting will be introduced to green the high street, providing opportunities for people to connect with and enjoy nature. Trees will also provide a range of ecosystems service benefits, such as to reduce the urban heat island effect, provide shade, contribute to sustainable urban drainage, and improve biodiversity and air quality.

In order to fulfil the vision for the High Street, new developments must address the design principles for Intra below. Details on how to deliver these principles specifically can be found in **Section 4**.



Celebrating heritage & local identity

- Retaining, conserving and celebrating buildings of historical importance along the High Street such as the Ship Inn and Chatham House amongst others;
- Take design cues from the surrounding architecture including architectural details, materials and scale.



Creating great new homes

- Ensuring apartments of different sizes are provided above the ground floor commercial offer in all developments to provide for all types and sizes of families;
- Building sustainable homes with dual aspect using appropriate and robust materials.



Stimulating economic vitality

- Creating and supplying new retail / commercial space at ground floors to activate the High Street;
- Using public realm enhancements to provide spill-out space for drinking and dining along the High Street.



Creating a thriving High Street

- Ensuring new development provides a wide variety in size and scale of commercial properties to attract a mix of businesses;
- Using infill developments to restore the historic building line enclosing the High Street contributing to a regenerated, safe and unique environment.



Improve Intra permeability

- Providing public realm improvements alongside new development such as narrowing the carriageways and widening the footpaths.
- Making it safer and easier for pedestrians and cyclists to move along the High Street through Intra by slowing vehicular traffic.



Promote biodiversity & sustainability

- Installing pocket parks, parklets and new trees within the public realm and improving the green aspect for businesses, residents and users;
- Creating small formal / informal open spaces within suitable and identified locations along the High Street.

3.3.2 Wharves & Lanes

The Wharves and Lanes are a key pieces of urban tissue that connects High Street with the River Medway. They form the historic core of Intra and once contributed to the economic prosperity of the area. In addition to preserving and enhancing existing lanes, 'lost' lanes will be reinstated when new development comes forward, increasing connectivity and improving access to the riverfront.

Historic buildings will be restored and sensitively adapted to suit new contemporary uses. New development must respond in scale, massing height and configuration to the historic precedent to create a response that is contextual, place specific and enhances the special qualities of the Intra.

Development should be of a high quality and serve a dual purpose – It needs to contribute to enclosing the lanes, maintaining their narrow, human-scale whilst also creating shared yard spaces in between the lanes which can be used as spill-out spaces supporting commercial ground floor uses as well as providing with courtyard spaces for residents.

In order to fulfil the vision for the High Street, new developments must address the design principles for Intra below. Details on how to deliver these principles specifically can be found in **Section 5**.



Celebrating heritage & local identity

- Retaining, conserving and celebrating buildings of historical importance along the Lanes such as the Lion Brewery and using historic features such as cobbled lanes;
- Use warehouse typologies and yard arrangements to create a unique streetscape within the lanes.



Creating great new homes

- Ensuring houses and apartments of different sizes are provided for all types and sizes of families along the lanes whilst respecting the flood zone parameters;
- Ensuring tight knit developments provide sufficient privacy and avoid direct overlooking between neighbours.



Stimulating economic vitality

- Creating and supplying new business premises for creative and light industrial uses at ground floor and within courtyards to activate the lanes;
- Creating safe, direct and new links from the lanes to the commercial attractors along the High Street, uniting Intra.



Reimagine the Wharves & Lanes

- Revitalising the wharves and lanes with new creative small businesses and shared spaces where new communities can mix and grow;
- Using the lanes to create improved access between High Street and the river whilst becoming a destination in their own right.



Improve Intra permeability

- Improving and enhancing existing lanes through public realm improvements and improved public access.
- Reinstating historic lanes that have been lost over time and providing brand new lanes, increasing the connections to the river throughout the local area.



Promote biodiversity & sustainability

- New developments are encouraged to consider green walls and climbing plants along facades and elevations;
- Architectural designs could utilise rainwater catching and grey water systems in addition to installing PVs onto any flat roof developments.

3.3.3 River Edge

The River Edge has been the key driver for the development of the area over time, acting as the economic, cultural and visual backdrop to Intra and still evokes a strong sense of character.

The river has the potential to once again act as a major attraction and to contribute to the revitalization of Intra. With investment, there can be increased use for transporting goods, entertainment, moorings, passenger tours and trips as well as associated riverine activities such as outdoor dining overlooking the water, enjoyment of views and the riverwalk.

The framework sets out a number of focal and destination points along this stretch of the River Medway that can be implemented by applicants as individual developments come forwards. This should have the positive effect of drawing people towards the river edge, especially in conjunction with development within the wharves framing the river. The river edge also has the potential for significantly enhanced biodiversity and habitat creation.

Where maintenance of the water's edge permits, trees and other greening could be established along the riverwalk. New trees will provide opportunities for people to connect with and enjoy nature. Trees will also provide a range of ecosystems service benefits, such as to reduce the urban heat island effect, provide shade, and improve biodiversity and air quality.

In order to fulfil the vision for the High Street, new developments must address the design principles for Intra below. Details on how to deliver these principles specifically can be found in **Section 6**.



Celebrating heritage & local identity

- Retaining, conserving and celebrating items of maritime culture found along the river edge such as boat cranes and historic docks and piers;
- Reinstating active frontages and commercial activities along the water's edge reflecting its past economic significance to the area.



Creating great new homes

- Ensuring no residential bedrooms are provided at ground floor and appropriate flood defences have been designed into all new buildings along the waterfront;
- Maximising blue outlooks across the river environment for residents to enjoy with generous fenestration and large balconies.



Stimulating economic vitality

- Creating and supplying new retail / commercial spaces at ground floors to activate the waterfront;
- Using the jetties and piers to run river related commercial and leisure activities, especially at Sun Pier in the east.



Increasing Use of the River Medway

- Using the river for leisure activities such as cruises, kayaks and paddle-boarding;
- Installing new tidal ecological habitats and board-walks for people to enjoy;
- Protect existing houseboat community and promote expansions through new moorings



Improve Intra permeability

- Ensuring more of the riverfront is accessible and connected.
- Connecting Rochester to Chatham through new riverside walks - perhaps even new sections of board-walks.



Promote biodiversity & sustainability

- Fostering inter-tidal ecosystems and habitats along the mud-flats and riverbank
- New planting and trees could be installed along the riverwalk to provide greenery and shading.



Chapter
The High Street 4.0

4.1 Built Form

4.1.1 Street Character

Development must:

- ▶ Retain existing setbacks and use them for enhancement of the public realm.

Development should:

- ▶ Use gables for murals, artwork or painted signage that reinforce Intra's identity.
- ▶ For new development, undertake a street view assessment to ascertain if a setback and exposed gable would benefit their location.



Images showing the series of blank gable ends along the High Street to be retained and celebrated

Building Line

There is a distinct historic building line along the length of the high street which is interrupted by a number of setbacks where modern 20th century redevelopment has been realigned to accommodate potential road widening that did not come forwards. These occurs at periodic intervals and add a sense of depth and character.

Keeping Gable Ends

A consequence of these setbacks is that they reveal gable ends along High Street and create another characteristic of Intra. Some gable ends remain blank but others have 'ghost' signs or more contemporary street art.

The gables / setbacks identified within the diagram must be retained and utilised as a minimum.

New Gable Ends

Any new gables that are formed through infill development should emulate their character using materials, colour or art to contribute to the identity of Intra. These gables should articulate return elevations in such a way as to encourage exploration of the side lanes. Emphasis should be placed on providing active frontages to side gables whenever possible.



- 01 Upper section on south side of St Margaret's Bank
- 02 Lower section on north side of St Margaret's Bank
- 03 Corner of High Street and Doust Way
- 04 Corner of High Street and Sufferance Wharf
- 05 Corner of High Street and Foundry Wharf
- 06 On the High Street near Ship Lane
- 07 21 High Street



4.1.2 Scale & Massing

Development must:

- ▶ Conserve the rhythm and scale of the historic frontages along High Street.
- ▶ New frontages to be approx. 6m in width and architecturally expressed as such if the frontage is longer.
- ▶ 2 - 3 stories are generally acceptable although Any proposed buildings which are taller than specified must be of exceptional design quality, established through a rigorous design review process, and protect / enhance the heritage context.
- ▶ Frontage strategy must provide non-residential active frontages on the ground floor with a high level of glazing.
- ▶ Dormer windows are permitted but they cannot be full-width and must relate proportionally to the rest of the facade.

Development should:

- ▶ Allow rooflights to be used but ensure they are restricted to the non-high street facing side of the roof.
- ▶ Built-over courtyard / lane entrances could be considered within the streetscene.



Rhythm of the tall, narrow frontages along the High Street



Commercial ground floor that is distinct in the facade



Gable fronts along the High Street and used to end runs



Slight variation in heights along the High Street adds variety



Dormer windows as part of a High Street roof typology



Built-over entrances into courtyards and streets



Frontages

Buildings along the high street typically have narrow frontages which create a strong vertical emphasis and rhythm which must be reflected in new development. Ground floors should be taller to accommodate non-residential uses

Roof Form & Heights

Proposed buildings must conform to the existing building heights of 2-4 storeys along High Street to create an acceptable level of consistency. The roof form and ridge heights shall relate to their immediate neighbours and context.

Rooflights are acceptable, although preferred to be on the rear of the roof (non-high street facing). Dormer windows are acceptable but they must not be the full width of the elevation and must be proportional to the rest of the facade.

Note: Any proposed buildings which are taller than specified in the SPD must be of exceptional design quality, as established through a rigorous design review process, and protect and enhance the surrounding heritage context.

Built-over Entrances

In the west, built-over entrances to courtyards and

spaces are more prevalent, particularly along the space between High Street and the railway (west of Five Bells Lane). They contribute to the streetscene by providing glimpses of spaces and activities beyond and could be replicated in future designs.

Most entrances remain open to public access and this should be encouraged as the standard so that gates / fencing do not adversely impact upon the streetscene.



Developers must undertake an elevation and materiality study of the surrounding streetscene & architectural character to help the planning authority to understand how appropriately a proposal responds contextually.

National Design Guide
C1, C2, I1, I2, I3, B2

Vision Principles



4.1.3 Architecture

Development must:

- 01 Predominantly utilise pitched roofs whose slope is locally determined by context.
- 02 Employ feature detail / material change in facade to create visual interest.
- 03 Ensure the ground floor / each 6m frontage are distinct.
- 04 Ensure ground floors have taller floor-to-ceiling height for non-residential uses.
- 05 Not have high street facing balconies / Juliet balconies.
- 06 Locate shared core entrances between shops to maintain facade rhythm.

Development should:

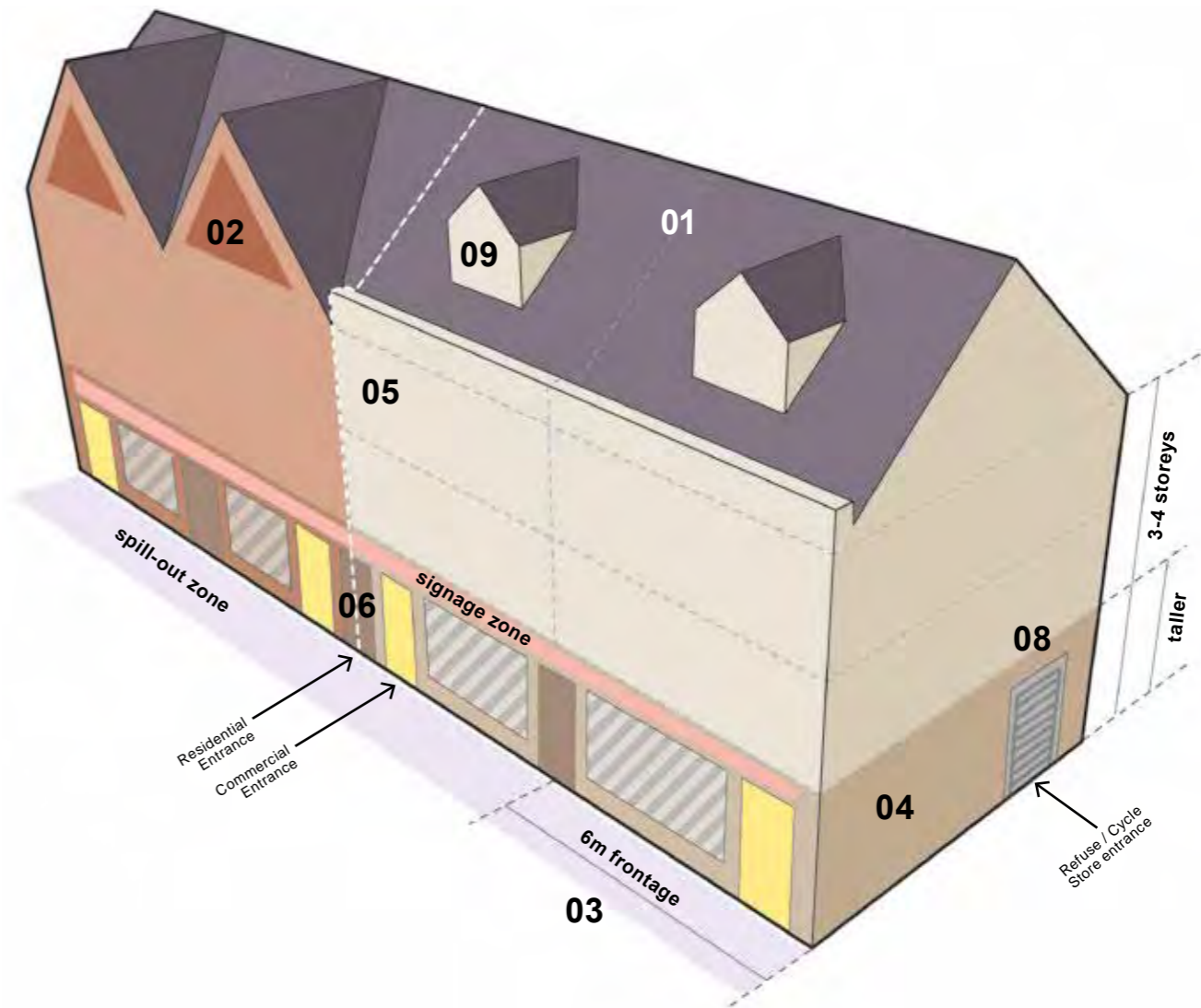
- 07 Locate the core to the rear of layout to maximise frontage onto the high street.
- 08 Create access to the refuse/plant rooms from the side / back of the block preferred if achievable
- 09 Include parapet walls and/or dormer windows into the design.

Architectural typologies used within the high street should be in keeping with existing historic buildings. Ground floors will be taller to accommodate commercial and activate the street particularly in the east. In the west of the high street, residential at ground floor maybe more suitable and in keeping with site context.

Ground floors should also be differentiated from the rest of the building through material changes or detailing such as horizontal banding. The ground floor should read as a base across all frontages whilst frontages should appear to be subdivided into individual units, approximately 6m wide. Sufficient space needs to be left between the ground floor and first floor windows to comfortably install a fascia and associated signage in regards to commercial elements.

Residential entrances should be discreetly located, although easily identifiable, between shop fronts to maintain the facade rhythm. Ancillary uses such as stair cores, refuse, cycle and plant should be located to either the rear or side of the building in order to declutter the facades and maintain a clear hierarchy. This should only be implemented if the development can be serviced adequately from those locations.

Pitched roofs should be used predominantly but there are instances of hipped, and decorative blind gables. Pitches should be orientated determined by their immediate context. Parapet walls can also be used and dormer windows are encouraged, reflecting the existing architectural treatment of many of the buildings fronting onto High Street and presenting opportunities for additional architectural detailing.



Expressive architectural detailing to gable fronts facing the High Street



Timber and stone detail features added to facades



4.2 Public Realm

4.2.1 Movement

Public realm enhancements must:

- ▶ Improve pedestrian safety and create a more pedestrian and cycle friendly environment.
- ▶ Enhance the public realm in line with the historic high street character.
- ▶ Widen footways to create an accessible environment and introduce dropped kerbs.
- ▶ Introduce urban greening to improve air quality, biodiversity and reduce the urban heat island effect.

Public realm enhancements should:

- ▶ Introduce traffic management to reduce vehicular speeds and reduce through traffic by design.
- ▶ Improve the pedestrian connection between Rochester High Street and Chatham High Street through Intra.
- ▶ Better integrate the interface between Manor Road and High Street through improvements to the public realm such as extending the paved area west of Sun Pier Chambers.
- ▶ Introduce spillout space to support local businesses.

! Developers, the LPA and the council's Highways Department must collaborate to implement wholesale changes to areas beyond their ownership to maintain a holistic vision, realising the aims and ambitions as set out within this document.

National Design Guide
M1, M2, M3, P1, P3

Vision Principles



High Street concept principles

- High Street carriage / footway improvements
- Potential reinstated building line
- Retained exposed gable corners
- Junction improvement works
- Historic centre
- Links to open space

The junction at Star Hill creates a barrier to pedestrian movement and severance between Rochester High Street and Intra High Street. Collision data highlights key safety issues around this and other junctions along the street.

In other sections, the pedestrian environment is constrained by narrow footways, particularly within the historic core and to the east. Long stretches of visitor parking means that it is difficult for large vehicles to pass which gives rise to congestion, noise and poor air quality.

The High Street is also non-inclusive for people with restricted mobility which is further compounded by a lack of dropped kerbs in certain locations and no footway at street level along St Margaret's Bank.

Traffic management and enhancement of the public realm should reduce the number of vehicles moving through High Street, reduce vehicular speeds and widen footways to create a safer, more attractive environment for pedestrians and cyclists to use and spend time.

In combination with the above, the introduction of trees and rain gardens, where possible, will provide opportunities for people to connect with and enjoy nature. Trees will also provide a range of ecosystems service benefits, such as to reduce the urban heat island effect, and contribute to sustainable urban drainage, biodiversity, and air quality.

A yellow line represents high street highway improvements.
Pink shading represents junction improvement works.
A blue line represents potential reinstated building line.
Vertical lines represent historic centre.
A blue star represents retained exposed gable corners.
A green dashed line represents links to open space.



! Developers, the LPA and the council's Highways Department must collaborate to implement wholesale changes to areas beyond their ownership to maintain a holistic vision, realising the aims and ambitions as set out within this document.

National Design Guide
C1, C2, M1, M3, N1, P1, P3

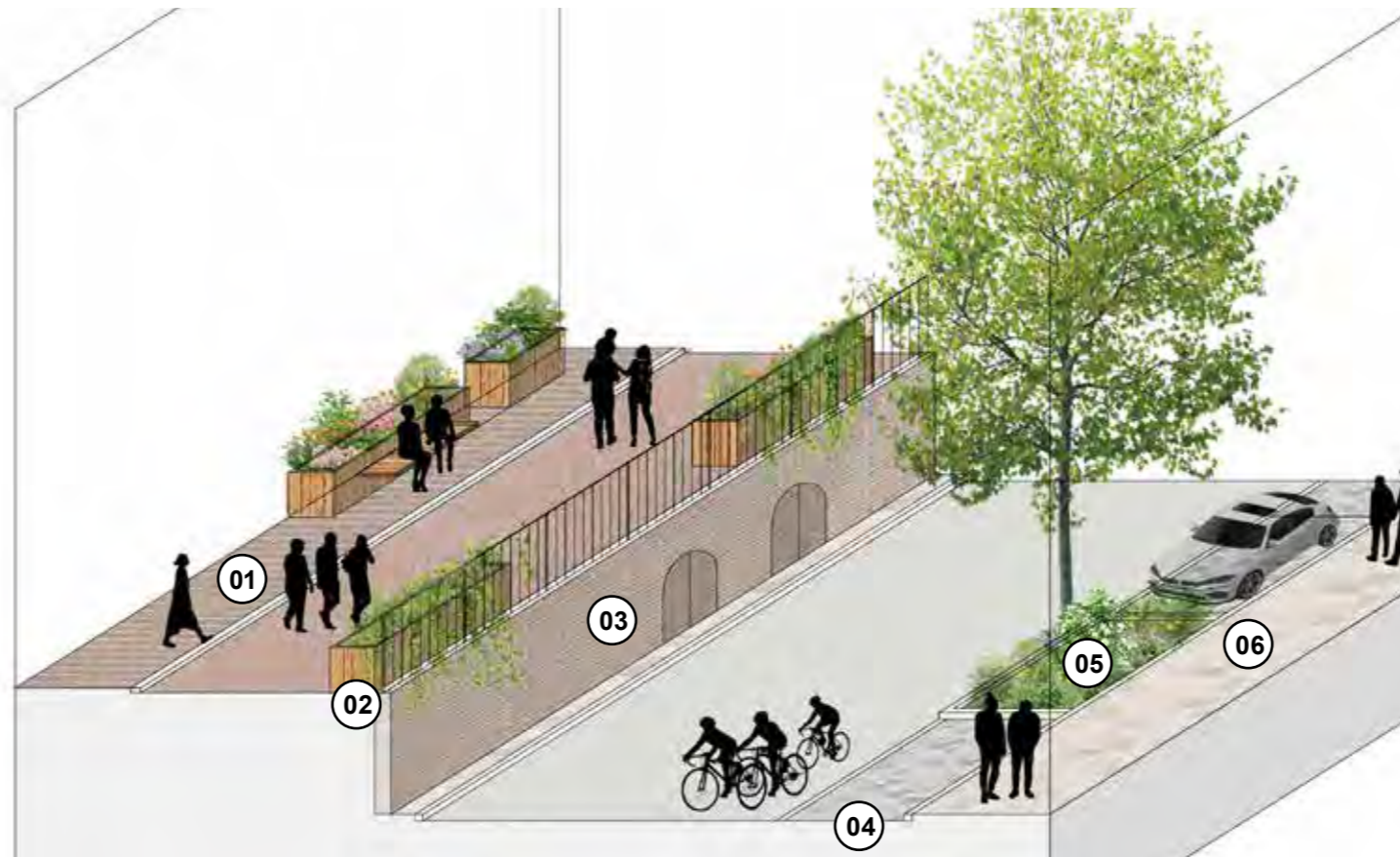
Vision Principles



4.2.2 High Street - West

Public realm enhancements should:

- ▶ Increase pedestrian accessibility and safety by creating continuous footways with a minimum width of 2m.
- ▶ Reduce carriageway widths
- ▶ Relocate parking to the northern side of the street.
- ▶ Introduce tree planting along the highway. Planting will adopt the principles of right trees in the right places for the right reasons, with space for them to grow and conditions that aid their establishment.
- ▶ Introduce sustainable drainage such as rain gardens.



Illustrative diagram showing potential highway and public realm improvements

- 01 Raised footway
- 02 Communal planters along raised footpath with climbing plants
- 03 Existing historic wall exposed
- 04 Parking spaces relocated to the north side of the street
- 05 Potential for rain gardens and tree planting between parallel parking spaces
- 06 Northern footway extended to a minimum of 2m width



High Street West - Star Hill Junction > Ship Lane

The Western section of High Street extends from Star Hill Junction to Ship Lane. On the southern side, raised footways run along St. Margaret's Banks with parking at lower level against the base of the retaining wall.

It is proposed that the parking will be relocated to the northern side of the street and interspersed with rain gardens and trees to enhance the street and improve safety. As a consequence there will be a reduction in spaces. A cohesive planting and street furniture strategy can make the two sides of the street feel more connected.



Brick wall and pedestrian crossing points along St Margaret's Bank



Brick wall and car parking arrangement along St Margaret's Bank

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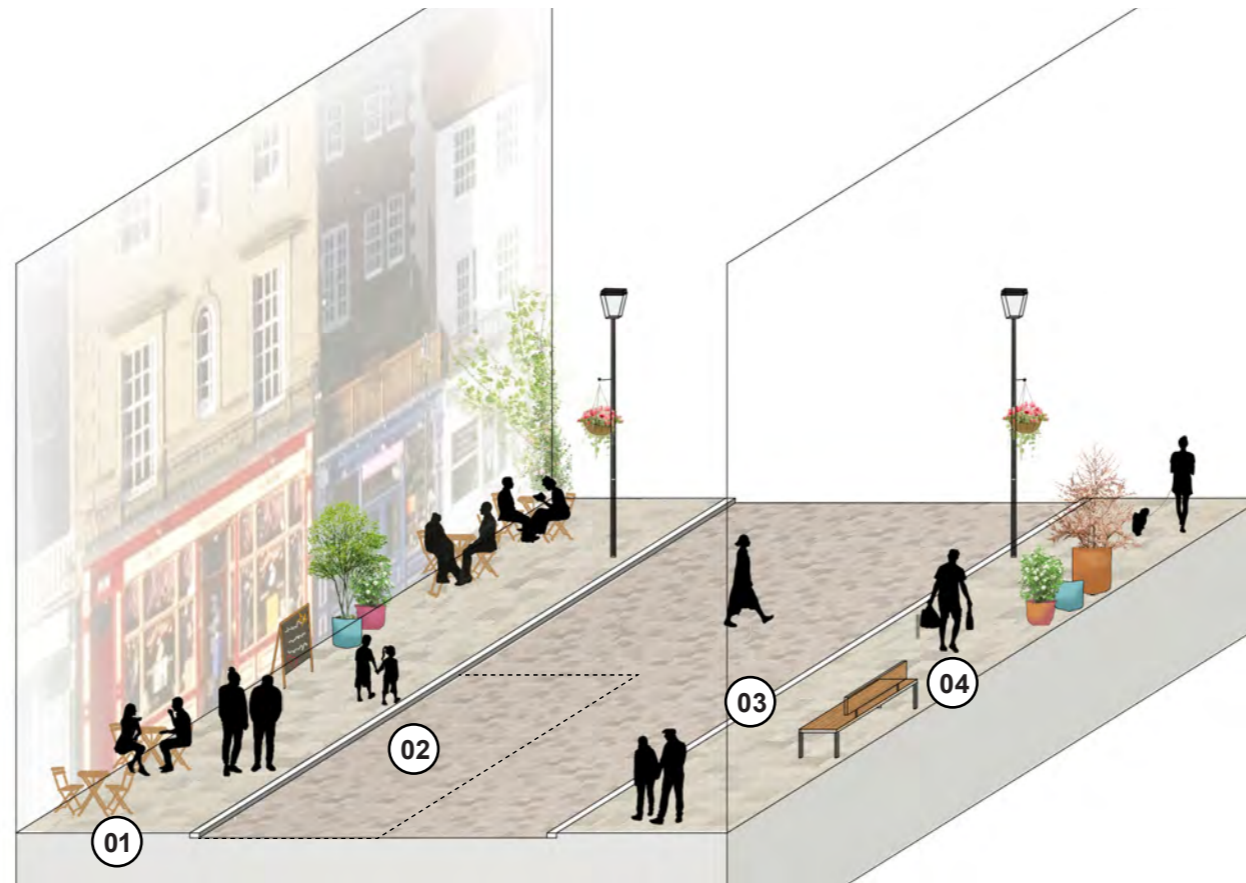
Vision Principles



4.2.3 High Street - Centre

Public realm enhancements should:

- ▶ Reduce carriageway widths on entry and exit of this section of the highway to reduce vehicular speeds
- ▶ Remove non-essential parking to enable wider footways and increased space for pedestrians.
- ▶ Provide loading bays / disabled spaces which will be time limited.
- ▶ Enhance the material treatment of the public realm / carriageway to differentiate the historic centre from elsewhere along High Street.
- ▶ Declutter and rationalise street furniture.



Illustrative diagram showing highway and public realm improvements

- 01 Potential for cafe / restaurant spill-out zones
- 02 Timed loading bays within the carriageway
- 03 Upstand kerb to discourage informal parking along the footways
- 04 Addition of street furniture to aid dwelling



High Street Centre - Ship Lane > Gundolph Road

The central section of the High Street spans from Chatham Memorial Synagogue opposite Ship Lane to St Bartholomew's Chapel at the junction with Gundolph Road. This part of the high street suffers from narrow footways, dense parking and vehicle dominance.

It will benefit most from traffic calming and provision of additional space for pedestrians, so that people are encouraged to meet, sit and spend more time within the street surrounded by history and character.

Introduction of a 20mph zone, raised tables and textural surface changes will help to slow and reduce traffic.

This area will be given a distinct aesthetic treatment to highlight the historic centre of Intra and be decluttered to provide an aesthetic cohesiveness.

Pay and display parking currently extends into the late evening, and is routinely enforced. This stretch of the High Street would benefit from being altered to cover working hours only, potentially resulting in the evening economy receiving a significant boost.



The current High Street running through the historic core

! Developers, the LPA and the council's Highways Department must collaborate to implement wholesale changes to areas beyond their ownership to maintain a holistic vision, realising the aims and ambitions as set out within this document.

National Design Guide
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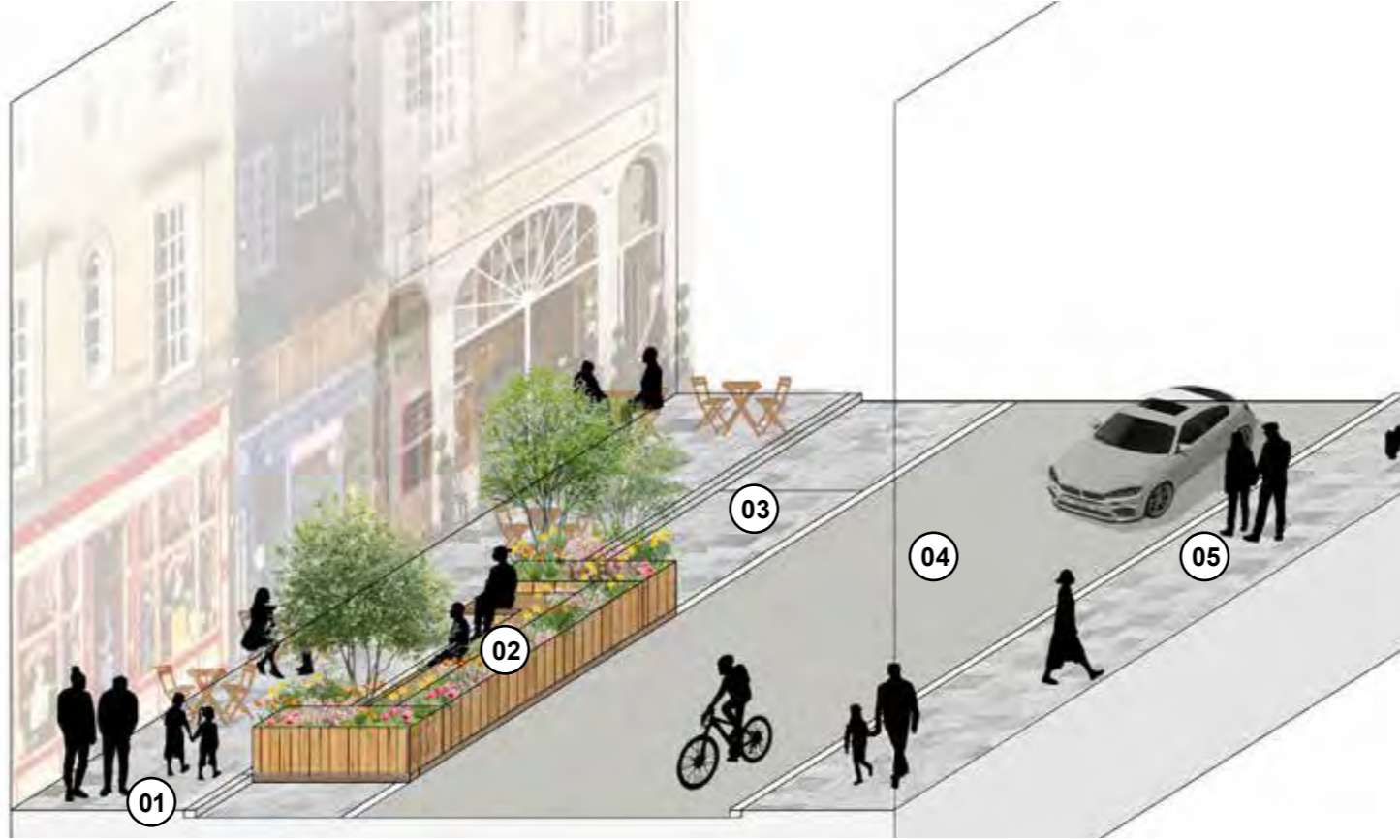
Vision Principles



4.2.4 High Street - East

Public realm enhancements should:

- ▶ Increase pedestrian accessibility and safety by creating footways with a minimum width of 2m.
- ▶ Reduce carriageway widths and provide passing spaces for vehicles.
- ▶ Utilise existing setbacks and introduce opportunities, by reconfiguring existing parking spaces, for public realm enhancements such as parklets, tree planting, seating, or sustainable urban drainage.
- ▶ Consider reducing duration of pay and display parking maximum stay to mitigate loss of bays by encouraging greater turnover.
- ▶ Declutter and rationalise street furniture.



Illustrative diagram showing highway and public realm improvements

- 01 Southern footway width retained
- 02 Potential for parking to be suspended for parklets in front of cafes
- 03 Parking / passing places
- 04 Carriageway width reduced as required
- 05 Northern footway extended to a 2m minimum width

The Eastern end of the High Street extends from St Bartholomew's Chapel to the junction with Medway Street at the entrance to Sun Pier House. The carriageway and footpaths are narrow and there are 4 hour maximum pay and display parking bays.

activating the high street and providing spaces to spend time.

The majority of the food and drink establishments are focused in this section of the high street, most of which do not have any external space.

Trees and other greening can be introduced through use of setbacks, moveable planters or within rain gardens, and parklets in between parking spaces.

By reducing the level of parking, it not only creates a cleaner, quieter and safer environment, but frees up space within the public realm for shops, cafes and restaurants to have tables and chairs outside,



High Street West - Gundolph Road > Medway Street



Existing eastern High Street with no planting or spill-out areas



4.2.5 Public Realm Materiality

Public realm enhancements should:

- ▶ Retain and re-use existing natural stone paving.
- ▶ Extend natural stone paving throughout footways.
- ▶ Implement a special carriageway surface treatment within the historic centre and demark entry / exist to this section of High Street.
- ▶ Retain and restore historic features and furniture.
- ▶ Declutter and rationalise the modern furniture, signage, and fixtures to create a palette and aesthetic that is appropriate for a historic environment that is traditional in design and colour.

Existing materiality



Yorkstone flags footpath



Pedestrian crossings in cobbles



Brick wall and metal railing



Historic bollards

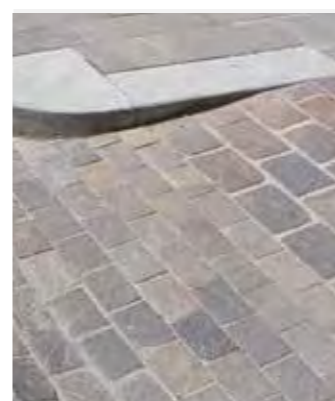
Appropriate material selection



Historic core: feature paving to carriageway



Yorkstone flag paving



Raised table on entry / exit



Pedestrian crossings in stone



Simplified complementary furniture palette

Many of the footways along High Street are composed of yorkstone with natural stone kerbs. This treatment should be implemented along the length of the High Street - new or widened footways should use natural yorkstone, reclaimed if possible.

Along the High Street's northern side, footways should be made continuous by implementing crossovers at the junctions with side streets to give pedestrian priority.

The historic core could have a special threshold and carriageway treatment that acts as traffic calming,

enhances that character of this section of the street and makes vehicles aware they are entering a pedestrian priority space.

Historic bollards and furniture should be retained. Modern furniture, signage, lighting, bins and bollards should be decluttered and rationalised to maintain a simplified material palette appropriate for a historic environment.

The materials palette should be traditional in design and colour, avoiding the use of 'modern' style materials, fixtures and furniture.



4.2.6 Open Spaces

Development must:

- ▶ Conserve and enhance existing green spaces.
- ▶ Reinstate historic open spaces that have been lost.
- ▶ Increase trees and planting to provide further opportunities for people to connect with and enjoy nature, as well as a range of ecosystems service benefits. For example, reducing the urban heat island effect, providing shade, contributing to sustainable urban drainage, and improving biodiversity and air quality.

Development should:

- ▶ Make a contribution to enhancement and creation of new open spaces along High Street, Riverfront or nearby open spaces if sufficient amenity cannot be provided in a development plot.
- ▶ Enhance planting for biodiversity in existing green spaces - implemented with local interest groups when appropriate funding is made available.
- ▶ Implement a Sustainable Urban Drainage System such as rain gardens where possible.



Identified open spaces diagram

Proposed open spaces

Whilst the High Street has a distinct character, there are few public open spaces and no street trees. Several historic gardens have been lost including: the burial ground to Bethal Chapel (currently a car park) and tree lined lane, and gardens to the Victualling Yard.

The framework identifies a number of new open spaces which shall be implemented as development comes forwards as follows:



Green shading represents proposed open spaces.

- 1. Former site of Bethel Chapel:** Reinstatement of an open space fronting on to High Street. There is also the potential to reinstate a tree lined lane within this development parcel which was historically present.
- 2. Railway Arches:** The Arches can be redeveloped for small businesses in the creative or hospitality industries, benefiting from the set back from the high street. The open space in front of the Arches provides an ideal space for spill-out in a public courtyard with some greening.
- 3. Former station site:** New public open space associated with any change of use that comes forward for the former railway station.
- 4. Dust Way Open Space and carpark:** Reconfiguration and enhancement of the open space and carpark either side of Dust Way.

Consideration is to be given to how the green spaces can be funded and implemented. Where sufficient amenity cannot be provided within a development plot, a contribution may be made to enhancement of / creation of new open spaces elsewhere to provide enhanced amenity within the requisite walking distances for residents.

Smaller interventions such as enhanced planting in existing green spaces could be implemented in conjunction with local interest groups.



LION
BREWERY

Chapter **5.0**
Wharves & Lanes

5.1 Built Form

5.1.1 Street Character

Development must:

- ▶ Reinforce enclosure of the lanes
- ▶ Arrange buildings in a way that creates a yard / courtyard typology.

Development should:

- ▶ Embrace and enhance the shared yard as a typology and as a shared amenity space.



Long, narrow, timber-clad warehouse typologies are prevalent



Variation in heights and roofscapes along the lanes



Pitches, storey height, colours and materials contrast



Old painted signs and advertising are a character of the lanes



Industrial materials such as corrugates steel and railings

The Wharves and Lanes are architecturally rich in character and diverse in materiality. Their arrangement is indicative of piecemeal and organic delivery over time.

Along the lanes, a loss of buildings has resulted in piecemeal frontage and erosion of the historic building line. New development should seek to reinforce enclosure of these routes and framing of views to the river.

As a result of their former use, the arrangement does not conform with current privacy or overlooking standards. There are no privacy strips or defensible space, doors and windows open directly onto shared amenity spaces which are mainly in the form of the yards.

New buildings should seek to capture some of this quality, build hard up against the street edge, create compact and creative infill solutions and embrace and enhance the yard typology. Consideration should be given to installing green planted elements to create courtyard spaces.

Conversions of existing warehouse buildings from industrial to residential use is encouraged if it helps to preserve their historic character. Where possible, such conversions should seek to retain and restore historic features with special attention paid to external materials, appearance and the building's contribution to the streetscene.





5.1.2 Scale & Massing

Development must:

- ▶ Conserve, retain, restore and sensitively adapt historic buildings.
- ▶ Respond in scale, massing, height and configuration to the existing local context, referencing warehouse and workshop typologies and historic layouts.
- ▶ Ensure roofs are predominantly pitched or M-shaped, set at contrasting angles.

Development should:

- ▶ Rooflights are encouraged, affording light and views without compromising privacy or overlooking within such a tight urban space.



Sketch of new development within the lanes.



Integrating Old and New

New development must respond in scale, massing height and configuration to the historic precedent to create a response that is contextual, place specific and enhances the special qualities of the Intra. The public realm treatment and articulation of new buildings shall respect the setting of listed and locally significant buildings to integrate new and old.

Where new development is proposed on sites that include historic buildings, there will be a presumption in favour of their retention, restoration and sensitive adaptation.

New buildings shall of high architectural quality and reference the form, detailing and materiality of historic buildings in the area.

Massing and articulation

Existing warehouse and workshop typologies shall be referenced in new development – with long narrow buildings running perpendicular to the waterfront and buildings configured to create enclosed courtyard spaces in the larger wharf areas.

Roofscape

The roofscape of the existing wharf areas contributes to the specific character of the area – the majority being pitched and M-shaped pitched, set at contrasting angles.

Their scale, composition and form requires careful consideration so to the current interest and variety in the skyline. Rooflights are encouraged - they are typical of warehouse typologies, allow light into deep plans and afford light and views without compromising privacy or overlooking.



Development must:

- ▶ Create heights between 1-4 storeys and incorporate a range of heights within a single development. Any proposed buildings which are taller than specified must be of exceptional design quality, established through a rigorous design review process, and protect / enhance the heritage context.
- ▶ Not adversely impact strategic long-distance views - please refer to the CAAMP for specific views and corridors.
- ▶ Create breaks in the building in larger development parcels to retain strategic views across the water. Preserve views of the river from the high street.
- ▶ Maintain privacy when buildings are in close proximity within the Wharves and Lanes area.

Development should:

- ▶ Discuss heights and offset distances in further detail with the Local Planning Authority to ensure the best outcome.



Larger storey heights and pitches are common in the lanes

Heights

Building heights within the wharf areas must relate to their immediate context. The built form across much of the area varies from 1-4 storeys and new development shall be restricted to these parameters.

Developments must maintain the varied roofscape and ensure that there are a range of heights designed into any single development - uniform height across a plot will not be accepted.

Note: Any proposed buildings which are taller than specified in the SPD must be of exceptional design quality, as established through a rigorous



Variety of heights is a character trait of the wharves and lanes



Mix of heights, roofscapes and materials within the lanes

design review process, and protect and enhance the surrounding heritage context.

Views

Development must not adversely impact strategic long-distance views from Jacksons Field and Victoria Gardens and Chatham Waterfront and to Rochester Cathedral and Castle by interrupting the existing sky or roofline.

Breaks in the building massing / line must be incorporated into larger development parcels to retain strategic views across the water and it is imperative that any new development must preserve



Architecture used to create vistas and views along the lanes



Narrow laneways which mitigate overlooking policies

views of the river from the high street.

Overlooking

Historically, the urban density and grain was greater than it is today. To achieve this character, dispensation shall be given to building proximities allowing for closer and tighter arrangements within the Wharves and Lanes area.

The extent to which this distance can be decreased will need to be discussed in further detail with the Local Planning Authority and will be decided on a case-by-case basis.



5.1.3 Access & Servicing

Development must:

- ▶ On-street parking within the wharf areas shall be restricted to commercial, visitor, car club and blue badge only.
- ▶ Any residential associated parking provided must be off-street and within the footprint of the block within garages or undercrofts.
- ▶ Ensure that active uses and access points are located along key frontages, with ancillary uses such as cycle parking and refuse located towards the back of buildings.
- ▶ Ensure that access is available for both emergency vehicles and refuse vehicles and that they may enter the lanes unimpeded.

Development should:

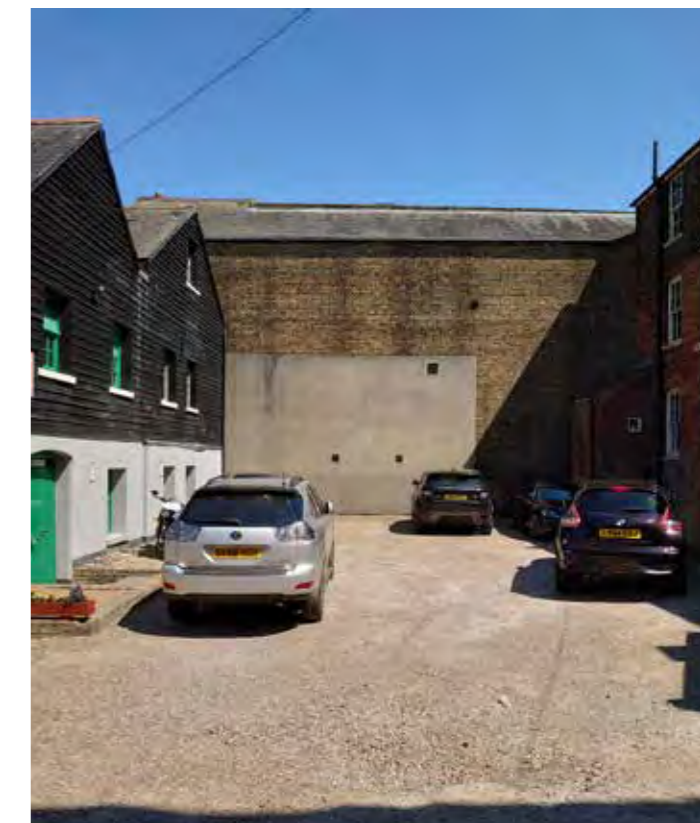
- ▶ Garages may be incorporated into the ground floor of individual houses.
- ▶ Consider a commercial parking strategy that enables servicing of businesses whilst providing a pedestrian focused public realm.



Examples of fencing / restricted access along the Lanes



Ground floor windows and entrances along the Lanes



Existing large yard areas currently used for parking

Ground Floor Uses

Due to flooding constraints, it is unlikely that habitable rooms will be located at ground floor along the Wharves and Lanes.

Developers shall ensure that active uses are located along key frontages, with ancillary uses such as cycle parking and refuse located towards the back of buildings. Please see section 2 for more information about flooding.

Parking and Servicing

There is special dispensation within Conservation Areas to adjust parking ratios from Medway Council Residential Parking Standards and a lower provision can be justified in an urban area with good links to transport and where day-to-day facilities are within easy walking distance. Transport and parking provision in new developments should be discussed with the Council at an early stage.

Intra benefits from a high degree of accessibility with access to two rail stations and a bus station and access to the high street which contains local amenities. Garages may be incorporated into the

ground floor for individual houses. The infrastructure is in place and amenities in close proximity to support a modal shift.

In order to create an attractive public realm that supports outdoor commercial and leisure uses, parking within the wharf areas shall be restricted to commercial, visitor, car club and blue badge only.

Access must be created for emergency and refuse vehicles. It is expected that these vehicles will reverse back out so turning heads will not be necessary.



Developers must undertake an elevation and materiality study of the surrounding streetscene & architectural character to help the planning authority to understand how appropriately a proposal responds contextually.

National Design Guide C1, C2, I1, I2, I3, H1

Vision Principles



5.1.4 Architecture

Development must:

- 01 Predominantly use pitched /multi-pitched roofs and rooflights to combat overlooking.
- 02 Utilise warehouse and workshop architectural typologies that reference the form, detailing and materiality of historic buildings in the area.
- 03 Use commercial space to activate lanes and interact with courtyard spaces.
- 04 Provide active frontage at ground floor that has a larger floor-to-ceiling height to accommodate non-residential uses.
- 05 Projecting balconies can be used within courtyards or overlooking the river but not within the lanes themselves.
- 06 Not install defensible space / privacy strips along the lanes.

Development should:

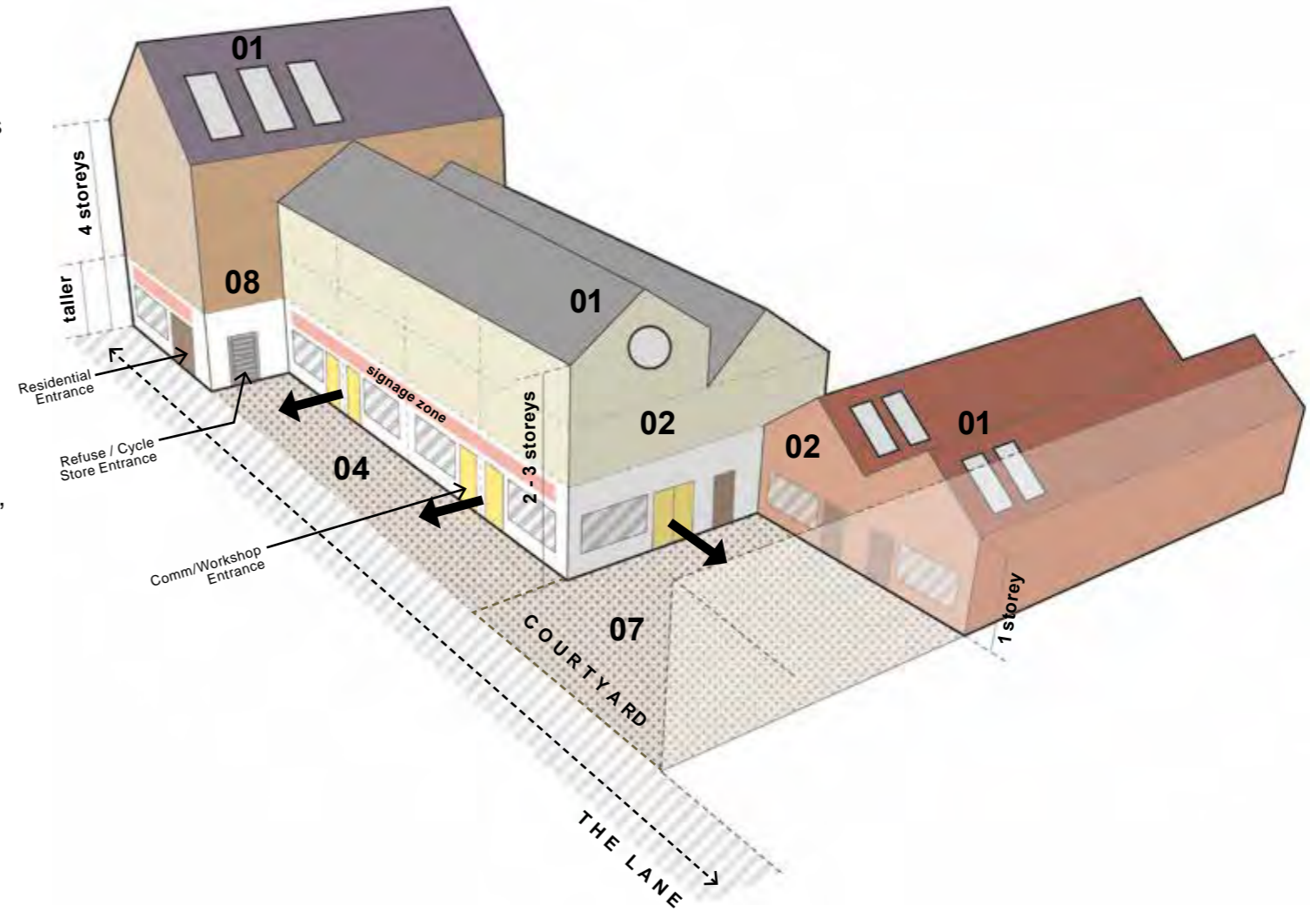
- 07 Step the building line to introduce wider spaces where appropriate.
- 08 Place the core at the rear of layout to maximise frontage onto the lanes, subject to servicing and access.

Architectural typologies used within the Wharves and Lanes should remain in keeping with the existing buildings. Long, low warehouse typology buildings tightly enclosing the lanes but arranged in ways to create yard spaces between the lanes is preferred as a solution.

Communal residential entrances should be easily identifiable and accessed directly from the street. Where possible, individual private entrances to ground floor uses should be used to activate the streetscene. Ancillary uses such as stair cores, refuse, cycle and plant should be located to either the rear or side of the building but only if this can be serviced adequately from those locations. If not, a street focused solution will need to be found that works with the rhythm of the elevations.

Workshop typologies could be introduced along the ground floor, as flood risk could prevent incorporating habitable residential rooms in this location. Workshops would allow for creatives to set up studios with the focus on manufacture and production and perhaps linked with apartments above creating live/work opportunities.

This typology would work nicely along the lanes and even around the ground floor of the yard spaces where interaction between different creatives and residents could take place.



Raised ground floor covered spill out space along the lanes



Variety of material, roofscapes and colours within a single lane



5.2 The Riverfront

5.2.1 Riverside Character



Development must:

- ▶ Conform and adhere to the design principles set out in the lanes (See - 5.1.1) except for the below:
- ▶ Maximise views for residents using projecting balconies and a high level of fenestration.
- ▶ Locate ancillary / commercial uses on ground floor and provide spill-outs space adjacent to the riverwalk if required.
- ▶ Ensure that if a fifth storey is used it must form part of the roofscape and be expressed as either a mansard or dormer / rooflight solution.

Development should:

- ▶ Waterfront developments should generally be between two and four storeys although occasionally a fifth storey within the roof space may be acceptable.
- ▶ Actively try to avoid any blank frontages along the river frontage. If it does become unavoidable then the design must ensure that 'blank' facade employs additional architectural details to maintain visual interest along the riverside walk.
- ▶ Vary height to avoid large areas of homogeneous roofscape.



Sketch of revitalised riverfront development

Developments along the riverfront must follow the same principles in scale, massing and appearance as within the Wharves and Lanes but a few subtle differences that have been made to exploit views across the river.

Views

Buildings must be orientated to maximise views over the water with river-facing frontages forming the primary facade. Balconies - especially projecting balconies - full height glazing and roof terraces should all be explored as a means to make the most of the riverside location.

Flood Risk

Ground floors may not be able to accommodate habitable rooms due to the risk of flooding. Alternative ground floor uses such as communal lobbies and non-residential land uses can absorb this risk and provide active frontage to waterfront locations. A lack of ground floor habitable rooms render defensible space unnecessary however some commercial spill-out zone will be required for the non-residential uses.

Bicycle / refuse storage and plant must try and avoid being located along the waterfront but if unavoidable, then any blank facades must have

some architectural detailing such as patterned brick or rainscreen cladding to provide visual interest along the riverfront. All residential ancillary uses must be accessible from the lanes as there can be no vehicular access, servicing or delivery from the riverside.

Height & Roofscapes

Roofscapes should consist of pitches and M-pitches and heights can raise to 5 storeys along the riverfront but the top storey must be embedded within the roofscape using either mansards or dormer solutions. Variations in height are strongly encouraged to avoid long stretches of homogeneous facade / elevation.

! Developers must undertake an elevation and materiality study of the surrounding streetscene & architectural character to help the planning authority to understand how appropriately a proposal responds contextually.

National Design Guide
C1, C2, I1, I2, I3, H1

Vision Principles



5.2.2 Architecture

Development must:

- 01** Predominantly use pitched roofs that run perpendicular to the river.
- 02** Explore potential of development at the fifth storey to be designed as part of the roofscape i.e dormer or mansard solution. Any proposed buildings which are taller than specified must be of exceptional design quality, established through a rigorous design review process, and protect / enhance the heritage context.
- 03** Utilise balconies, projecting balconies, full height glazing and high levels of fenestration along the riverfront.
- 04** Locate commercial or ancillary uses on the ground floor, avoiding blank facades where possible but providing extra visual detail if unavoidable and focus on providing spill-out space on the riverside.
- 05** Ensure flood defences are integrated into building designs where deemed necessary through discussions with the Environment Agency.

Development should

- 06** Consider different variations in balcony design and arrangement to provide richness to the riverside elevations.

This building typology has been designed to be utilised along the riverfront of Intra, responding to the historic context of the former warehouse buildings which were tall, narrow buildings with openings to accept crane operated delivery of goods.

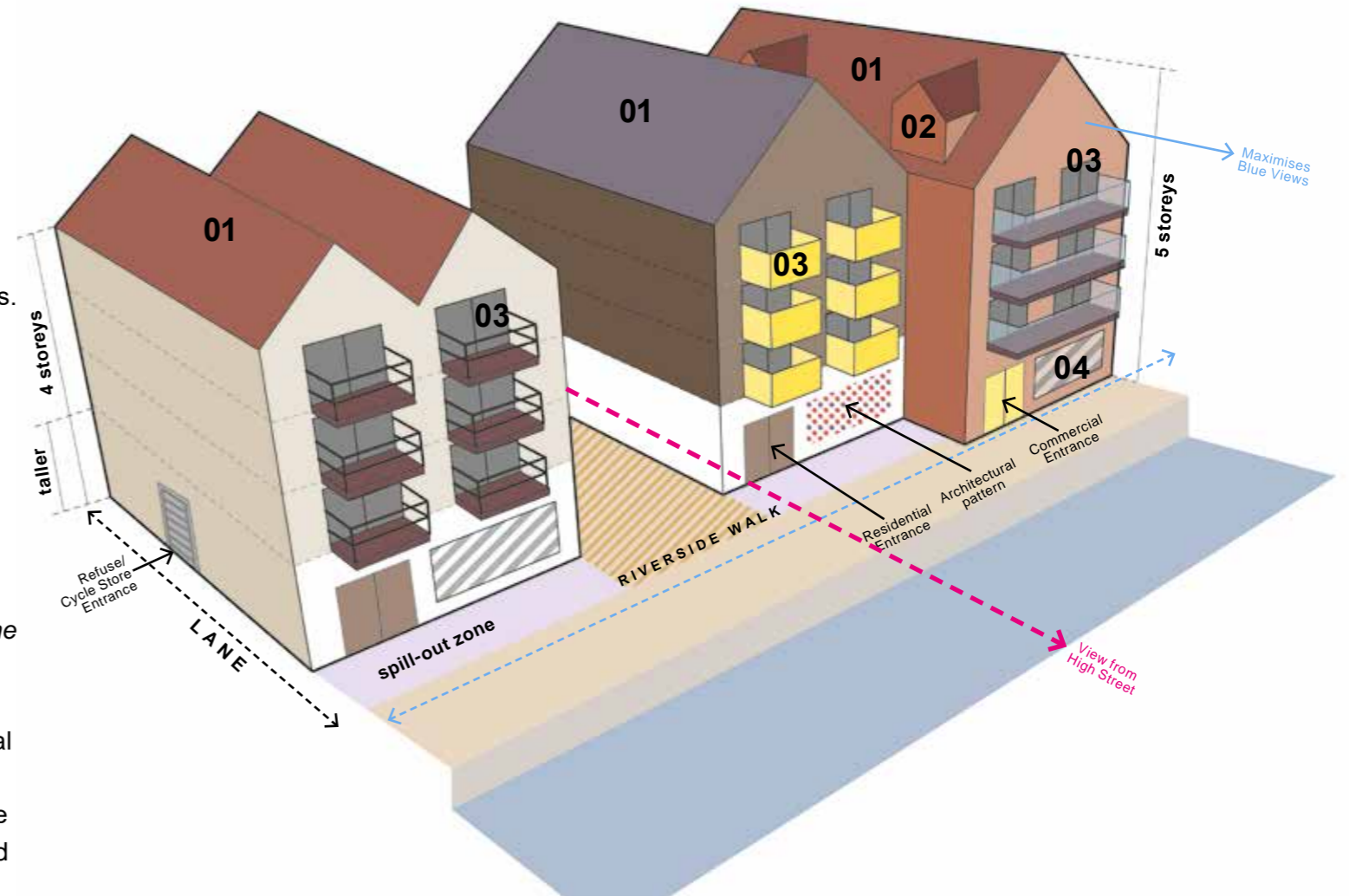
Heights, massing, and roof lines all respond to the former buildings to ensure the history of Intra is intact. Building height can vary between 4-5 storeys. Height variance is highly encouraged to avoid a homogeneous appearance. If a fifth storey is used it must appear to be part of the roofscape as either dormer windows or mansard design solutions.

Note: Any proposed buildings which are taller than specified in the SPD must be of exceptional design quality, as established through a rigorous design review process, and protect and enhance the surrounding heritage context.

Balconies, full height glazing, bay windows and dual aspect are all encouraged to be used to maximum effect on the river fronting facades allowing for wide blue views for residents. Projecting balconies would work especially well in this location.

The ground floor has restricted use due to flooding issues. Any commercial space must provide a positive interface with the public realm. Bins and bicycle storage must be accessible from the adjacent lanes.

Flood defences may need to be installed within the buildings themselves particularly across doors or any other low openings across the ground floor and will need to be fully integrated into design proposals.



Variety of brick buildings with pitched roofscapes



Ground floor used for ancillary uses in anticipation of flooding



5.3 Public Realm

5.3.1 Movement & Connections

Development must:

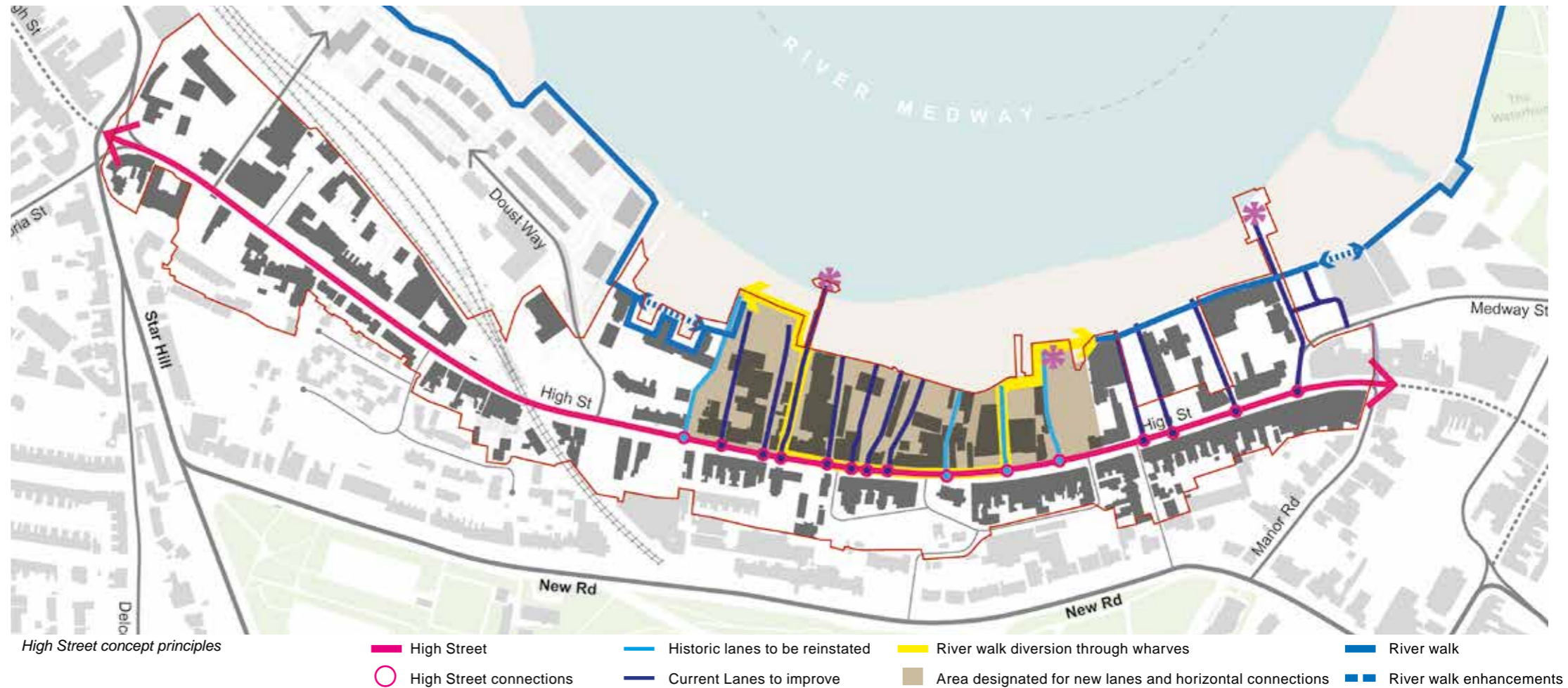
- ▶ Enhance the public realm in line with the historic wharf character and identity.
- ▶ Encourage increased public access.
- ▶ Reinstate lost historic lanes, improving connections and general permeability.

Development should:

- ▶ Promote additional east-west connections between yard spaces.

The lanes are a fundamental part of Intra's character, linking the High Street with the River Medway and framing views of the river. Enclosure of existing lanes should be reinforced, and lost lanes reinstated when new development come forward.

The potential to introduce east-west routes, linking the lanes through the different wharfs should be considered as part of the design of development plots. This would be secondary to the lanes and courtyards spaces in hierarchy.



High Street concept principles

Access is an issue as most of the lanes are in private ownership and the public restricted by gates and fences. There is an aspiration to remove these barriers and allow increased access and movement through the Wharves and Lanes area.

Any design that includes vehicular access, even if just for service and delivery, needs to be mindful of the limited space. Access must be prioritised for emergency vehicles and refuse vehicles.

Lining and heavily delineated parking bays are to be avoided in favour of multi-functional public realm and designed traffic management.

- A pink line represents high street.
- A pink circle represents high street connections.
- A light blue line represents historic lanes to be reinstated.
- A dark blue line represents current lanes to improve.
- A yellow line represents river walk diversion through wharves.
- Brown shading represents area designated for new lanes and horizontal connections.
- A blue line represents river walk.
- A blue dashed line represents river walk enhancements.





5.3.2 Courtyard Spaces

Development must:

- ▶ Create modern interpretations of historic yards and spaces in between within the Wharves and Lanes.
- ▶ Provide residential amenity.
- ▶ Ensure that vehicular movements are carefully integrated in a people orientated space.
- ▶ Provide well-designed courtyard spaces that support ground floor uses and allow both residents and ground floor business to co-exist.

Development should:

- ▶ Courtyard spaces should be well overlooked with direct entrances to residential properties / shared cores.
- ▶ Introduce planting and greening that is sympathetic to the historic environment.



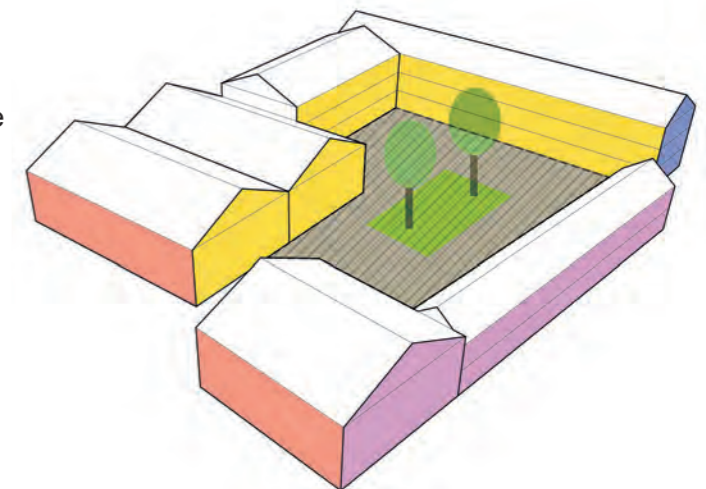
Illustrative sketch showing how courtyards could become a part of the lanes

The configuration of the courtyards will evoke the figure ground and character of the historic yards, providing sheltered spaces with a sense of enclosure.

They will provide a mix of residential amenity as well as supporting ground floor commercial uses and incorporation of opportunities for people to spend time and use these spaces should be encouraged.

Vehicular access, parking and servicing should be kept to a minimum and carefully considered so that the courtyards are people orientated spaces.

The courtyards provide an opportunity for greening and can be part of a wider SUDS strategy and should contribute to an improved ecological network. Opportunities exist to create a mix of vibrant community gardens, growing areas as well as communal yard type spaces.





5.3.3 Public Realm Materiality

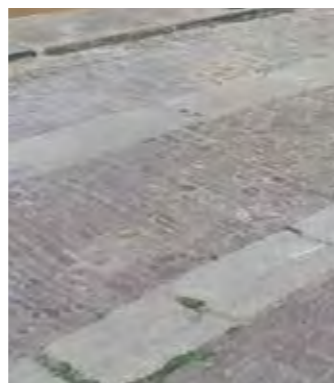
Development must:

- ▶ Retain historic paving and artefacts and make good where necessary with suitable matching materials.
- ▶ Ensure new materials and furniture complement the historic and industrial heritage.

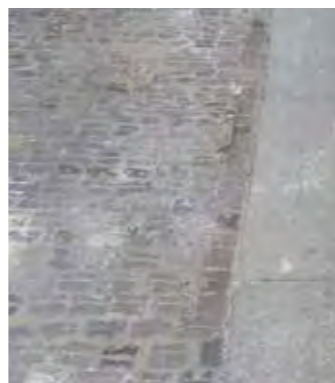
Development should:

- ▶ Show a preference towards low carbon and sustainably produced materials.

Existing materiality



Porphyry/granite setts and cobbles with granite kerbstones



Buff brick walls

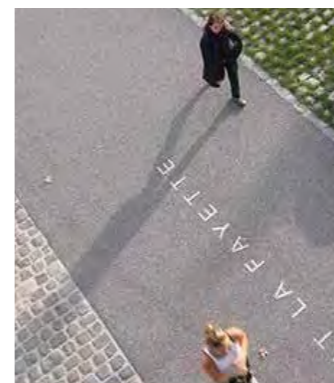


In - situ concrete

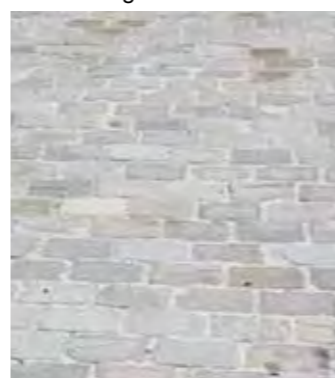
Appropriate material selection



Complement contemporary and heritage elements



Light coloured tarmac with feature wayfinding/branding



Natural stone Granite or porphyry setts



In-situ concrete with exposed aggregate



Facilitate urban greening to populate paved spaces / courtyard



The wharves have evolved over time to meet the needs of maritime industry with a variety of paving including cobbles, in-situ concrete and large format flagstones.

Some of the lanes have remnants of historic materials partially covered by tarmac. These should be retained and restored. Where original paving cannot be retained in-situ, it should be reclaimed and re-used.

New paving should complement the historic and industrial heritage. For example, natural stone cobbles/setts or with a contemporary industrial

language such as in-situ concrete with exposed aggregate. Small unit concrete paving or stainless steel type furniture is not considered appropriate for this context.

The courtyards have a more domestic character and can transition from the primarily hard wharves to green spaces in a manner appropriate to their historic setting. For example, as if nature has reclaimed the space.

Free standing planters and movable furniture for use by businesses and residents are encouraged to help provide interest without adversely impacting on the historic character.

The design of the public realm as a whole must also consider safety and security. This often requires consideration not only of materiality, but also of access to and the type of spaces, lighting, and visibility or being appropriately overlooked.

This should help ensure that spaces and paths feel safe and secure including from the perspective of women.

Wayfinding and signage also need to be taken into account.



Chapter
River Edge 6.0

6.1 Public Realm

6.1.1 Movement & Connections

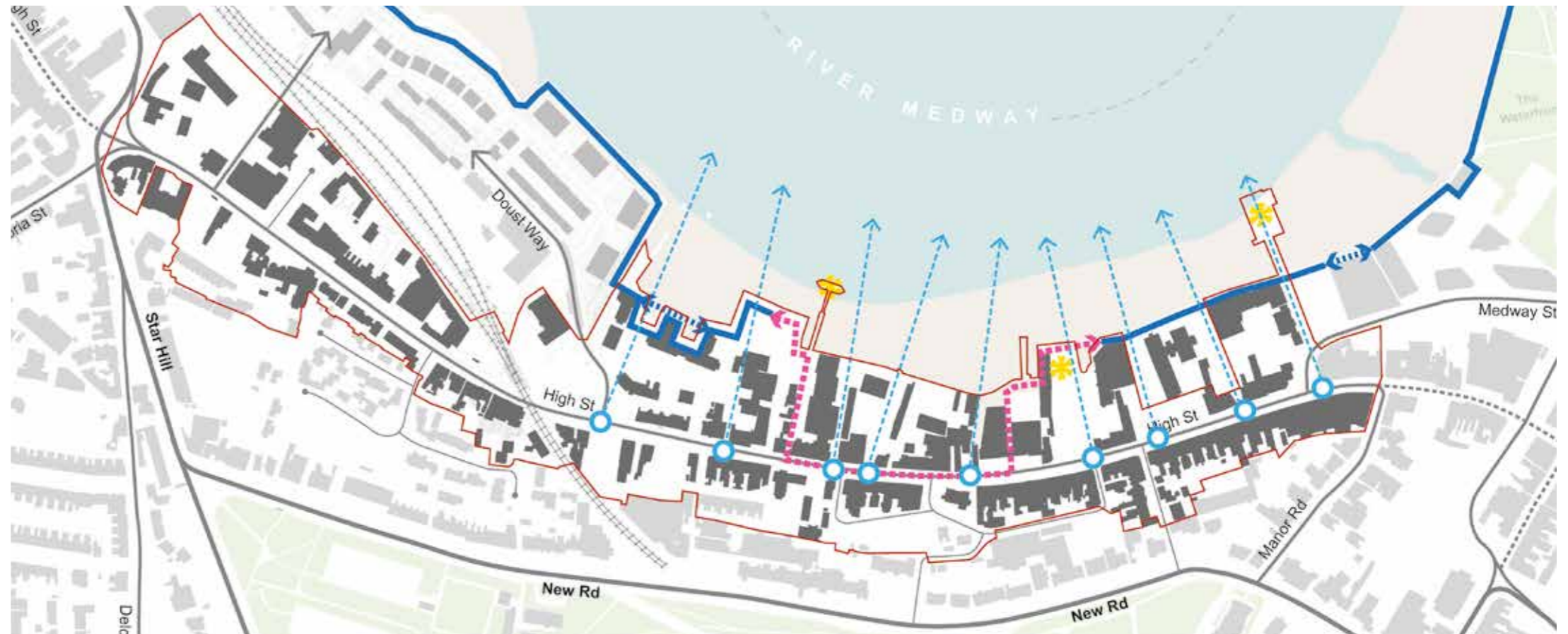


Development must:

- ▶ Provide the infrastructure to facilitate increased use of the waterfront and river.
- ▶ Support commercial uses, tourism, residential moorings and other facilities to support river use.
- ▶ Increase public access to the waterfront.
- ▶ Create new sections of riverwalk to connect Chatham and Rochester through Intra.

Development should:

- ▶ Create waterside destinations and facilitate year-round use to increase use of the riverfront.



River Edge design principles

- Existing riverwalk
- - - New riverwalk sections
- - - Riverwalk diversion
- River views from High Street along lanes
- ★ River activity focal points

Use of River Medway has been a key driver in the development of Intra over time and still evokes a strong sense of character.

With investment, the riverfront can support increased use of the river for transporting goods, entertainment, moorings, passenger tours and trips as well as associated riverside activities such as dining overlooking the water.

The framework sets out a number of focal and destination points along this stretch of the River

Medway that shall be implemented by applicants as individual developments come forwards. This should have the positive effect of drawing people towards the river edge, especially in conjunction with development within the wharves.

Although little can be done about existing stretches of riverwalk which were not built up to the boundary of all sites, development should use reasonable endeavours to try and connect to existing or emerging parts of the river walk.

The riverside walk should be extended through Intra to connect Rochester to Chatham. Improving the public realm of the river edge will make it more appealing for pedestrians and cyclists. There is also a chance to enhance biodiversity and greening within the public realm and aquatic environment.

A blue line represents existing riverwalk.
A blue dashed line represents new riverwalk sections.
A pink dashed line represents riverwalk diversion.
A blue circle represents river views on high street along lanes.
A yellow star represents river activity focal points.



6.1.2 Piers & Jetties

Development must:

- ▶ Ensure existing piers and jetties are retained, restored and made safe.
- ▶ Retain a mix of publicly accessible piers as well some that are private residential space.
- ▶ Seek technical guidance from Engineers, the Environment Agency, and Peel Ports in relation to any proposed pier / jetty.

Development should:

- ▶ Bring into use and increase public access to the piers and jetties for both pedestrians as well as leisure vessels.
- ▶ Enhance security for residential moorings
- ▶ Provide ancillary infrastructure along the water's edge to support use of the piers and jetties.
- ▶ Improving mooring facilities.
- ▶ Increase the facilities at Sun Pier. Consideration should be given to reinstating a canopy / shelter and upgrading mooring facilities.



Sun Pier PHOTO CREDIT: Rikard Osterlund

Sun Pier was constructed in the 1860s but has been changed and updated over the years. In the 1970s, an ornate roof structure, designed by Sir Joseph Bazalgette, was lost as a result of fire in the 1970s. It was built and owned by the Best Brewery (who also owned the Sun Hotel at the end of the Pier).

The pier currently does not fulfil its potential. The pier is large, open and has potential to act as a major focal point and destination. Significant enhancement should be provided including improved services and activities, including the potential to increase mooring

facilities as well as exploring reinstating the canopy structure (subject to feasibility).

There are a few other existing jetties and piers along Intra. However, they are currently under private ownership, and are not publicly accessible. Opening these up for wider use would encourage new water-based activities but significant investment is needed to make them safe for public use.



Existing small timber jetties leading to boat activities



Some existing piers are currently inaccessible to the public with restricted access



As the river is tidal, any new structures extending into the river will be subject to agreement with the Environment Agency and Peel Ports. New structures would also need to be compatible with the flood defence strategy along the river edge.

The jetties and piers would also need to be fully accessible for all users and management of access at night-time needs to be considered in terms of health and safety and antisocial behaviour.

6.2 The Riverwalk

6.2.1 Determining a Route

Development must:

- ▶ Complete sections of the riverwalk as plots come forwards.
- ▶ Where necessary, ensure that proposals are integrated with flood defences to provide an attractive, safe and useable solution.

Development should:

- ▶ Ensure that the riverwalk is promoted through wayfinding.
- ▶ Liaise with other landholders so improvements are considered holistically and can be determined as such.



Riverwalk - Doust Way > Sun Pier



Reinforcing historic frontage diagram

- Existing riverside walk route
- - - Potential new river walk additions / diversions
- - - Private houseboats - potential for capacity increase

The waterfront is a major asset, providing long views across the water, fresh air, large skies and a connection to nature. The existing river walk extends from Rochester Riverside to Doust Way in the west and from Chatham Waterfront to Empire Reach in the east. It is interrupted in the central section of Intra where buildings extend to the river edge in some places and there is no public access in others.

It is an aspiration to provide a continuous route all the way from Rochester Riverside through Intra to Chatham waterfront. Developers shall be required to implement these new sections of the riverwalk to complete the route as application sites come forward.

The new proposed route would retain the privacy of existing residential moorings by directing people of existing residential moorings by directing people through the lanes and into the historic section of the high street rather than along the waterfront in this central area. This will enable people to experience differing character areas and increase footfall through the wharfs and high street and should be supported by a robust wayfinding strategy.

It is envisioned that the new parts of the riverwalk may extend over the River Medway. Here, they could be constructed using decks, floating pontoons or boardwalks. Consent must be sought from the Environment Agency, Peel Ports and any other relevant authority.

Vision Principles





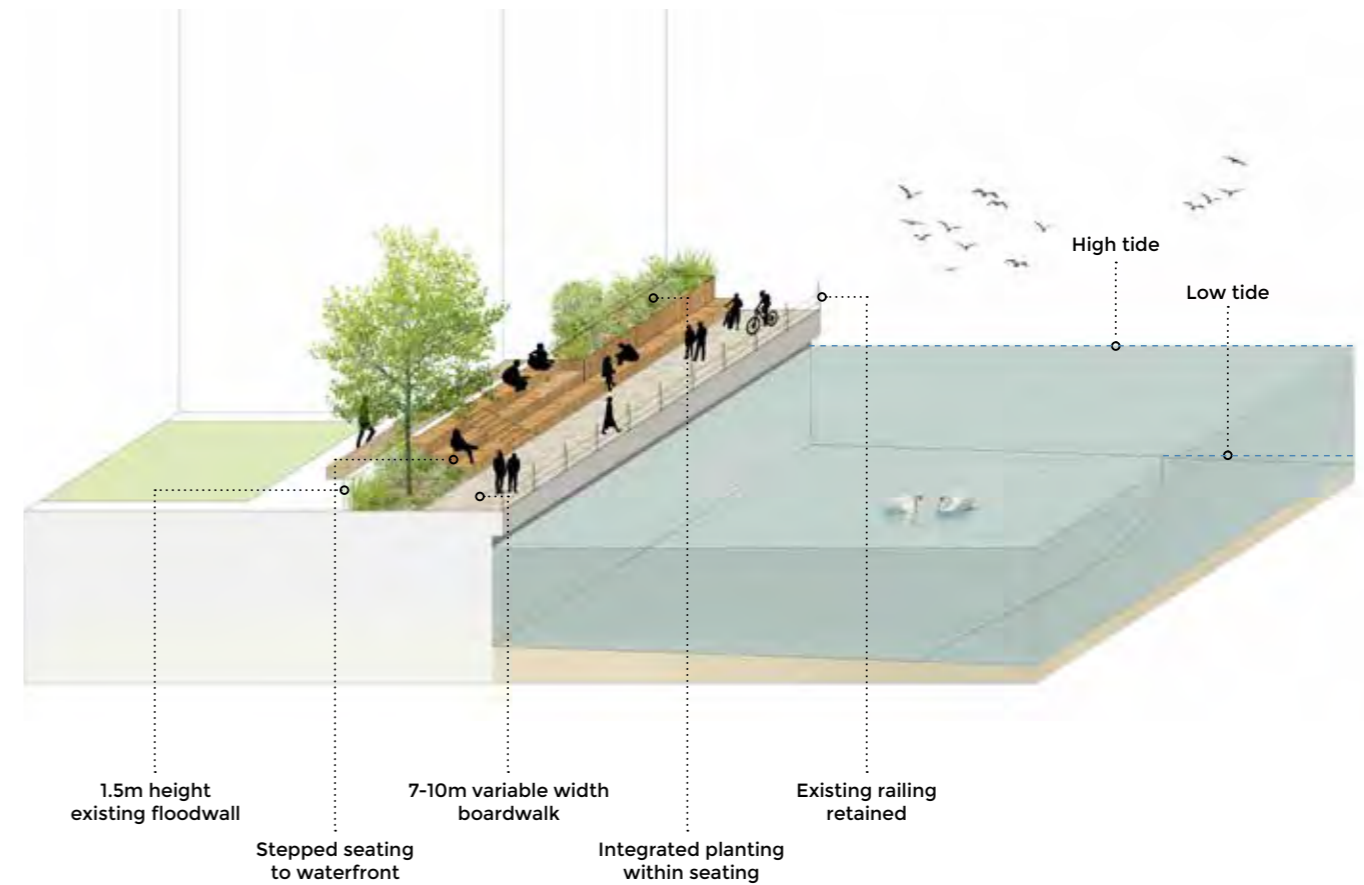
6.2.2 Riverwalk Enhancements

Development should:

- ▶ Improve connections between the riverwalk and High Street through the Wharves and Lanes.
- ▶ Improve the character of the riverwalk to become more inviting, varied and better used.
- ▶ Ensure all interventions are considered in the context of flood mitigation.
- ▶ Engage in consultation with the Environment Agency on any proposals that may alter the riverside.
- ▶ Improve biodiversity and ecological connections.



Existing riverside walk with hard materials and no greenery



Riverwalk enhancements from Sun Pier to Grays Garage site

This section of the riverfront is poorly connected to the wharves, lanes and high street and is underused. The north facing aspect, concrete flood wall and expanse of concrete paving can appear austere. The space should become more inviting, varied and better integrated into the wider movement network.

Creative interventions should be employed to overcome physical barriers and improve biodiversity as part of wider green infrastructure enhancements.

For example, the physical constraint of the concrete flood wall could be overcome by installing stepped

planters/seating on either side to allow a pedestrian connection up and over, doubling up as a space for people to meet and sit overlooking the river.

Floating planted islands could be implemented along the water's edge, and relics such as abandoned boats could be planted.

All interventions should be considered in the context of flood mitigation and ecological enhancement and relevant permissions sought.



Chapter **7.0**
Delivery & Implementation

7.1 Delivery

7.1.1 Delivery Strategy

in order to achieve the heritage-led regeneration of Intra, a comprehensive and area-based approach will be required to bring forward the design principles and objectives set out in this development framework as part of the vision which will shape Intra's future.

Having a joined-up approach is critical to achieving this, as it will allow stakeholders to work together amongst the constraints and opportunities in Intra to deliver the best and most equitable outcomes.

The next step in achieving this will be for an action plan to be produced in collaboration between the Council, the community, landowners and developers, and other interested stakeholders.

7.1.2 Landowner Collaboration

Planning applications must explain how landowners have collaborated to achieve the aims of the framework.

There may be opportunities to collaborate with other stakeholders involved in projects in close proximity to Intra. The benefits of these partnerships should be explored.

The framework has been designed to take account of land ownership boundaries, an important characteristic of Intra. Development by plot or across small groups of plots has taken place successively over many generations creating a varied and interesting urban form.

Development will be supported on assembled or individual sites within the constraints of the Conservation Area. Public bodies will be required to work with private landowners to bring forward infrastructure.

Collaboration with public bodies can generate strong private and public partnerships which combine development experience and expertise with the ability to apply for funding to unlock viability obstacles.

Applicants will be required to demonstrate how their proposal fits in with the framework structure and how they will facilitate bringing forward future phases even if owned or delivered by others to achieve the overall aspirations set out within this document.

Development plots should not compromise the ability of adjacent land to come forward.

Planning applications must be accompanied by Statements of Community Involvement (SCI) which demonstrate how landowners have :

- Approached adjacent landowners and delivery partners to assist with land assembly and delivery, and
- Prepared co-ordinated proposals to ensure that development proposals do not prejudice other parts of the framework area coming forward
- Engaged with local residents, businesses, Local Planning Authority and community groups
- Developed a plan for Post Occupancy Evaluation.

7.1.3 Urban Environment

Proposals should align with the Local Plan, and follow the guidance in this SPD closely to add value to existing heritage assets, open spaces and connections to create a place-specific, contextual development that successfully integrates the new with the historic environment. They will need to;

- Retain and conserve historic buildings wherever possible
- Substantially improve public realm, commercial premises, homes and open space supported by appropriate facilities, including high quality open space
- Support a mix of uses supporting active lifestyles by encouraging walking and cycling

Early phases of development will need to demonstrate how they will contribute to the area becoming more attractive over time. Later phases of development will be supported by the improved desirability of the area from earlier phases.

7.2 Implementation

7.2.1 Infrastructure

Planning applications must demonstrate how the on-site infrastructure needed to facilitate development will be delivered.

It is expected that the necessary infrastructure will be delivered as part of the development within specific development plots, or secured through the use of planning conditions and/or planning obligations via Section 106. The Council will endeavour to make use of grant funding to help deliver wider public realm improvements where it is available and appropriate to do so in collaboration with Applicants and Developers.

Constraints in the sites within Intra may prevent healthcare capacity and school places from being fully accommodated on-site. Therefore some infrastructure may be delivered through financial contributions where demonstrably appropriate to do so and as agreed by the Council in consultation with the Guide to Developer Contributions and Obligations which explains the requirements for S.106 contributions and in line with the tests for planning obligations as set out in national planning policy and legislation, including the NPPF. Reference should also be made to any other guidance on local infrastructure provision at the time a planning application is made.

Similarly, given the tight urban grain and high street location, open space, play provision and biodiversity targets may not be achievable with individual application sites. The basis for public realm enhancement and habitat creation within Intra is set out in this framework. It could be implemented through developer contributions as part of the planning process.

7.2.2 Post-Occupancy

To create a cohesive place each new development within the area learns from earlier phases and avoids repeating any mistakes. The use of Post Occupancy Evaluation is expected in all planning applications and should take place with residents and businesses.

Development should also consider the latest information from the Council on nearby development proposals, where they have planning consent or are being built. The results should inform the design of subsequent development proposals.

During construction, this information should be used to identify any shortfalls in the provision of facilities, for example units designed to accommodate specific land uses, or open space designed for a specific need. Measures must be taken to minimise the impacts of construction activity and associated logistics on local residents and businesses.

7.2.3 Application Requirements

All applications affecting designated heritage assets require a Heritage Impact Assessment as part of a detailed planning application. An application of Listed Building Consent may be required alongside an application for planning permission.

For other proposals at Intra, an assessment to identify non-designated heritage assets is required. Archaeological investigation is required to continue understanding the history of this place and identify any impacts on emerging development proposals. Targeted archaeological excavations to mitigate otherwise unavoidable harm may be required prior to any construction.

To inform proposals, evidence of building recording and archaeological survey during the early stages of design development are required. Joint pre-application discussions will be required with Historic England, Kent County Council and Medway Council's Conservation Team. The applicant is expected to draw on the significant amount of information already available for both designated and non-designated heritage assets, and supplement this information with further analysis and investigation following advice from Historic England and Medway Council.

The requirements for planning applications should be followed. Where they are not strictly followed, applications will need to justify an alternative approach demonstrating that issues raised in the guidance are addressed appropriately and a similar standard is achieved.



Chapter
Appendix 8.0

8.1 Policy Context

8.1.1 Introduction

The Development Plan

The Town and Country Planning Act (1990) requires Local Planning Authorities (LPAs) to make decisions on planning applications in accordance with the development plan, with regard to any material considerations.

The Planning (Listed Buildings and Conservation Areas) Act 1990 (as amended) consolidates enactments relating to special controls and the grant of planning permission in respect of Listed Buildings and Conservation Areas.

At the time of drafting this SPD, the development plan comprises of the Medway Local Plan 2003, Local Plan Saved Policies, and the associated Proposals Map. Whilst the Local Plan covers the period from 1996-2006, most of the policies are 'saved', having been judged as compliant with current national planning policy.

The development plan in Medway will change over time, and developments will need to comply with the relevant plan in place. Medway Council is presently reviewing and preparing a new Local Plan. The Council is working to a further Regulation 18 consultation in the summer/autumn of 2023 with adoption anticipated in 2025.

The following Supplementary Guidance Documents (SPGs) are also relevant:

- *Medway Housing Design Standards (2011)*;
- *Building Heights SPD (2006)*;
- *Medway Council Air Quality Planning Guidance (March 2016 – Revised November 2021)*;
- *Waste and Recycling Requirements for new residential developments in Medway (2019)*

Given the age of the Local Plan, the National Planning Policy Framework (NPPF) is particularly relevant. The NPPF is a material consideration in determining planning applications. The National Design Guide (NDG) and National Model Design Code (NMDC) are both relevant as national initiatives and policies. There are also several neighbourhood plans in preparation in Medway, but these do not include or are adjacent to the Chatham Intra area.

There are a number of regional and local planning strategies and initiatives which should be considered as part of the context in Medway and Chatham Town Centre. This includes the designation of Chatham Intra High Street as a High Street Heritage Action Zone (HSHAZ) by Historic England.

8.1.2 Policy Framework - National & Regional Initiatives

National Planning Policy Framework (2023)

The National Planning Policy Framework (NPPF) requires Local Planning Authorities to positively plan to identify appropriate land for homes. It promotes well-designed places where people will want to live, with sustainable transport links and supporting infrastructure, local facilities, and opportunities for employment and leisure.

The Government is currently considering a number of potential amendments to the NPPF in light of the Levelling Up and Regeneration Act. The NPPF is supported by online Planning Practice Guidance (PPG) which is updated regularly and provides supporting guidance.

The National Design Guide (2019)

The National Design guide (NDG) addresses the question of how we recognise well-designed places, by outlining and illustrating the Government's priorities for well-designed places in the form of ten characteristics. These characteristics form the basis of design considerations to underpin the Chatham Design Code: Context, Identity, Built Form, Movement, Nature, Public Spaces, Uses, Homes and Buildings, Resources and Lifespan. The NDG defines and expands on each of these characteristics in detail.

National Model Design Code (2021)

The National Model Design Code (NMDC) is Government's guidance on the production of design codes, guides and policies to promote successful design. It expands on the ten characteristics of good design set out in the National Design Guide, which reflects Government's priorities and provides a common overarching framework for design.

The NMDC guidance sets out clear design parameters to help local authorities and communities decide what good quality design looks like in their area, based on local aspirations for how their area will develop, following appropriate local consultation. The Chatham design code approach is based on the NMDC's coding process, covering Analysis, Vision and Code stages.

Thames Gateway Parklands Vision (2008)

As an aspirational vision and spatial framework, the Thames Gateway Parklands Vision seeks to outline ways in which the public, private, third sector and local government organisations can shape the future of the Thames Gateway's urban and rural landscapes and improve the quality of life for people who live in the Thames Gateway. This vision is taken forward in the Thames Estuary 2050 Growth Vision. The Thames Gateway Spatial Framework identifies the following five key ambitions:

- *Thames Waterfront – develop a continuous Estuary Path running alongside both banks of the river, in addition to improving waterfront environments;*
- *Thames Gateway World Class Heritage – support heritage improvement within the Thames Gateway, as well as supporting the bid for a new World Heritage Site at Chatham Historic Dockyard;*
- *Thames Gateway Landscapes – to identify and support delivery of landscapes of regional importance;*

- *Thames Gateway Corridors – to support environmental improvements to strategic transport corridors; and*
- *Thames Gateway Squares – to work with partners to identify and start work on two or three large squares.*

Thames Gateway Kent - Plan for Growth 2014 - 2020

This report comments on the Medway region sitting within the core of the of Thames Gateway regeneration area, which is expected to accommodate significant economic growth within the next 20 years. Its vision for North Kent is set out as:

- *An area of exciting towns and cities complemented by an outstanding natural environment, providing an exemplar of urban regeneration;*
- *A thriving business centre attracting leading investors and businesses through its diverse and skilled workforce, high-quality commercial sites and local services, and accessibility to transport links and strategic routes to UK and continental markets;*
- *An area of strong, integrated communities with harmony between new and existing businesses and residents – where the benefits of development and investment are shared by the whole community;*
- *A vibrant cultural hub with a thriving social scene and civic pride – attracting a growing student population, bringing youth and vigour to the community.*

Thames Estuary 2050 Growth Commission 2050 Vision, 2018

Medway forms part of the north Kent Foreshore. The area will balance delivering growth in the health sector with new jobs, new homes, a renewed focus on skills, and high quality town centres set around world class heritage and natural assets.

Medway Local Plan (2003)

Medway's current development Plan is comprised on the Medway 2003 Local Plan, which is supported by various Supplementary Planning Documents. Medway Council is presently reviewing and preparing a new Local Plan. This has gone through three stages of consultation to date, i.e. Issues & Options, Development Options and Development Strategy. The Council is currently refining the Local Plan for a further regulation 18 consultation in the summer/autumn of 2023 with adoption anticipated in 2025.

Preparation of the Design Code will, in conjunction with the emerging Local Plan once adopted and the Chatham masterplan, provide a means to ensure that development comes forward along with the quality and design aspirations to maximise the development potential in Chatham.

Medway's Cultural Strategy (2020 – 2023)

Heritage is a significant part of the culture in Medway and particularly at Intra. The Cultural Strategy sets out the basis for cultural development and activities in Medway until 2030, and how these can offer opportunities to allow Medway to 'reposition itself as a cultural hub' and develop a

reputation on the national and international stage. The Strategy explains how this will lead an ambitious bid for UK City of Culture in 2025, which will galvanise the strength of Medway's creative community and be a beacon for inclusivity and culture. All the while, the Cultural Strategy highlights the importance of Medway's heritage, and sets out a radical framework called the Creative Medway Delivery Model which will ensure that the Council's cultural objectives are achieved.

The Joint Health and Wellbeing Strategy for Medway (2018 – 2023)

This Strategy is a high-level framework for improving the health and wellbeing of people in Medway, and has a vision of making people's lives as full, meaningful, and healthy as possible. It sets out five core themes which are used to inform commissioning services within health and care facilities in Medway, including investing in provision for young children (aged 0-5), enabling the older population to live independently and well, preventing early deaths and increasing healthy life duration, improving mental and physical health and well-being, and reducing health inequalities. Development which contributes to employment, healthy living, good wellbeing, and which creates opportunities for people would fall within these five themes.

Medway Council Strategy (2022 -2023)

The Local Plan supports Chatham as the city centre with accompanying policies that direct significant retail and leisure proposals and any other large scale proposals to Chatham to attract visitors

and further investment. This report sets out three priorities:

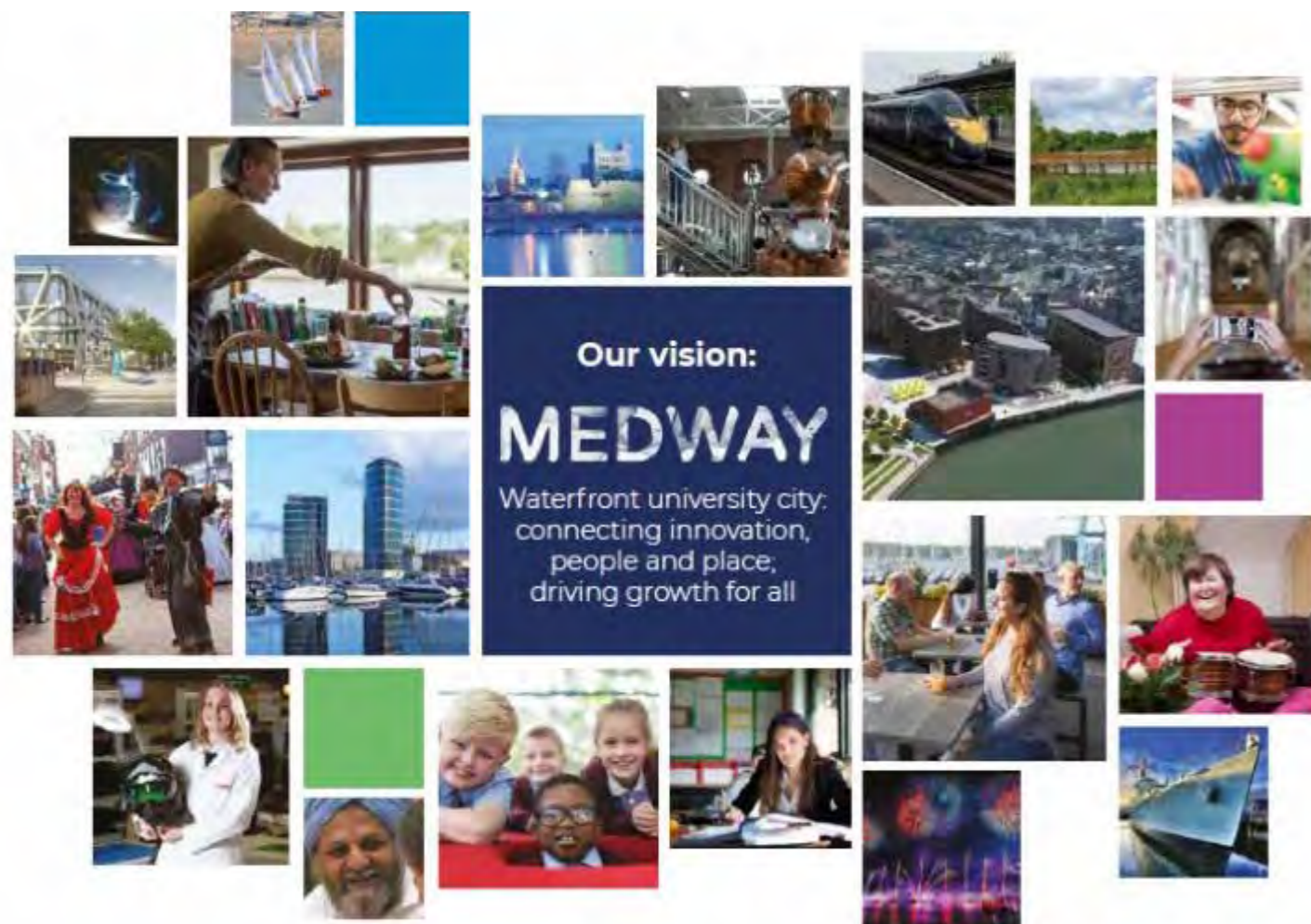
- *A place to be proud of;*
- *Maximising regeneration and economic growth;*
- *Supporting Medway's people to realise their potential;*

The council plan promotes Medway as a great place to work, live, learn and visit, supported by strategies for culture, tourism and regeneration. The council priorities provide a strong basis for positively planning for Medway and more specifically Chatham's needs, boosting the economy, improving access, delivering new homes, improving residents' health and wellbeing, and caring for the environment.

The council aims to boost economic performance through supporting local businesses and attracting inward investment, and targeting the creation of high skilled jobs.

Chatham Dockyard and its Defences Planning Policy Document (2015)

Whilst this document relates to the Chatham Dockyard and its Defences area along the River Medway to the north and east of Intra, it identifies that part of the Star Hill to Sun Pier Conservation Area is within the environs of Chatham Dockyard and its Defences, which is considered to have international heritage significance.



PEOPLE	PLACE	GROWTH
Supporting residents to realise their potential	Medway: A place to be proud of	Maximising regeneration and economic growth - growth for all
Healthy and active communities	A clean and green environment	A strong diversified economy
Resilient families	Put Medway on the map	Residents with jobs and skills
Older and disabled people living independently in their homes		Preventing homelessness
All children achieving their potential in education		Delivering new homes to meet the needs of Medway's residents
		Getting around Medway

Medway Council Strategy 2022-23 Vision and Key Principles

8.1.3 Policy Framework - Town Centre Initiatives

Chatham Centre & Waterfront Development Framework – Supplementary Planning Guidance (2004)

To deliver social and economic regeneration within Chatham, this framework proposes that any new development should be focused on the town centre and waterfront, and proposes that bold (rather than modest) transformation is required to allow Chatham to develop as the true economic and cultural centre of Medway. The framework outlines several projects and investments aimed at transforming Chatham's centre.

- *Accessibility improvements to the centre;*
- *New central library;*
- *New riverside promenade, improved access to the waterfront and improvements along the high street;*
- *New waterfront with performing arts facility, visual arts facility, new park and hotel;*
- *Over 8,000 sqm of small scale office space and leisure uses;*
- *New food store of approximately 8,000 sqm;*
- *Refurbishment and expansion of the Pentagon Centre – increase of 8,000 sqm;*

Chatham Town Centre Masterplan (2019)

This masterplan covers the Town Centre area of Chatham which is located immediately east of the SPD area. It identifies the sites which could potentially accommodate growth as set out in the

Local Plan, and high-level planning and design guidance to allow development to come forward in a more comprehensive manner, considering areas such as heritage, transport, the waterfront, and archaeology. The overall aim of the masterplan is to achieve a friendly, healthy, and vibrant centre which is the gateway to Medway.

Chatham Centre & Waterfront Development Brief – Supplementary Planning Document (2008)

This framework document intends to transform Chatham into a city centre for Medway, by developing key regeneration areas along the River Medway. To achieve this, the following objectives were outlined:

- *Appropriately scaled new development should be within prominent locations, framing public spaces creating vibrancy through the centre;*
- *Taller buildings should be of high quality, minimising overshadowing of key public spaces and add to and reinforce Chatham's historical and cultural identity;*
- *A sense of arrival can be established through the formation of clearly identifiable gateways and routes from the railway station which can connect via the High Street to adjoin the waterfront, through Town Hall Gardens and to Great Lines;*
- *Uninviting areas are to be activated and upgraded;*
- *Direct pedestrian links should be established between the High Street and the Waterfront and north-south routes should be established across*

the town centre from New Road to the Great Lines City park;

- Use of existing pedestrian routes should be enhanced where possible;
- Perimeter block development should be promoted enclosing semi-private spaces where appropriate;
- Visual connection should be created from within and to the centre and waterfront, linking with the River Medway and the Great Lines City Park;

Chatham 21 - Chatham High Street / Best Street Area Masterplan (2010)

This Masterplan – focusing on the High Street / Best Street area of the Chatham Town Centre seeks to create an attractive town centre, through focusing on the improvement of the overall townscape, creation of active frontages, public space provision, pedestrian links between the town centre and the waterfront, the creation of landmarks and the protection and improvement of view corridors.

Gun Wharf Masterplan SPD (2010)

The Gun Wharf Masterplan sets out an analysis of weaknesses and constraints, strengths and opportunities, and then outlines a masterplan for The Ordnance Site and the Civic Quarter. The Gun Wharf Masterplan also sets out a vision for the precinct:

“Gun Wharf will be an attractive civic and cultural quarter in Chatham, contributing to a Wider Medway regeneration. New development will take advantage of the site’s location adjacent to the waterfront and

unique assets to create an exciting and attractive destination for Chatham.”

The Masterplan proposes that any new buildings should respect the scale and importance of existing buildings, encourages enhancements to pedestrian links, river frontages and public open space, adaptive re-use of St. Mary’s Church, and removal of buildings which currently detract from the overall townscape quality of the area.

Chatham Placemaking Masterplan (2016)

The Chatham Placemaking Masterplan sets forth a public realm masterplan and implementation strategy for the improvement of the Chatham Waterfront Civic Space, which stretches from the River Medway and Waterfront Pumping Station towards the Bus Station, The Paddock and the entry of the Pentagon Shopping Centre. The masterplan sets out a long-term vision to *“Make Chatham a City Centre for Medway”* by establishing a powerful and distinctive identity for Chatham, which will be achieved through the following objectives:

- *Sequential Connectivity: Create an enticing, accessible journey from the station to the waterfront;*
- *Enhance Identity and Sense of Place: Recover Chatham’s historic core and existing qualities;*
- *Destination: Create landscape infrastructure that makes a bold, attractive change to the Waterfront;*
- *Delivery: Focus on achievable goals and employ deliverable solutions;*

Chatham Intra High Street Heritage Action Zone

Pioneered by Historic England and supported by Government funding, the High Streets Heritage Action Zones (HSHAZ) programme aims to aid the recovery of local high streets across England. It will do this by using heritage to fuel economic, cultural, and social recovery and create a long-term sustainable future for heritage assets and high streets.

The Sun Pier to Star Hill Conservation Area (which is also the Framework SPD area) has been awarded £1.6m in HSHAZ funding from Historic England for the heritage-led regeneration of this area including the High Street which runs through and unites it. The focus is on bringing the history of the area back to life and working to boosting the local economy by 2024. Funding was also made available through the programme for cultural activities to encourage local communities to engage with their high streets.

Chatham Future High Streets

In January 2021, Medway received grant funding as part of the Government’s Future High Streets Fund, aiming to inject funding into the regeneration of high streets in 72 areas across England to transform high streets into vibrant hubs for future generations and to protect, and create jobs.

The funding will be invested in a number of exciting projects in the heart of Chatham including reconfiguring the first floor of the Pentagon Shopping Centre for community use; delivering a creative and collaborative workspace at The Brook Theatre to support the development of the area’s creative sector; creating an innovation hub to support business start-ups and refurbishing the

vacant St John’s Church, which is part of the Church of England Diocese of Rochester.

The Grade II listed church will be restored as a place of worship and have community areas redeveloped to provide conference and event spaces for the creative and digital sectors.

The Future High Streets Fund is also being used to transform The Paddock (the space between Chatham Waterfront Bus Station and the Pentagon Shopping Centre) into a vibrant public space for residents and visitors to enjoy. The improvements will celebrate the area’s heritage and make the area more accessible.

8.2 Emerging Context

8.2.1 Local Context

Within the Framework SPD area, a number of sites have already been granted planning permission for new development prior to the adoption of this SPD. Whilst this illustrates how Intra is an attractive, dynamic part of Medway which is already attracting investment, the purpose of this SPD as mentioned in the previous section is to help guide this development so that it achieves the key objectives of ensuring a holistic, mutually beneficial heritage-led regeneration of the area.

01 Bardell Wharf MC/22/1810

Mixed use development comprising residential units (Class C3) and commercial floorspace (Class E) together with vehicular access off Bardell Terrace, vehicle (including on site and on street spaces) and cycle parking provision, private amenity space, landscaping, engineering works, public realm, and associated works. Highway works to the junction of Corporation Street and High Street and alterations to pedestrian crossing - demolition of all buildings on site. The proposal seeks the provision of 374 residential units and 357 sqm of commercial space.

02 Rochester Riverside MC/17/2333

Hybrid planning application seeking outline permission for the erection of up to 1,400 no. dwellings including a primary school and nursery (D1 use), up to 1,200sqm of commercial floorspace (A1/A2/A3/A4/B1/D1 and D2 uses) together with a pedestrian footbridge, parking, open space and landscaping. Full permission for phase 1, 2 and 3 of the development consisting of the erection of 489 no. dwellings (of the 1,400 no. total), the provision of a hotel (use Class C1), 885sqm of commercial floorspace (A1/A2/A3/A4/B1/D1 and D2 uses) along with site access/spine road, parking, open space and landscaping

Yellow shading represents major planning applications within the SPD site.
 Pink shading represents major planning applications outside of SPD site.
 Orange shading represents applications still to be determined.



Emerging local planning context

■ Major Planning Applications within the SPD site ■ Applications still to be determined
■ Major Planning Applications outside of the SPD site



CGI of the scheme at Bardwell Wharf



CGI of the first few phases of Rochester Riverside

03 209-217 High St MC/21/2271

Partial demolition of existing buildings and construction of a 4 storey residential student accommodation building (Class C2) comprising 85 bedrooms (including 2 for staff), with parking, amenity space and associated infrastructure

04 Theatre Royal MC/19/2762

Conversion of existing restaurant to provide smaller restaurant (Class A3) premises on ground floor and formation of 3 flats (1 x one bedroom and 2 x two bedroom) with provision of access to flat 2 (102) via 106 High Street and installation of roof light to side and additional window and alteration to door at ground floor side; glass screen doors to provide security to existing entrance and provide associated parking to rear - Resubmission of MC/19/1749

05 Anchorage House MC/20/1625

Prior notification of a proposed change of use from office (B1(a)) to residential (C3) to provide 81 units - resubmission of MC/18/2708

06 Land South of Anchorage House MC/23/0048

Construction of a 4 storey building comprising of 12 flats and 6 Class E units

07 Mountbatten House MC/20/3237

Change of use and conversion of Mountbatten House, as well as for a 12-storey extension to the building, to provide a total of 164 new homes and flexible commercial floorspace at the ground floor. The proposals also include a new rooftop restaurant at the top of Mountbatten House, a new entrance to the Pentagon Shopping Centre, and pop-up container-style retail/café units at the lower level of the former bus station along with public realm and landscaping works which include a new public square.

08 Chatham Waterfront Globe Lane MC/18/3659

Redevelopment of existing car park to provide 176 residential apartment, 1141m2 of commercial floor space (A3/A4/B1), landscaping and associated parking

MC/21/1895

Application for non-material amendment to planning permission MC/18/3659 to enable 182 residential apartments and 895m2 of commercial floorspace (A3/A4/B1), landscaping and associated parking, updates to construction information and relocation of UKPN substation.



CGI of the scheme at Chatham Waterfront



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