

# THE INTERFACE LAND, CHATHAM

SUPPLEMENTARY PLANNING DOCUMENT



OCTOBER 2010

**TURLEYASSOCIATES**

# FOREWORD

The closure of HM Naval Base Chatham in 1984 left an area in excess of 400 acres of largely unoccupied buildings, land and water. This included:

- a poorly maintained 80 acre estate to the south with a diverse collection of historic building representing the finest example of a dockyard of 'the age of sail' in the World;
- a substantial range of fine Edwardian brick barrack buildings;
- a 1960/1970s built two steam nuclear submarine refit complex with an extensive assortment of large low grade 20th century buildings on contaminated land; and
- three factory basins each of some 20 acres providing deep water moorings, one with dry docks and the most northerly operating as a commercial port with locks to the river Medway.

All of this with huge potential but with extraordinary challenges and associated costs.

Since that date there has been significant investment in the location by the Chatham Historic Dockyard Trust (the 'Trust'), a succession of Government regeneration agencies, most significantly the South East England Development Agency ('SEEDA'), and the private sector. This has resulted in the southern area, the Historic Dockyard, becoming a successful tourist destination and mixed use site, the establishment of the Universities and Chatham Maritime becoming one of the top three office locations in Kent. In addition, over 1,000 homes have been built alongside a marina, a large retail and entertainment complex with a cinema, bars, restaurants and hotels.

As investment continues in Chatham Maritime and The Historic Dockyard we, as landowners, are delighted to jointly bring forward a Supplementary Planning Document (SPD) to guide future development for the Interface Land, Chatham. The land forms the last majority undeveloped site of the former Royal Navy Dockyard and together we share a common aim in seeking the regeneration of this major site at the heart of Medway.

The landowners' and local planning authority's aspirations are the development of a high quality mixed use scheme providing scale and significance against the Covered Slips and other historic buildings and features. SEEDA and the Trust are committed to overseeing the long term development of the Interface Land in their role as landowners. This important commitment will drive the delivery of the proposals, whilst ensuring consistency and high standards are maintained in the quality of design and construction.

By adopting a joint working approach, the proposals set out in this SPD will complement and support the key regeneration objectives of the area and help to address additional long-term challenges to ensure that the potential of the Interface Land is realised and future development complements the investment to date.

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Revision: -

Date of Issue: October 2010

Status: FINAL

Checked: JF

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**What is a Supplementary Planning Document?**

- 1.1 Supplementary Planning Documents (SPDs) are so titled because they add details to policies set out in development plans in order to explain how the local authority's policies can be taken forward. They may take the form of design guides, area development briefs, master plans or issue-based documents. These documents can use illustrations, text and practical examples to expand on how the authority's policies can be taken forward.
- 1.2 A Supplementary Planning Document is a Local Development Document that may cover a range of issues, thematic or site specific, and provides further detail of policies and proposals in a 'parent' Development Plan Document.
- 1.3 SPDs are used to assist with determining priorities, guiding investment and deciding planning applications.
- 1.4 The current local development plan is the Medway Local Plan, which was formally adopted in 2003. The Local Plan will soon be succeeded by the Local Development Framework (LDF) which will plan for the future of Medway up to 2026. This SPD will form part of that framework. The SPD has been produced in accordance with Policies S8 and S9 of the 2003 Plan.
- 1.5 Local authorities are required to involve local communities in the preparation of SPDs before they can be adopted as formal planning policy.



Covered Slip Buildings looking north

Various key stakeholders were involved during the preparation of this SPD.

- 1.6 A Sustainability Appraisal, attached at Appendix 1, incorporating the requirements of the Strategic Environmental Assessment Directive, has been prepared alongside this SPD. Medway Council, Natural England, the Environment Agency and English Heritage were consulted on the requirement for a Sustainability Appraisal of the SPD in November 2009. A Pre-Assessment Report was consulted on in February 2010 to ensure the most up to date policies and baseline data available were identified. The SPD was appraised to predict and evaluate the likely effects. The Sustainability Appraisal has helped to ensure that this SPD will promote sustainable development.

### Role and Objectives of SPD

- 1.7 This SPD sets out an overview of the development requirements for the site following a strategic baseline review of existing framework documents; sets out the overarching spatial design principles and components to deliver successful and sustainable change and concludes with the recommendation of a delivery strategy to ensure the successful implementation of the plan.
- 1.8 The Interface Land is a strategic site with the ability to reconnect the former elements of the Naval Dockyard - The Historic Dockyard and Chatham Maritime. The Historic Dockyard is the world's most complete historic dockyard from the age of sail. There are a number of key objectives for the future development opportunities within the Interface Land including:
- promoting a development appropriate to the scale and character of the historic environment and which enhances that environment and the setting of its buildings, most of which are Scheduled Ancient Monuments (SAM);
  - establishing a development that reconnects the formerly integrated elements of the original Naval Base and Dockyard whilst preserving the Historic Dockyard's secure boundary;
  - maintaining and enhancing existing vehicular and pedestrian routes, with the addition of a riverside walk to the north side of Covered Slip 7. Existing parking numbers are to be retained;



Covered Slip Buildings looking south



Covered Slip 7

- the creation of an appropriate sense of arrival at the Historic Dockyard; and
  - creating a mix of uses likely to enhance and sustain the emerging Chatham Maritime/Historic Dockyard destination.
- 1.9 The main role of this SPD is to set out a Spatial Masterplan that forms a framework that guides planning and investment for the future redevelopment of the Interface Land. The Masterplan should be read in conjunction with all supporting technical documents set out in Appendices 1 to 6; these set out the background, technical requirements and review emerging guidance, all of which are considered as part of this SPD.
- 1.10 The Chatham Maritime Interface Land Supplementary Planning Document is currently in consultation status supplementary to the planning policy.

**Vision statement**

- 1.11 The vision for the Interface Land is to create a sustainable development that provides an opportunity to link Chatham Maritime with the Historic Dockyard, whilst reflecting and respecting the heritage sensitivities of the study area. The proposals will create a complementary character that combines high quality buildings and public realm and respond well to its context to attract and retain investment.
- 1.12 The emerging development proposals should reflect the historic age of sail, industrial past and pioneering technology developed at Chatham. It will contain a mix of uses that complement the study area and its surroundings. It will also support the creation of a more attractive, accessible, safe and sustainable environment.
- 1.13 In order to bring forward the vision, future development of the Interface Land should seek to:
  - provide high quality built form and public realm;
  - introduce innovative sustainable design solutions;
  - preserve and/or enhance the special historic character and appearance of the heritage assets both within and outside the site;
  - reinstate cohesion within the surrounding area;
  - introduce a range of complementary and compatible land uses; and
  - create a distinct identity but respect its setting;



North Mast Pond



Mast House and Mould Loft

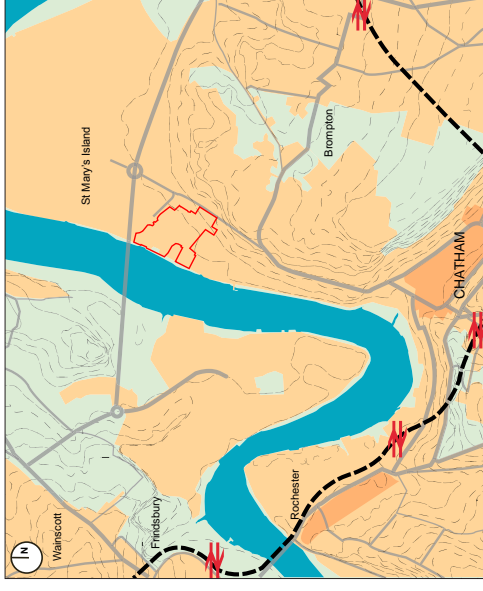
- form a co-ordinated approach from the landowners to development on the site.
- 1.14 The Government's objective is that the historic environment and its heritage assets should be conserved and enjoyed for the quality of life they bring to this and future generations. To conserve assets in a manner appropriate to their significance, it is important that:
  - wherever possible, heritage assets are put to an appropriate and viable use that is consistent with their conservation;
  - the positive contribution of such assets to local character and sense of place is consistent with their conservation;
  - consideration of the historic environment is integrated into planning policies, promoting place-shaping.
- 1.15 The site's location in the river valley and historic features dictate the massing of future built form. In addition, there is the need to protect the identified strategic views and connections to the ridgelines, which limits the scale of the future built form in relation to the wider setting. There is a need to protect views to the north from within the Historic Dockyard.
- 1.16 In order to respond to this vision the Interface Land will need to be subject to detailed masterplanning work.

### Location and Context

- 1.17 The Historic Dockyard is located to the north of Chatham town centre on the southern bank of the River Medway. Occupying an area of some 32 hectares (80 acres) the Dockyard was in continuous occupation by the Royal Navy from 1618 to 1984.
- 1.18 The Interface Land forms the last major undeveloped site in the former Royal Naval Dockyard, Chatham, and is in the ownership of South East England Development Agency (SEEDA) and the Chatham Historic Dockyard Trust (a Registered Charity established by government following the Dockyard's closure in March 1984. The land sits between the earlier Georgian Dockyard to the south and the large nineteenth century extension to the north and is historically associated with the age of sail.
- 1.19 The Interface Land extends to 10ha in size, however there are a number of heritage buildings and designations both above and below land which form significant constraints to future redevelopment. The land is currently disused or partly used for parking associated with the tourist elements of the Historic Dockyard. The Interface Land provides the arrival gateway to the Historic Dockyard.
- 1.20 Since the closure of the Royal Naval Dockyard in 1984 a programme of repairs and refurbishment has created a mixed-use site, which is a successful tourist destination in its own right attracting up to 170,000 visitors per annum. The Historic Dockyard has a thriving community of over 140 small businesses and organisations employing approximately 1,000 people; in addition there are around 400 residents. Approximately 2,000 people use the site daily,

equivalent to 700,000 per year.

- 1.21 A conservation management plan (2004 – 2009) for the Historic Dockyard Conservation Area has been prepared. This document has helped to inform the preparation of this SPD by identifying the conservation issues affecting the wider Dockyard and providing appropriate guidance.
- 1.22 Over the past decade, the wider Chatham Maritime location has attracted a number of large corporate occupiers, a new university campus, an outlet centre and a residential quarter. At Chatham Maritime it is estimated there are 3 million visitors per annum, whilst the destination provides 3,000 jobs, accommodates 3,000 residents and also comprises around 7,000 students.



Plan showing the site in relation to its wider context



Plan showing the site boundary



### Historic Context

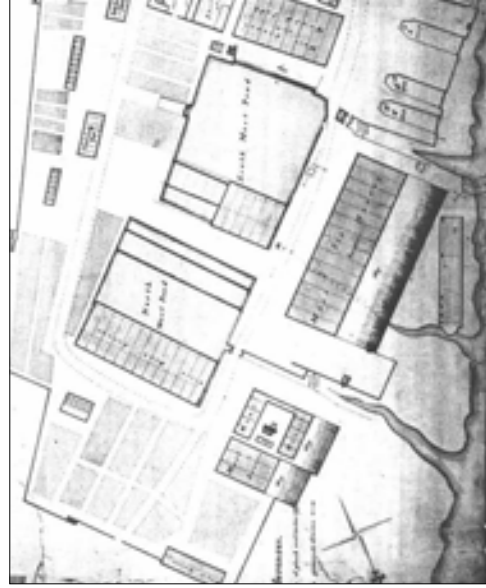
- 1.23 The first documented evidence of the Royal Navy's use of the River Medway can be found in 1547 with the rental of two storehouses on 'Jyllingham Water', whilst the first war ship constructed was the Sunne in 1586. In 1613 the dockyard moved downstream to the present location of the Historic Dockyard and by 1618 storehouses and a ropewalk had been built.
- 1.24 The Interface site lies north of the 17th century dockyard and, developed the first mast pond (south) in 1696 with a series of arches and racks integrated within this for underwater timber storage. The second pond followed circa 1700. The dockyard grew significantly in the 1720s



Plan showing the layout of the dockyard in the early 18th century with the two mast ponds and the sea wall clearly visible.

within a new boundary wall with tower houses that crossed the Interface site. The Mast House and Mould Loft were completed in 1755 and today provide an example of timber framed construction by the dockyard workforce.

- 1.25 The 1770s saw the construction of a number of sawpits, some in saw houses and, subsequently timber seasoning stores of which two survive. This activity relates to a 1773 decision taken under the Earl of Sandwich to use the dockyard primarily as a building and repair yard. Timber processing was then the main purpose of the Interface site and a 1795 plan shows a series of Mast Houses and a Boat House, that developed adjacent to the river, and Store Boat Houses that were constructed over the North and South Mast



Plan showing the layout of the dockyard in 1795 with the 18th dockyard wall visible.

ponds. This plan also clearly shows the then alignment of the river front with wharfage made out of massive vertical timbers.

- 1.26 The next significant development within the dockyard was the development of Marc Brunel's steam powered sawmill in 1814 as part of the demands imposed for ship building by the Napoleonic wars. A canal was developed to transport logs from the south mast pond to an elliptical shaft at the sawmill. After cutting these, timbers would be moved by overhead crane to timber stacking yards to the east and south of the Police House. Expansion for the sawmill saw the early 18th century dockyard wall taken down and replaced by the present example.

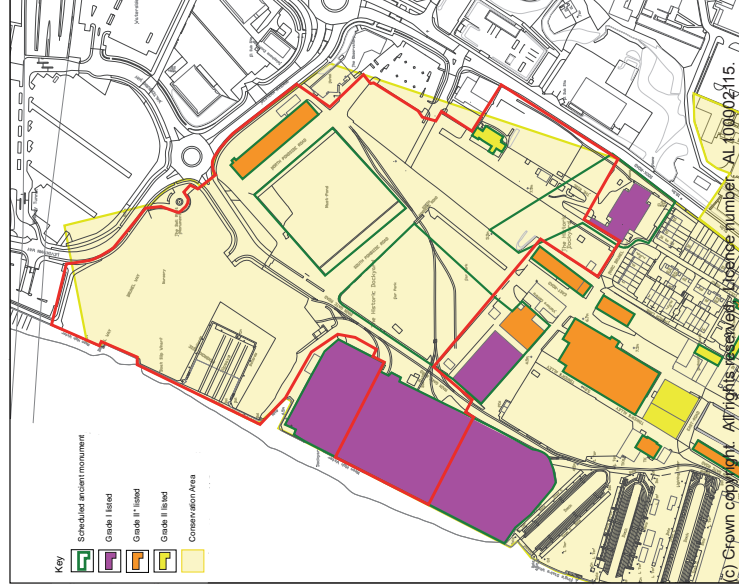


Plan showing the layout of the dockyard in 1879 with ... visible.

1.27 Land reclamation westwards into the River Medway occurred after 1817 on which new covered building slips were then built. No 3 Slip in timber from 1838 and subsequently Slips 4, 5 and 6 from 1846. Slip 7 was constructed from 1851. With the introduction of steam power and iron hulls the existing dockyard built for a wooden sailing navy was too small and a major extension was carried out after 1860 on St Mary's Island. This left the Interface site as an area between a new Victorian steam facility and the core of the older dockyard. New uses were found for the site as the importance of timber declined and land reclamation westwards into the river increased the space available and left the 18th century wharfrage fossilised behind the present day river wall. The last Chatham built battleship was built on the former open air Slip no. 8 in 1905, with the covered slips then predominantly being used for submarine building.

1.28 In 1984 the dockyard closed with many of the undesignated buildings in the northern part of the site being demolished. Today, a number of structures which were retained form an integral element and backdrop to the Interface site. The importance of the surviving dockyard buildings and some buried structures has been recognised through statutory listing or designation as Scheduled Ancient Monuments. Archaeological buried remains of equal importance are not designated. All these heritage assets form a part of the conservation area.

1.29 The current conservation area designation recognises that the significance of the Dockyard is the sum of the individual designated assets and those which cannot be documented by individual designations. The complex holistic character of the Dockyard that is embodied by both structures and spaces and the linkages between these.



Plan showing the Historic Designations

1.30 Following recent public consultation on the future of World Heritage, the Department for Culture, Media and Sport has announced that it will be drawing up a new UK Tentative List of possible nominations for World Heritage status, with the view of submitting a new List to UNESCO in 2011. Chatham Dockyard and its defences were previously on the Government's shortlist of sites and has applied under the new guidance to reaffirm this position.

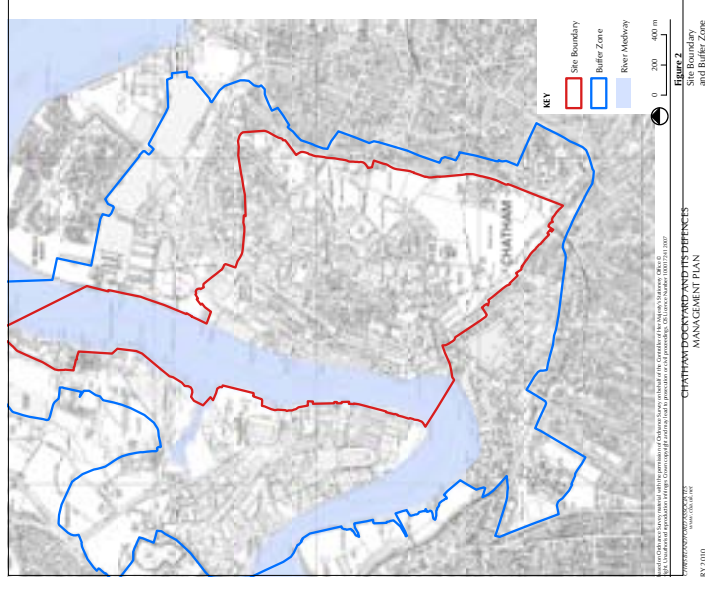
1.31 The site has been proposed as a future World Heritage Site because:

- Chatham is the world's **most complete** example of an historic dockyard from the age of sail and early age of steam (1700-1865).
- Chatham was **instrumental** in securing and maintaining Britain's worldwide influence, leading the world in industrial design, naval architecture and military technology.
- The **completeness** and **survival** of the Dockyard and its defences - Fort Amherst and the Chatham Lines, Upnor Castle and the Great Lines - is unrivalled.
- A series of historic barracks and military installations demonstrate the inter-relationship between the army and navy that shaped Chatham as a **complete industrial military complex** and **garrison town**.

1.32 Under current legislation, it is hoped that Chatham will be nominated for World Heritage status in 2012.

### World Heritage Site Nomination

"Chatham Dockyard is the supreme example of a Royal Dockyard largely unaltered from the age of sail.... The importance of the Dockyard is enhanced by its close association with contemporary military establishments. This combination of a substantially intact 18th century Dockyard with its contemporary massive landward defences is unique" from 'World Heritage Sites: the UK Tentative List' (Department for Culture, Media and Sport, 1999).



Plan showing the proposed World Heritage Site boundary and buffer

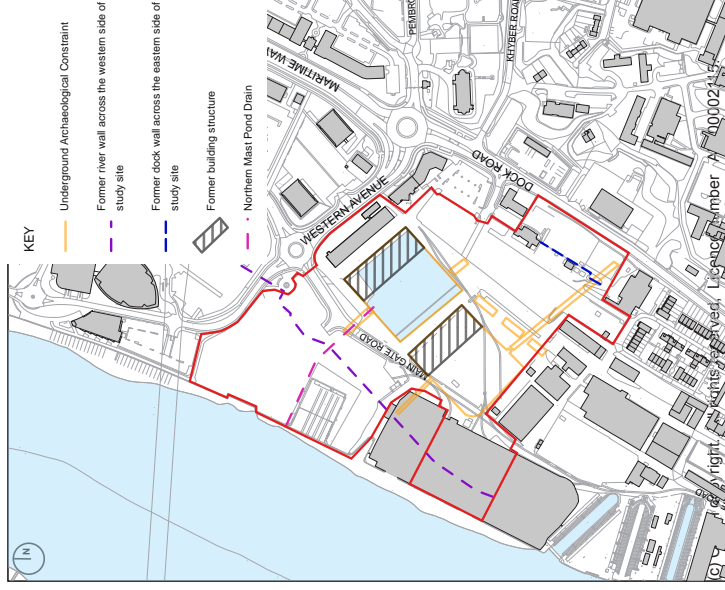


### Archaeological Statement

- 1.33 There is substantial evidence within the Interface Land for timber and masonry structures of the Georgian Dockyard (of the age of sail) dating to the late 18th and early 19th centuries. These structures can now primarily be found underground. However, it is essential that these are acknowledged, recorded and in some cases protected as they form an integral part of the Historic Dockyard and demonstrate the historical functional relationships of the structures in the working Dockyard.
- 1.34 The principal features include the former timber, river wall illustrating the form and location of the 18th century Dockyard prior to any land reclamation, active and redundant conduits linking the mast ponds with the Medway and a series of saw pits which were integral to wooden ship building. In addition a canal linked the South Mast Pond and the Sawmill.

### Understanding Heritage Values

- 1.35 English Heritage provides guidance relating to how inter-related heritage values may be attached to a place. It is important that these values are considered in relation to future development of the Interface Land. Alterations and new work to a significant place should normally be acceptable if:
- there is sufficient information comprehensively to understand the impacts of the proposal on the significance of the place;
  - the proposal would not materially harm



Plan showing key Archaeological Features

- the values of the place, which, where appropriate, would be reinforced or further revealed;
- the proposals aspire to a quality of design and execution which may be valued now and in the future; and
- the long-term consequences of the proposals can, from experience, be demonstrated to be benign, or the proposals are designed not to prejudice alternative solutions in the future.

- 1.36 This guidance is set out in the English Heritage Conservation Principles, Policies and Guidance dated April 2003. The document can be downloaded at [www.english-heritage.org.uk](http://www.english-heritage.org.uk).

### Policy Context

- 1.37 SPDs must be consistent with national, regional and local planning policies. A review of relevant planning policies has been carried out and is provided at Appendix 2 of this document. However, below is a summary of some key policy documents that have been identified as a result of the planning policy review and which need to be considered for future development.
- 1.38 Planning Policy Statement 1 (PPS1) sets out sustainable development as the core principle to inform planning. Medway Council considers it essential that the redevelopment of the Interface Land embodies the principles of PPS1 including:
- making suitable land available for development in line with economic, social and environmental objectives to improve people's quality of life;
  - contributing to sustainable economic development;
  - protecting and enhancing the natural and historic environment and existing communities;
  - ensuring high quality development through good and inclusive design and the efficient use of resources; and

- ensuring that development supports existing communities and contributes to the creation of safe, sustainable, livable and mixed communities with good access to jobs and key services for all members of the community.

1.42 The Statement states that not all elements of a World Heritage Site or Conservation Area will necessarily contribute to its significance. Therefore, when considering proposals local planning authorities should take into account the relative significance of the element affects and its contribution to the significance of the area as a whole.

1.39 Planning Policy Statement 5 (PPS5) is now the policy basis for all plan making and assessing of heritage-related applications in England and is the first major step in the reform of the heritage planning system ahead of a new Bill. The Statement encompasses any 'heritage asset' considered to have some degree of significance, this includes all nationally designated and registered heritage assets (listed buildings, Scheduled Ancient Monuments, Conservation Areas, registered parks and gardens, battlefields, archaeology, World Heritage Sites), plus all locally designated heritage assets (locally listed buildings, sites of local archaeological interest etc).

1.43 Sustainable development is given a high priority by PPS5, with local authorities encouraged to seek the reuse and sympathetic adaptation of historic buildings to reduce carbon emissions.

1.44 A review of the Medway Local Plan identified that the Interface Land is not covered by any adopted policies that would prevent development within it. It falls within two strategic policies (Policy S8) Chatham Maritime and (Policy S9) Chatham Historic Dockyard. These both promote high quality urban design and townscape with the latter stating that "development that respects the historic character of the site will be permitted."

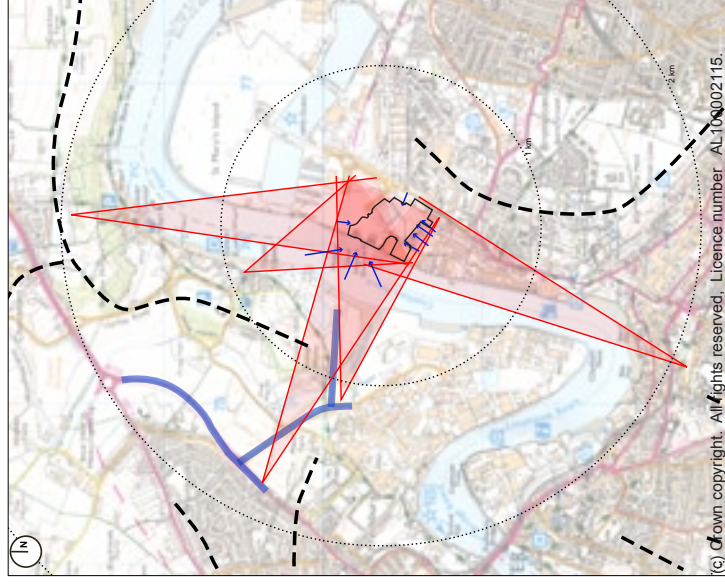
1.40 Through the Statement, local authorities are encouraged to identify and assess the significance of any element of the historic environment that may be affected by development proposals. This significance includes the value of an asset for this and future generations.

1.45 The Medway Waterfront Renaissance Strategy, 2004, identified the Interface Land for regeneration with housing, commercial and leisure uses, and offices uses. This mixed use development is seen as integrating the Historic Dockyard, to the south, and the Docks outlet shopping centre, to the north.

1.41 Heritage assets are defined in the Statement. Of particular relevance are those assets with archaeological interest that are not currently designated as Scheduled Monuments, but are of equivalent significance. The Statement confirms that the absence of designation does not indicate lower significance and that some assets may need to be considered under the same policies as for designated archaeological heritage assets.

1.46 The Interface Land falls within the University and Leisure Quarter and 3.11 of Part 2 of the Building Height Policy for Medway SPD and states that:

*"The area immediately to the north of the Historic Dockyard is known as the 'Interface Land'. It is largely redundant at present. However, it has the potential to be a major new leisure and tourist destination. Distinctive landmark buildings could help reinforce this role.*



Plan showing the strategic and local views

*This may justify carefully designed and placed higher buildings. However, it is important that the Covered Slips at Chatham Historic Dockyard remain a key focal point of riverside views and that the backdrop of the Brompton and the Lines remains prominent as a key part of the setting of the Dockyard. This will be an important factor in shaping design proposals for the area".*

### Conservation Area

1.47 The majority of the Interface Land falls within the Chatham Historic Dockyard Conservation Area.

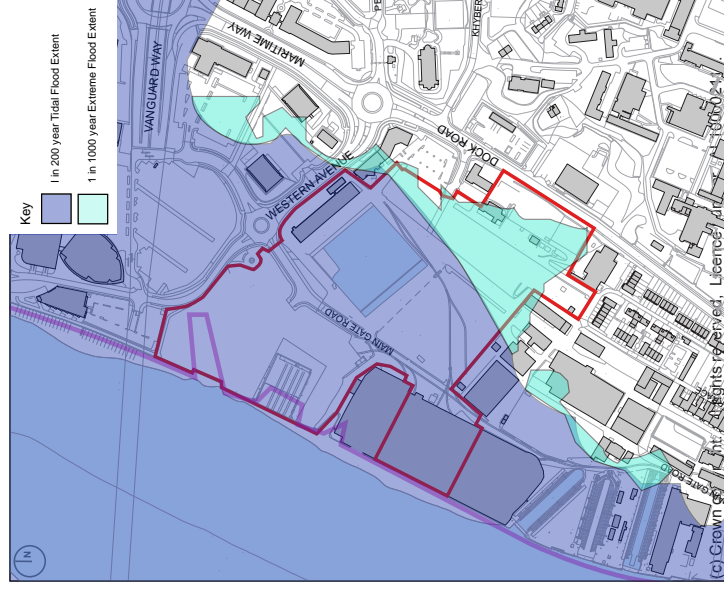
### A Building Heights Policy

1.48 An SPD relating to tall buildings in Medway was published by the Council in May 2006 to ensure that any proposals for tall buildings within the region are properly considered in the context of a clear long term vision. As a key regeneration site within Medway, the Interface Land must adhere to this document and conform with its recommendations.

1.49 The SPD comprises two sections. Part 1 sets out general location and design policy criteria for formulating and assessing high building proposals, whilst Part 2 identifies locations where taller (referred to as higher in the document) buildings are not appropriate within the defined Medway Waterfront Renaissance Strategy Area. Appendix A to Part 2 sets out the identified strategic and approach views, and the important features within each view.

1.50 As part of the contextual analysis, Turley Associates undertook a Building Heights Assessment to determine the appropriate building heights for emerging development proposals for the Interface Land. The complete assessment can be found at Appendix 3.

1.51 The policy and contextual assessment undertaken for the Building Heights Assessment concludes that any new development within the site would need to have limited visibility from within the Historic Dockyard, and have particular regard to the setting of the heritage assets.



Plan showing the Flood Risk within the Interface Land

1.52 The visual assessment focuses on the primary location for tall buildings within the land adjacent to Slip 7 and the River Medway to the northwest of the site.

### Flood Risk

1.53 The Interface Land is primarily located within tidal Flood Zone 3 (ie high probability flooding - > 0.5% annual probability of flooding from the sea), however the eastern end of the site is within Flood Zone 2 (ie medium probability of flooding - annual probability of flooding between 0.5% and 0.1%) and a small element Flood Zone 1 (ie low probability of flooding - annual probability of flooding lower than 0.1%).

1.54 PPS25 recommends that development should be directed towards those areas at a low probability of flooding (ie Flood Zone 1), only where there is no reasonable available site in Flood Zone 1 can Zones 2 and then 3 be considered. Also, if development within the floodplain is necessary, it should be sequentially allocated putting the less vulnerable development (eg commercial spaces) within the areas in which the expected probability of flooding and hazard are higher and the more vulnerable development (eg residential units) within the areas in which the expected probability of flooding and hazard are lower.

1.55 However it should be recognised that areas in Flood Zone 3 along the River Medway are also most desirable for residential use and the viability of any future development could be compromised in promoting alternative uses in these areas. Proposals will recognise and address the flood risk issues and ensure adequate mitigation measures are implemented in discussion with the Environment Agency and following the advice of PPS25.



1.56 A flood risk investigation report prepared as part of the SPD is included at Appendix 4.

#### **Transport Infrastructure/Movement**

1.57 It is recognised that any development at the Interface Land will generate additional vehicular traffic, therefore the key importance of transportation and access within the site and linkages to Chatham, Medway, Kent and London should be addressed. Sustainability principles are essential and development proposals should, where possible, demonstrate how they will reduce reliance on the private car, enhance public transport usage and improve walking and cycling facilities.

1.58 The quality of the bus services to Chatham is excellent due to a frequent servicing linking Chatham Maritime retail outlets to the town centre. The proposed scheme should seek to optimise access to these services, around half of which are available from Dock Road by Western Avenue, and all of which are available from Dock Road by Wood Street. The desirability of Chatham has also recently been further enhanced by the introduction of high speed rail services to London St Pancras.

1.59 The principal access route to the Interface Land will be from Western Avenue. It is important that all existing or proposed internal roads can accommodate lorry movements as these are required to service the businesses located within the Historic Dockyard.

1.60 At present, car parking for the adjacent Historic Dockyard is provided on the site, totalling some 400 spaces; these must continue to be accommodated within the site. The proposed



Figure ground plan showing the existing urban grain

development should seek to deliver a car parking strategy which accommodates the existing demand for car parking on the site, together with the demand generated by the additional development. In addition, parking facilities need to be provided for twelve coaches and provision of parking for the hotel should also be accommodated.

1.61 A sustainable transport strategy, prepared as part of this SPD is included at Appendix 5.

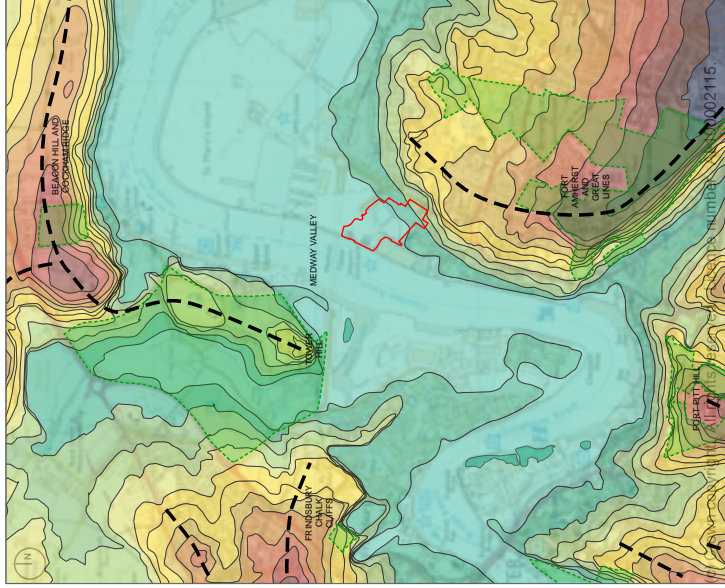
#### **Ecology**

1.62 Surveys will be required to confirm the presence of any protected species prior to any development being undertaken. These surveys should take place in consultation with Natural England to ensure compliance with relevant legislation and guidance.

1.63 Any tree works would require permission from the Council due to the Conservation Area designation.

#### **Urban Grain**

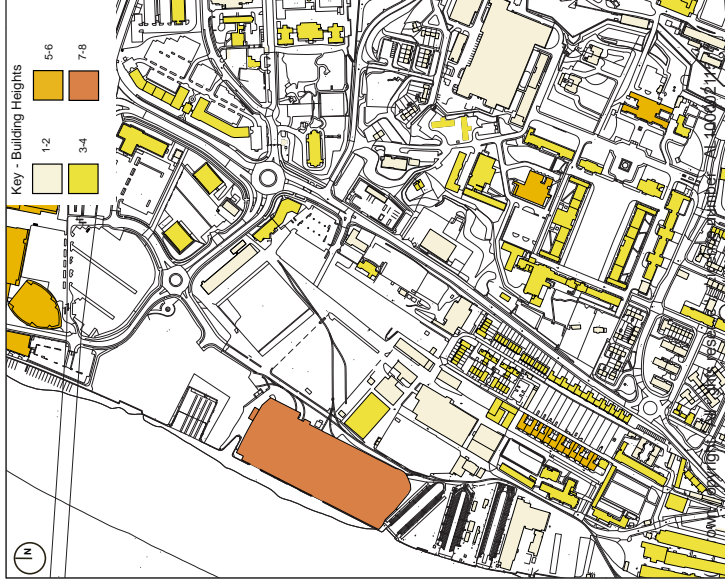
1.64 The figure ground study shows that the Interface Land has only a number of buildings within it and currently consists of predominately open land. It illustrates that the area to the south east of the Interface Land (the Historic Dockyard) boundary has a tight urban grain with medium to small buildings defining blocks and open spaces. There is a series of simple large building footprints adjacent to the River Medway to the southwest, formed by the Covered Slip buildings. To the north are a number of medium to large buildings that are set within open land which form car parks to the large retail centre.



Plan showing the existing topography

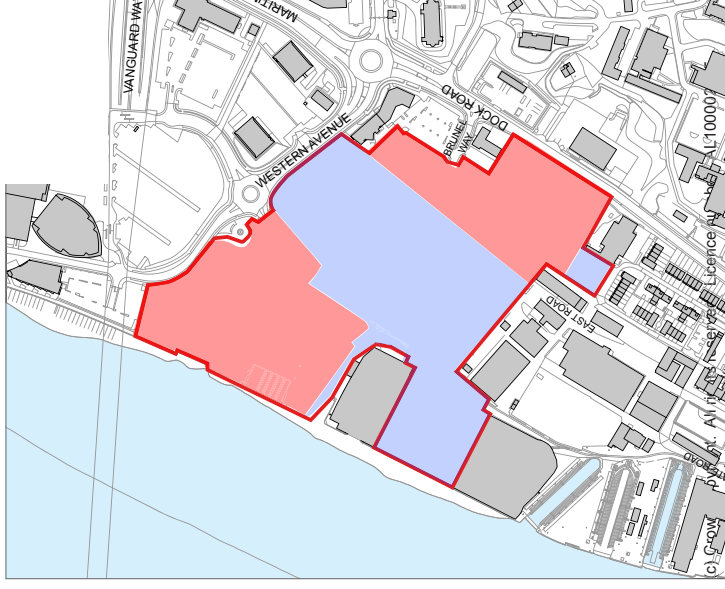
**Topography**

1.65 The Interface Land is located within the lower ground of the Medway Valley. It is framed by the surrounding raised topography of Beacon Hill and Cockham Ridge to the north, Tower Hill to the west, and Fort Amherst and Great Lines, to the south and east. The Building Height Policy SPD highlighted that the bowl, green hill slopes and ridges are important components of the Medway landscape.



Plan showing the building heights

1.66 Within the Interface Land the topography rises from 4.2 metres AOD to the northwest, dropping to 3.8 metres within the centre of the site between the Mast Ponds. Then rising up to 12.2 metres to the southeast area of the site, adjacent to the Brunel Saw Mill.



Plan showing the Interface Lands ownership boundaries

**Building Heights**

1.67 The building heights plan illustrates that the area to the southeast of the Interface Land is primarily one to four storeys in height (approximately 6 to 15 metres) and the medium to large footprints adjacent to the River Medway to the southwest and north are around five to eight storeys in height (approximately 18 to 27 metres). Further to the north (and not illustrated on this plan) there are two towers known as the Quays that are 20 and 16 storeys in height.

**Development Constraints**

1.68 The preceding strategic framework has highlighted the complexities of developing this large, largely vacant, site having regard to the Historic Dockyard to the south and the Chatham Maritime development to the north. Set out below is a summary of the constraints to development that must be considered as part of a future development strategy. A number of the constraints relate to the historic features of the site, therefore English Heritage is an important statutory consultee for any future development proposals.

**Heritage Considerations**

1.69 The site falls with the Chatham Historic Dockyard Conservation Area and includes a number of Scheduled Ancient Monuments, Grade I, Grade II\* and Grade II buildings.

**Archaeological Considerations**

1.70 Buried archaeological remains should be anticipated in most parts of the site, but with a lower potential in the reclaimed land of the riverside. Much of the buried archeology is of undoubted national significance and the advice of PPS5 paragraph HE9.6 applies.

**Land Ownership**

1.71 The complexity of boundary issues means building blocks or associated uses must not cross land ownership boundaries.

1.72 Slip 7 is privately owned, therefore it is important that its continued use is considered in emerging masterplan documents.

**Building Heights and Massing**

1.73 The volume, scale of the historic Slip buildings should be acknowledged and any new proposed development respect their industrial heritage and setting in the context of the River Medway.

**Flood Risk**

1.74 A large part of the Interface Land falls within the 1/200 year flood level area, whilst the majority of the remain land is 1/1,000.

**Access**

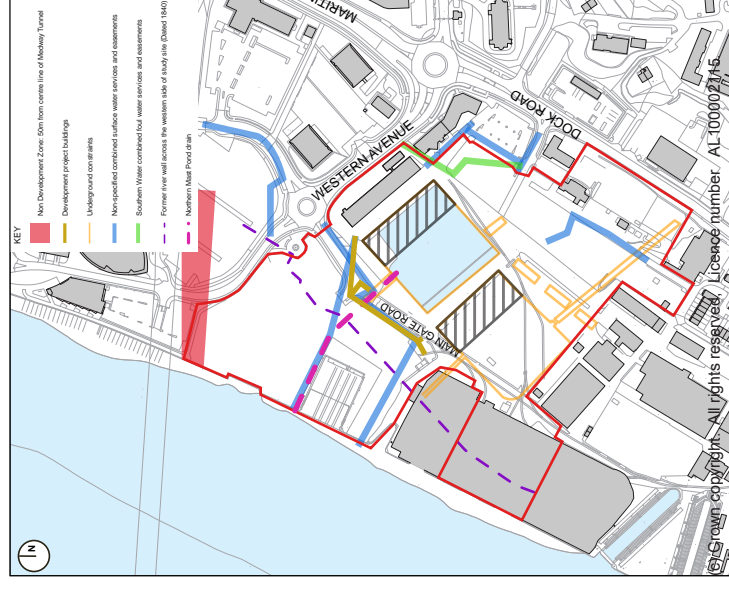
1.75 Main vehicular access must be from Western Avenue; access from Dock Road via Brunel Road is limited and can not be guaranteed.

**No Build Zone**

1.76 There is a non-development zone which applies 50 metres from the centre line of the Medway Tunnel. In addition, there is a 7 metres non development zone adjacent to the River Medway that will permit future maintenance and rebuilding of the river wall. This area should accommodate the extension of the Chatham Maritime Riverside Walk.

**Strategic Views**

1.77 From the other side of the River Medway the Covered Slips are visible landmarks that stand out against a relatively low tree ridgeline. Any future development would need to be of a sufficient scale to complement the Slips and future development should be at an appropriate distance to preserve their setting and overall integrity.



Plan showing the Interface Lands constraints

**Services**

1.78 A number of surface water, foul water and easements exist across the Interface Land.

**Noise**

1.79 The existing uses of the Saw Mill and Slip 7 sometimes generate noise. Mitigation methods should be sought through design.



### Ownerships

1.80 There are two principal landowners for the Interface Land; SEEDA, shown in orange on the plan on page 15, and the Chatham Historic Dockyard Trust, shown in blue. The Trust's interest relates to the Mast Ponds and surrounding land, whilst SEEDA owns the riverside land and that adjacent to Brunel Way/Pembroke Rise. A considerable constraint to any future development is the condition that built development (ie buildings) can not be sited over ownership boundaries.

### Land Contamination and Services

1.81 Due to the historic use of the site areas of the Interface Land are likely to be contaminated. Therefore a remediation strategy will need to be produced for any development proposals. Also a number of surface water, foul water and easements exist across the site.

### Character Areas and Zonal Planning

1.82 As a result of the preceding constraints, it has become apparent that a series of distinct areas exist within the Interface Land. These areas are set out below and form the basis of the emerging development framework.

- Riverside;
- North and South Mast Ponds;
- Brunel Way;
- Pembroke Rise; and
- Covered Slip.

### Riverside

1.83 Comprises land reclaimed from the River Medway circa 1840 and historically housed no substantial buildings. Due to its location to the north west of the site and distance from the Historic Dockyard, this area contains valuable development land which can optimise the proximity to the River and the views afforded.

### North and South Mast Ponds

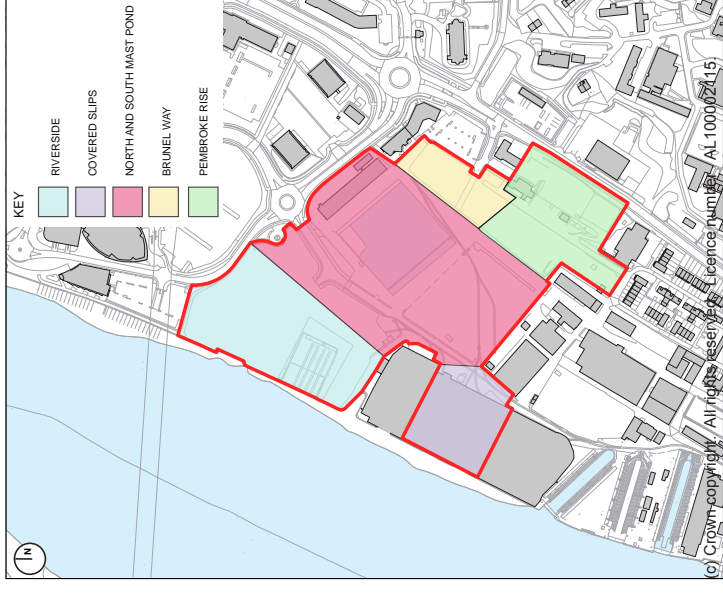
1.84 Currently a hostile open space disconnected from the Historic Dockyard. There is the opportunity to make this location the heart of any new development and a future landmark space for visitors and occupants alike.

### Brunel Way

1.85 In recent history, this location housed a large office complex associated with the Naval Dockyard. New commercial development has since been reintroduced on the periphery and the opportunity exists to create a commercial centre adjacent to the historic buildings and features.

### Pembroke Rise

1.86 This part of the Historic Dockyard was closely linked to the age of sail and the introduction of new technologies. Now largely vacant the land offers the opportunity for re-use and the introduction of new forms of development which utilise the raised land and vistas.



Plan showing Character Zones

### Covered Slips

1.87 These historic, dominant features present the opportunity for conversion to a range of uses.



### Future Development Requirements

- 1.88 It is important to promote development of an appropriate scale and character. However, due to the setting of the Interface Land it is essential that flexible and responsive policies are included to enable economic and site-specific circumstances to be fully taken into account. This strategic approach and need for flexibility reflects the balance between achieving objectives of massing through mixed-use development and acknowledging heritage constraints and the heritage assets.
- 1.89 In order to respect and enhance the urban environment and the character of the surrounding area, the Interface Land buildings should respect the industrial heritage by being simple in footprint and outline and being of an appropriate scale. Also any development proposals should consider high standards of sustainable design and construction such as reusing demolition and construction waste, promoting sustainable urban drainage systems and water efficiency within the Interface Land.
- 1.90 Flexibility is important in considering the form of residential accommodation. In parallel with market and affordable housing student accommodation should also be considered to support the growth of the higher education sector adjacent to, and within the Dockyard.
- 1.91 This document has been prepared to make the most efficient and effective use of land and buildings. It has been developed to avoid designating sites for single or restricted Use Classes wherever possible, instead planning for and facilitate a supply of land which can cater for

residential, business and employment needs but is flexible enough to be responsive to a changing economy or new business requirements.

- 1.92 High quality urban design and architecture must be achieved to ensure integration with the, and protection of the setting of the Historic Dockyard.

### Land use

- 1.93 The site lends itself well to a range of uses but the final mix needs to reflect its key gateway role. The site needs to be welcoming and busy – not closed. Over domination by a single use should be avoided as this would not create a feeling of life and vitality that is required.

- 1.94 Residential building types could include high quality waterfront apartments, 'lifestyle' units associated with the nearby marina, live/work units, and purpose built student accommodation.

- 1.95 Commercial/employment could include high quality offices, studio space, marine related activities such as chandlery, commercial leisure and at a limited scale, higher quality workshop accommodation.

- 1.96 There is also potential for either 'overflow' university accommodation for the Medway Campus and/or a new campus for the Medway Universities.

- 1.97 Neighbourhood services complementing those south of Basin 1 and helping to meet the day-to-day needs of the substantial residential and employment communities within the CHD and Chatham Maritime would be beneficial.

### Residential Development

- 1.98 In order to create a sustainable and inclusive community, it is essential that any residential development proposed for the Interface Land provides the appropriate quantum of affordable housing; the Council will seek to achieve its target policy of 25%. It is important to ensure that there is an appropriate mix of tenure and both house types and sizes provided to meet the needs of different types of households and to encourage the development of mixed communities.

- 1.99 A strategic approach will be taken towards the provision and location of affordable housing within the Interface Land.

### Economic and Commercial Development

- 1.100 It is important that the design of new commercial development addresses the challenges of climate change and the pressures on the natural and historic environment.

### Higher and Further Education

- 1.101 The Interface Land has previously been linked to higher and further education through local planning policy. This form of development is encouraged through future flexibility of policy and will assist the vision to develop a major university complex within Medway. This could be linked in with potential community uses and promote the development of local cultural and creative industries.

**Social Infrastructure**

- 1.102 To minimise the need to travel and to create inclusive communities, the proposed development should seek to include key ancillary facilities which are not available in the immediate vicinity of the site, this includes convenience retail facilities and supporting community facilities.

**Contaminated Land**

- 1.103 Contaminated land is a material consideration under the land use planning process. Given the site's industrial past, there is the potential for the site to be affected by the presence of contamination. Where contamination is present, it is recommended a detailed remediation strategy is developed in agreement with the Environment Agency.
- 1.104 Piling risk assessments will need to be undertaken as well as ground water monitoring as part of future site-specific investigations as detailed proposals are brought forward.



## Purpose of the Development Framework

- 2.1 This Development Framework provides legislative guidance to direct future development and investment for the Interface Land. It provides certainty for the future renewal of the area by presenting clear guidance on the acceptable spatial arrangement and form of redevelopment for the study area. It fulfils the landowner's requirements to enable a gradual transition from the successful new developments of Chatham Maritime to the north and the Historic Dockyard to the south, whilst reflecting heritage awareness and respect for the sensitivities and development constraints of the site.
- 2.2 The Development Framework is a three-dimensional proposal affecting physical, economic and social development. It is expressed as a series of plans, illustrations and through descriptive analysis of the proposed design approach to the future redevelopment.
- 2.3 The Development Framework reflects and responds to the study area's character and historic functions. This will ensure the development of a sense of identity for the place and contribute to the character and richness of any new development.



Photo showing the Brunel Saw mill



Photo showing the Covered Slip No. 7

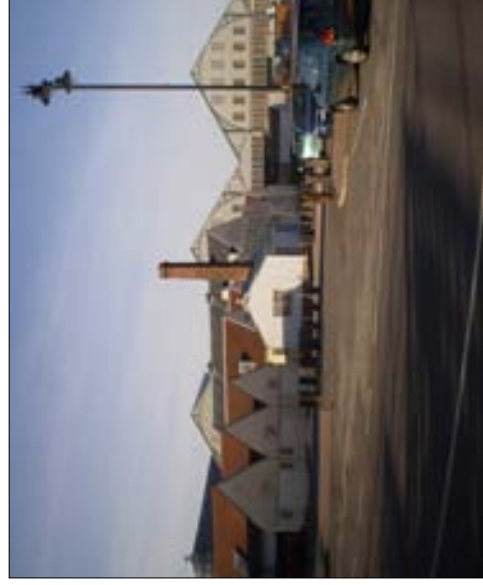


Photo showing Wheelright's Shop, the Old Forge and the Mould Loft in front of Covered Slips 4, 5 and 6.



Photo showing the Police House

**How the Development Framework has been Evolved**

- 2.4 The Development Framework has been evolved as a collaborative exercise by a team including: CZWG who are the principal Masterplanners; WSP who advised on transport, flood risk and environmental elements and also carried out the Sustainability Appraisal, published in parallel to this SPD; and Turley Associates dealing with Planning, Urban Design and Heritage issues.
- 2.5 SEEDA, Chatham Historic Dockyard Trust, Medway Council, and English Heritage have been consulted on the proposals, with the latter advising on all heritage issues. There have been a series of workshops with these bodies to develop the Development Framework and respond to issues as part of an iterative process.
- 2.6 The entire Historic Dockyard is a site of national archaeological importance and a number of key features, have been identified within the Interface Land.

- 2.7 The Development Framework reflects and responds to the infrastructure of the heritage buildings, the industrial past and the development of pioneering technology. It also fixes the key urban design principles that will shape the form and layout of the site.
- 2.8 Any future planning applications for development within the Interface Land will be tested against this Development Framework. Failure to comply with this SPD or to demonstrate that the design solution is equal or superior to the Development Framework is likely to result in a refusal of planning consent. To this end any planning application for this site must be accompanied by a thorough design statement that explains how it complies with, or departs from the SPD, the Development Framework and the design parameters.

**Key Features of the Interface Land**

**Internal**

- South and North Mast Ponds
- Covered Slips 5 and 6
- Brunel Canal route and features
- Lower Boat House
- Police House
- Timber saw pits and houses
- Former timber and stacking yards
- Original 18th century sea wall
- Original 18th century dockyard wall
- 19th century boat slip
- Existing dockyard wall
- Visual connections

**External**

- Covered Slips 3, 4 and 7
- Brunel Saw Mill
- Mast House and Mould Loft
- The wider Historic Dockyard
- Visual connections



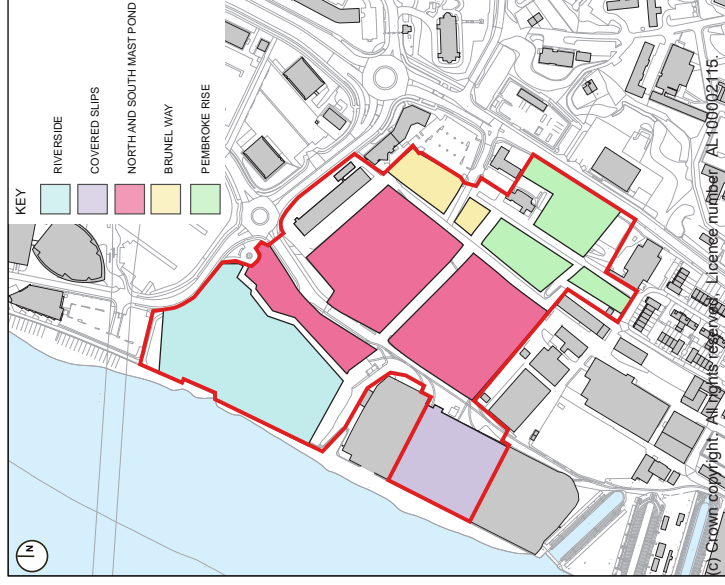
### Overarching Organising Principles

2.9 The Development Framework Plan provides structure that demonstrates how the development plots for the site are arranged. These have been shaped by the existing constraints, identified in the previous section, and proposed organising principles. The latter is defined as principles that when applied will contribute towards achieving the SPD's vision for the development proposals within the Interface Land.

In summary the key organising principles are:

- protection and enhancement of the significance of the heritage features;
- protection of the strategic and local views;
- flexibility of use, massing and height parameters;
- permeable movement and parking requirements;
- public realm treatment; and
- drainage and flooding management .

These are explained in the following sections.



Plan showing the emerging Development Framework

### Heritage Enhancement

- 2.10 The 'Chatham Dockyard and its Defences World Heritage Site Management Plan' provides a framework for understanding how the development site relates to the proposed World Heritage status. The heritage enhancement of the Interface Land has the opportunity to bring a significant derelict part of the dockyard back in to positive use and can provide an exemplar framework of how contemporary developments can enhance the proposed World Heritage Site.
- 2.11 Future development should consider the setting of heritage assets both within and outside the Interface Land and also reflect and respond to identified underground historical features. Where appropriate these features should be used to create focal points. This will help to reinforce the sense of place and local distinctiveness.
- 2.12 PPS5 contains specific policy relating to the setting of heritage assets which should be considered as detailed proposals emerge. A recent publication by English Heritage 'The Setting of Heritage Assets' should also be considered (at the time of production of the SPD, this is currently out for consultation).
- 2.13 Future planning applications for development at the site should include archaeological mitigation strategies to balance the appropriateness of safeguarding through preservation in situ with the alternative of excavation. These should cover each of the five emerging character zones.

**Protecting Strategic and Local Views**

2.14 The strategic views and sensitive local views are a significant factor in recommending the design solution for the site. These views are assessed in the Building Heights Assessment (and illustrated on page 12 of this document, appended to this SPD. It must be recognised that every view across the site cannot be protected but needs to be considered through future design.

**Flexibility of Use, Massing and Height**

2.15 Development within the Interface Land should relate to the scale, height and massing that exists within the surrounding area. Block resolution, namely shape and size of blocks, has not been defined through the Development Framework. Instead, development plots in yellow have been shown. These have emerged through the contextual analysis and development constraints identified as part of this process. It is also important that heights are not consistently applied across the site or across development blocks in order to provide a varied roofscape.

2.16 As highlighted in the Strategic Framework, at this stage it is essential that emerging character zones are considered for a range of land uses including commercial, residential, educational, community or leisure. The economic conditions are likely to dictate future land use and through detailed design it is hoped future development will have the freedom to explore imaginative solutions that fit.

**Movement and Parking**

2.17 The street network should be legible and connect with surrounding areas. The existing road network will not change greatly with the primary access from Main Gate Road, from Western Avenue, and limited access from Brunel Way and Dock Road.

2.18 Streets should be designed to encourage walking, provide platforms for social interaction and facilitate the safe interaction between pedestrians, cyclists and cars. Service vehicles should be subservient to pedestrians and cyclists.

2.19 Parking needs to be carefully located within the development and tightly controlled. This will ensure that vehicles do not become the dominant feature within the street. On this sensitive site a demand based approach to establishing car parking levels should be adopted to minimise the adverse effects of over or under provision of parking on the quality of the development. Parking provision should be convenient to the users so that parking will be used as intended.

**Public Realm**

2.20 Public realm treatments will have the opportunity to enhance the Interface Land development. An integrated strategy for the public realm, including lighting, street furniture, signage and hard and soft landscaping should be developed for the whole of the Interface Land. Innovative public realm treatments will be expected in order to complement the specified high quality buildings.

2.21 Consideration must be given to the boundary treatments defining public and private land, an example of which is the land owned by the Historic Dockyard Trust.

**Drainage and Flooding**

2.22 As has been previously identified, parts of the site are within Flood Zones 3 and 2, therefore the development will need to ensure that flood risk on and off site is mitigated in line with advice in PPS25 and from the Environment Agency. The drainage of the site should follow environmentally sound principles including, where possible, the use of Sustainable Urban Drainage Systems (SUDS) as, for example, permeable paving, green roofs, rainwater harvesting and grey water recycling.

2.23 Water efficiency measures should be considered in future detailed design. All developments should aspire to incorporate community water harvesting and re-use systems to achieve greater standards of efficiency.



### Riverside

2.24 The Riverside area is located to the northwest of the Interface Land, adjacent to the primary entrance off Western Avenue and adjacent to the River Medway. The land, which was reclaimed in the 18th century, has no tradition of substantial buildings or built form and is likely to have lesser archaeological significance to the land which formed the original Dockyard, therefore offers an opportunity for development that does not have a close historical precedent to the Dockyard.

Intensive archaeological investigation is highly likely to be required east of the line of the former river frontage.

2.25 The riverside presents the opportunity to animate a current dormant waterfront edge through a viable residential led mixed-use development. This use, along with the Riverside Walk which will connect with the rest of the Chatham Maritime Riverside Walk, will provide activity and open up the site around the river's edge whilst reaffirming the historic relationship between the river and the waterfront of the original Dockyard. Through the orientation of buildings the area also has the opportunity to define the Main Gate Road and create a principal entrance to the Dockyard for visitors and occupants alike.

2.26 Any development within this zone must not harm the current dominance of the Covered Slips, especially in views from across the River Medway, Upnor and Upnor Castle. The mass and scale of the buildings must also allow views from the west to the ridgeline to the east, whilst respecting the grain of the area.

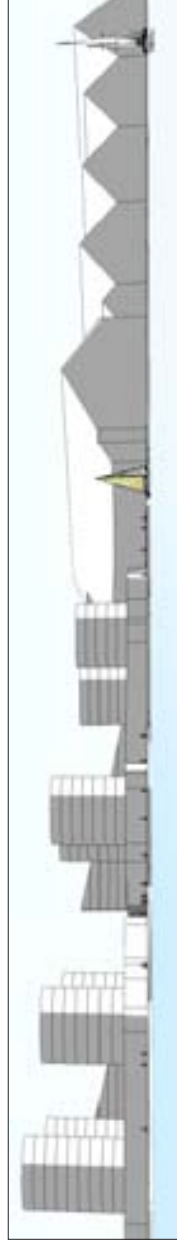
2.27 The alignment of any future development is a key design consideration, and the orientation of buildings must be encouraged to maximise natural light. In addition, the North Mast Pond

The design guiding objectives for the Riverside area are as follows:

- Strong gateway into the Historic Dockyard from the Interface Land.
- Proposed development must respect the scale of the Covered Slips as the dominant feature within current views. In addition, pedestrian access must be retained along the River Medway.
- Proposed flood mitigation measures must feature in future detailed design.
- The river is fundamental to the historic use of the Dockyard and therefore should retain a connection with the other development zones.
- The North Mast Pond drain should be protected and where necessary upgraded to ensure the River Medway continues to drain the pond.
- Provision of a Riverside Walk to link in with the existing footpath.

drain must be retained and linked to the River Medway.

2.28 The 18th century Dock wall should be retained in situ and recognised through alignment and design. It is recommended that the wall is protected from below-ground piling and



Indicative view into the Riverside Zone from the River Medway

development, and waterlogged conditions are retained to avoid the future unseen decay of the wall.

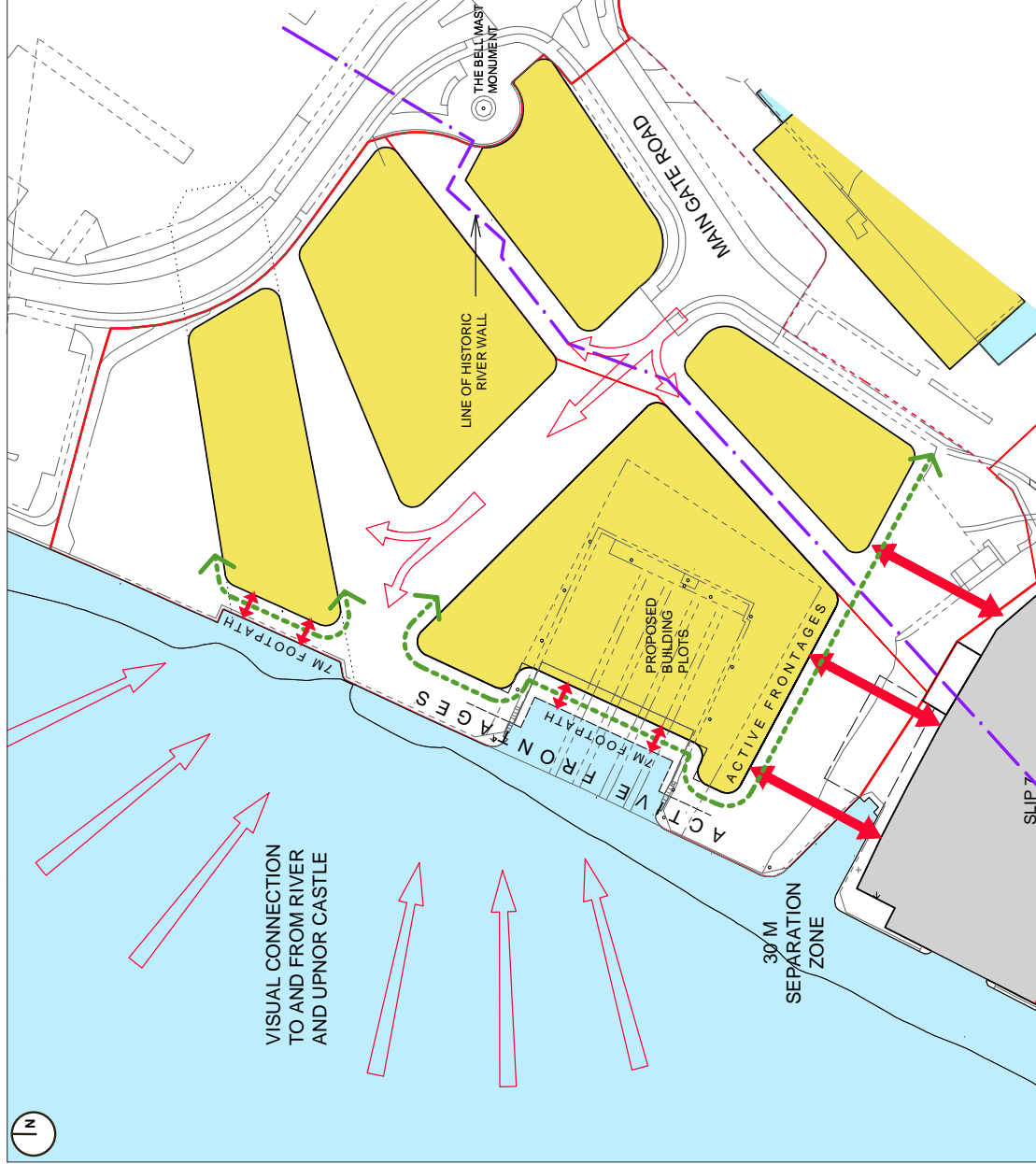
2.29 This area will accommodate the densest form of development within the site, it is therefore essential to ensure the built form does not detract from the existing views, both internal and external and is sympathetic to adjacent Slip buildings.

2.30 The overall building form should be broken up into individual buildings of different heights so as not to be too monolithic and not to compete with the covered slips. In addition views through and past the buildings to the green backdrop of Brompton Lines beyond will be important.

2.31 A building height assessment undertaken as part of this SPD exercise concluded that the setting of the Covered Slips should be protected by a no build buffer zone of 30 m, taken because this is the equivalent height of the Slips. It is recommended that development should step up away from the Slip buildings and that the height of the buildings nearest the Covered Slip should be related to the height of the eaves of the Slip.

### Parameters

Land Use	Residential led mixed use development of up to 65,000 sq m
Building Heights	Up to ten single storeys
Massing	Large to medium scale



Indicative view from the Medway onto the Riverside Zone



Late 19th century slip



Development opportunities within the Riverside Zone

View from the south west corner of the Riverside Zone to Upnor/Upnor Castle

### North and South Mast Ponds

- 2.32 Chatham is unique in having two Mast Ponds and whilst it is not a requirement to restore the South Pond to its original sunken and wet state; its historic use needs to be acknowledged within the future development proposals.
- 2.33 The North Mast Pond will be retained in its existing setting and will form a dominant feature of the Interface Land. It is important that it continues to use the River Medway for drainage.
- 2.34 New buildings will respect the scale of the adjacent Covered Slips and Mould Loft and boat houses. Views to the Slips should be retained, where possible, over the building lines and any new development.
- 2.35 The Lower Boat House is the only timber framed storage shed remaining in close proximity to water and should be recognised accordingly. It should retain its current relationship with the North Pond.
- 2.36 Car parking must be removed from the site of the South Pond to improve the setting of the historic environment. The re-introduction of water is not essential in conserving the South Pond. However future development will acknowledge its previous form.
- 2.37 There is the opportunity to have an area of flexible space within the South Pond. This will retain open views onto the façade of Covered Slip 7, Mast House, Mould Loft and the Galvanising Shop. This space could accommodate public events/summer festivals which will act as a stimulant for visitors to the Dockyard.
- 2.38 The line of the Brunel Canal should form a dominant feature within future development

The design guiding objectives for the North and South Mast Ponds are as follows:

- The two Mast Pond areas should be considered together, but detailed separately.
- Future development should be on a north/south axis.
- Reintroduction of water within the South Pond is not essential, however its historical use must be recognised through design.
- The setting of the Lower Boat House should be protected.
- There should be a visual representation between the Saw Mill and the South Mast Pond, and the river.
- Removal of the Western Avenue fence to encourage connectivity with Chatham Maritime.

proposals. This feature and the Saw Mill have a historic functional relationship with the South Mast Pond and therefore should be visually linked. This should include opening/displaying the shaft of the Canal at the point where it meets the Mast Pond and also the shaft where it links to the Saw Mill.

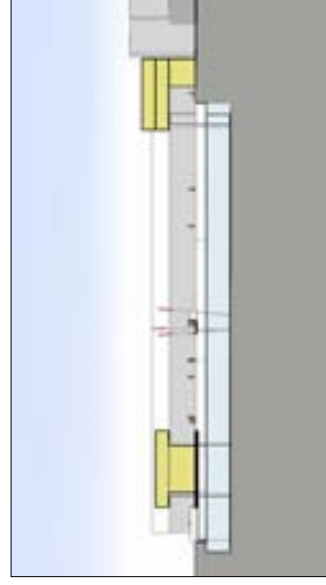
2.39 The proposals should illustrate the Dockyard's timber related past. Buried archaeology, including saw pits, should be investigated and if this cannot be preserved in situ, eg beneath new buildings, it should be recorded. A representative example of saw pits should be revealed and interpreted. Policy HE12 of PPS5 relates to recording of heritage assets which, here, means more than just buried remains

2.40 Future development is recommended to be built on a north/south axis to promote visual linkages with the historic dockyard buildings and the core of the historic dockyard. Pedestrian circulation around the water must be retained through recognised pathways or setting the buildings back from the water's edge. This would ensure that the historic movement pattern and function of the pond is reflected through future design.

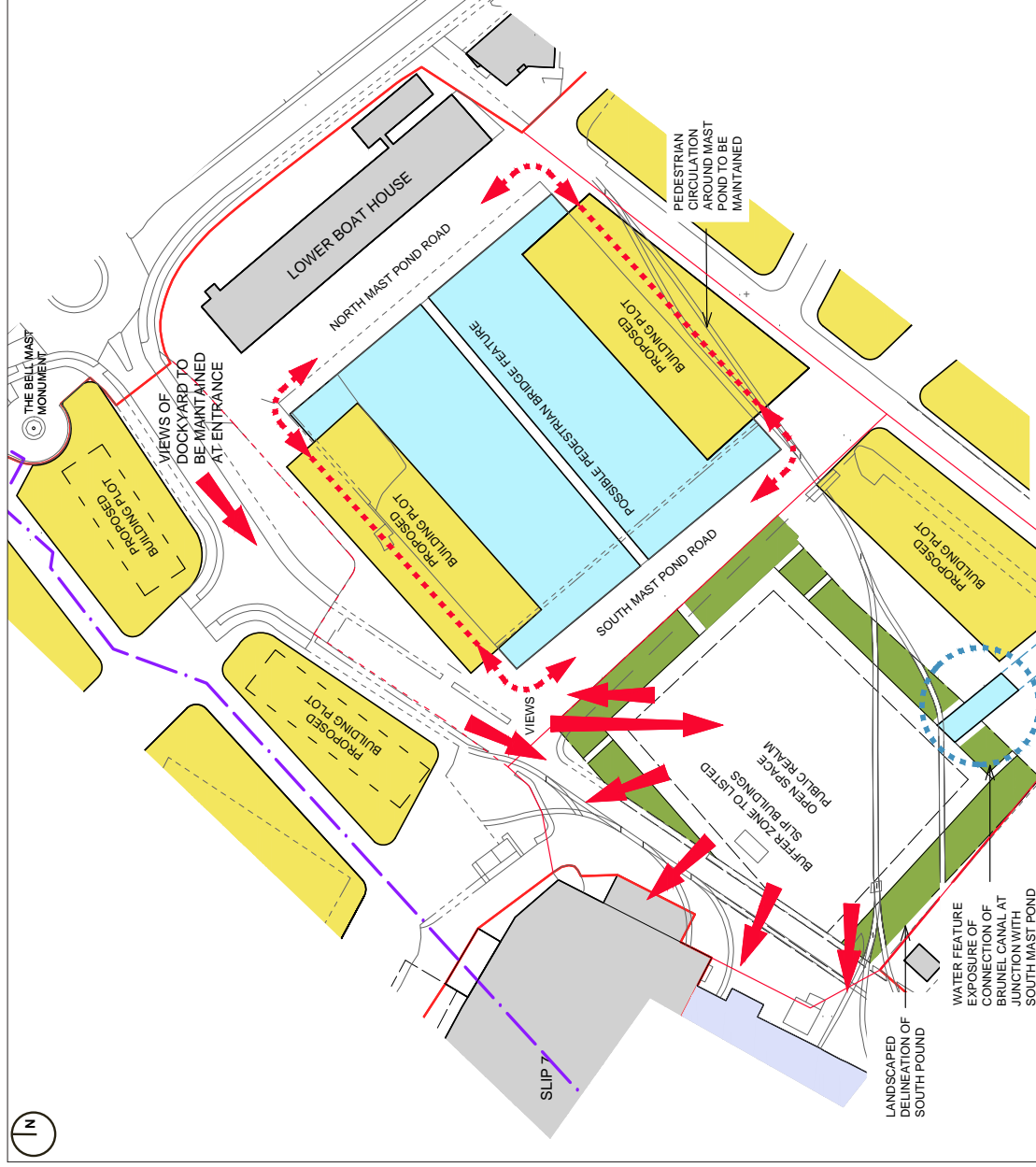
2.41 This area falls within 1 in 200 year tidal flood extent and this would need to be considered as part of the detail design of any future development.

#### Parameters

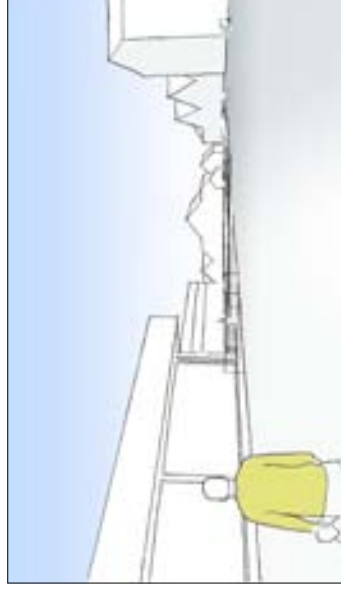
Indicative Land Use	Mixed use development of up to 12,000 sqm
Building Heights	Up to three storeys
Massing	Medium scale



Indicative section through the North Mast Pond (looking north)



North and South Mast Pond Zone - indicative development blocks



Indicative view from Main Gate Road, looking southwards



Indicative view from South Mast Pond Road, looking westwards



Thames Barrier Park, illustrating a form of public realm that could be developed for the South Mast Pond



## Brunel Way

- 2.42 Brunel Way is located to the northeast of the Interface Land. This area is primarily accessed from the Main Gate Road and the South Ponds Road, with limited access from Brunel Way.
- 2.43 It is important that any built development at Brunel Way does not infringe the setting of the North Mast Pond. A further constraint to guiding development is the ownership boundary, across which development can not take place as highlighted in the strategic framework. At this stage, there is therefore no guarantee of unimpeded access from Brunel Way to the Mast Pond area.
- 2.44 The zone should be developed with a largely commercial focus and link in with the adjacent existing office and hotel buildings.
- 2.45 This area was historically used for timber stacking and therefore future development should acknowledge this part of the Dockyard's timber related past. Historically this land also accommodated a number of saw pits. It is therefore recommended that a set of saw pits is conserved and restored as a historic feature of the site (alternatively this could be done within Pembroke Rise).
- 2.46 This area falls within 1 in 1000 year extreme flood extent and this would need to be considered as part of the design of the buildings.

The design guiding objectives for the Brunel Way are as follows:

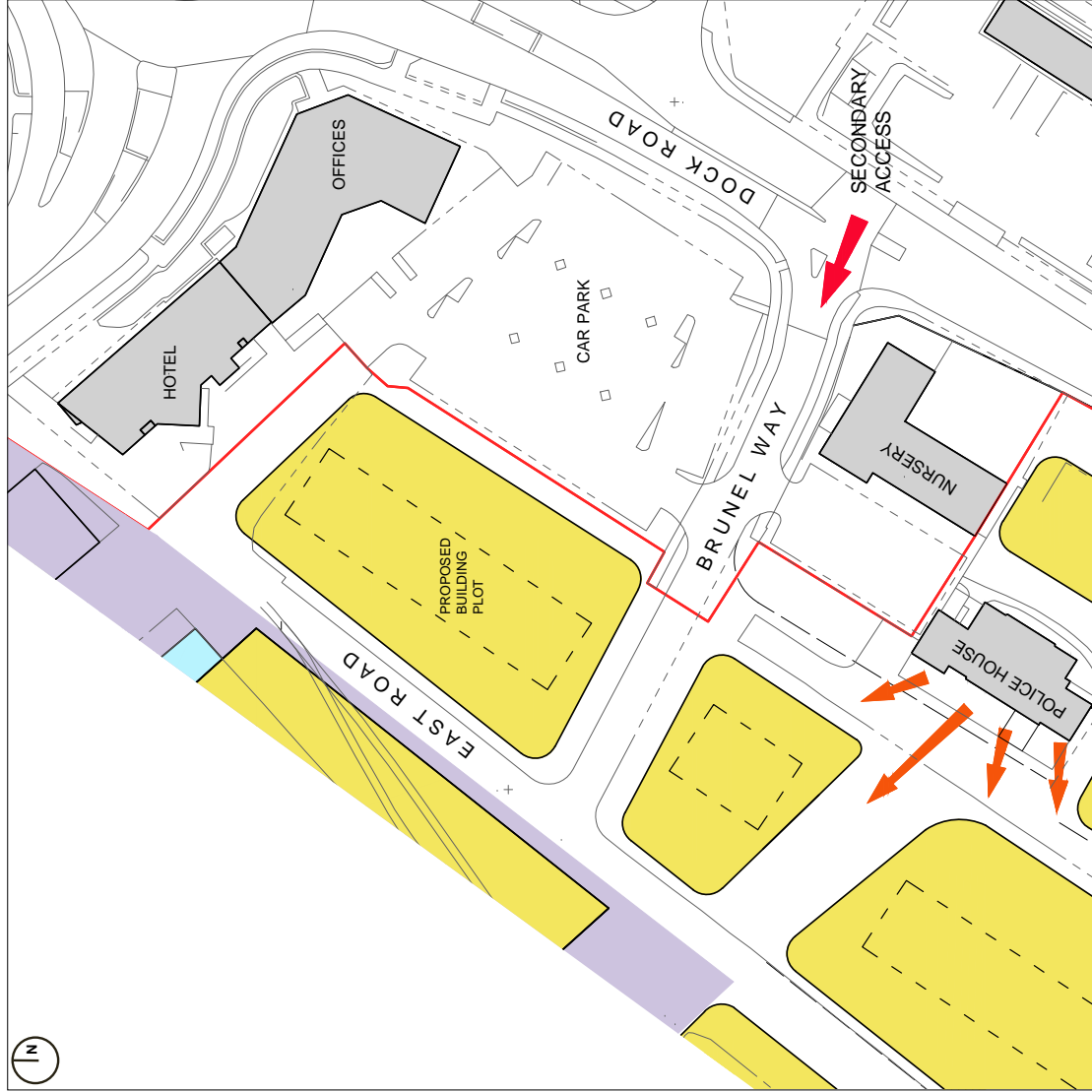
- Predominately commercial in land use and medium scale massing.
- Acknowledge in some form the historic use of the site. This could be represented through design or materials.
- The Police House forms a major landmark in this location; it is important that it is reused as part of any future development proposal.

## Parameters

Indicative Land Use	Commercial of up to 6,000 sq m
Building Heights	Up to four storeys
Massing	Medium scale



Brunel Way, looking southwards



Development opportunities within the Brunel Way Zone showing proposed development blocks



Watermans Place, Leeds and Black Lion Street, Brighton - precedent images



Existing view of the Lower Boat House, the Hotel and Office Block

### Pembroke Rise

2.47 Pembroke Rise is located in the southeast corner of the Interface Land and includes the highest land within the Site. This zone is closely related to the South Mast Pond and has a finer grain to the other zones, to reflect the adjacent southern grain.

2.48 Brunel Sawmill, along with the associated saw pits and timber stacking yards, must be considered together to reflect their historic use (timber would be floated through the canal and tunnel to the shaft at the base of the Sawmill. When the wood was sawn into planks, it was moved by carriage to the stacking yards and seasoning sheds). It is important to integrate the building with the Canal and adjacent stacking yard to maintain its historic illustrative and associative significance.

2.49 The early 18th century Dockyard Wall, as illustrated in the plan on page 12, (with tower houses) forms an underground feature within the site located to the south of the Police House. Whilst, this is not a Scheduled Ancient Monument it is an archaeological historic area of significant interest and illustrates the early Dockyard boundaries. It will be important to highlight this through design features. The existing dock wall will be retained and illustrates the growth associated with the Dockyard during the historic age of sail.

2.50 This area was historically used for timber stacking and therefore future development should acknowledge this part of the Dockyard's timber related past. Historically this land also accommodated a number of saw pits. It is therefore recommended that a set of saw pits is conserved and restored as a historic feature of the site (alternatively this could be done within Brunel).

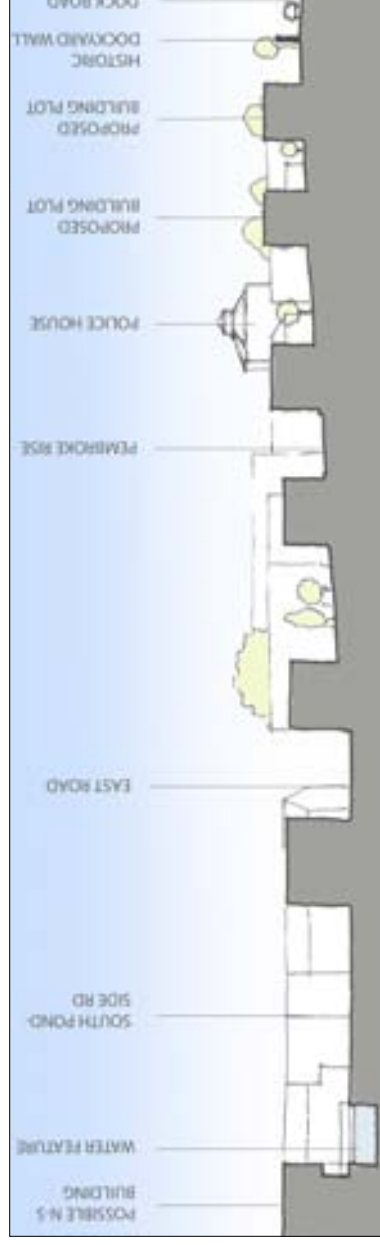
The design guiding objectives for Pembroke Rise are as follows:

- Predominately residential in use presented using traditional housing models.
- The urban form should define the high quality residential in character public realm areas.
- An access point could be established through the existing Dockyard wall.
- Acknowledge the historic timber stacking use of the zone through design and materials.
- Retain the visual link between the Brunel Canal, the Saw Mill and the South Mast pond.
- Consider views from the Police House and how future development could impact upon these.

2.51 There are a number of existing bricked up doorways punctuating the existing Dock Wall. These offer the opportunity to provide a pedestrian link onto the Dock Road without harming the setting of the wall. It is envisaged that only one access point will be necessary and that this could be on the alignment of the Brunel Canal, therefore utilising views through the site to the riverside and historic Slip buildings.

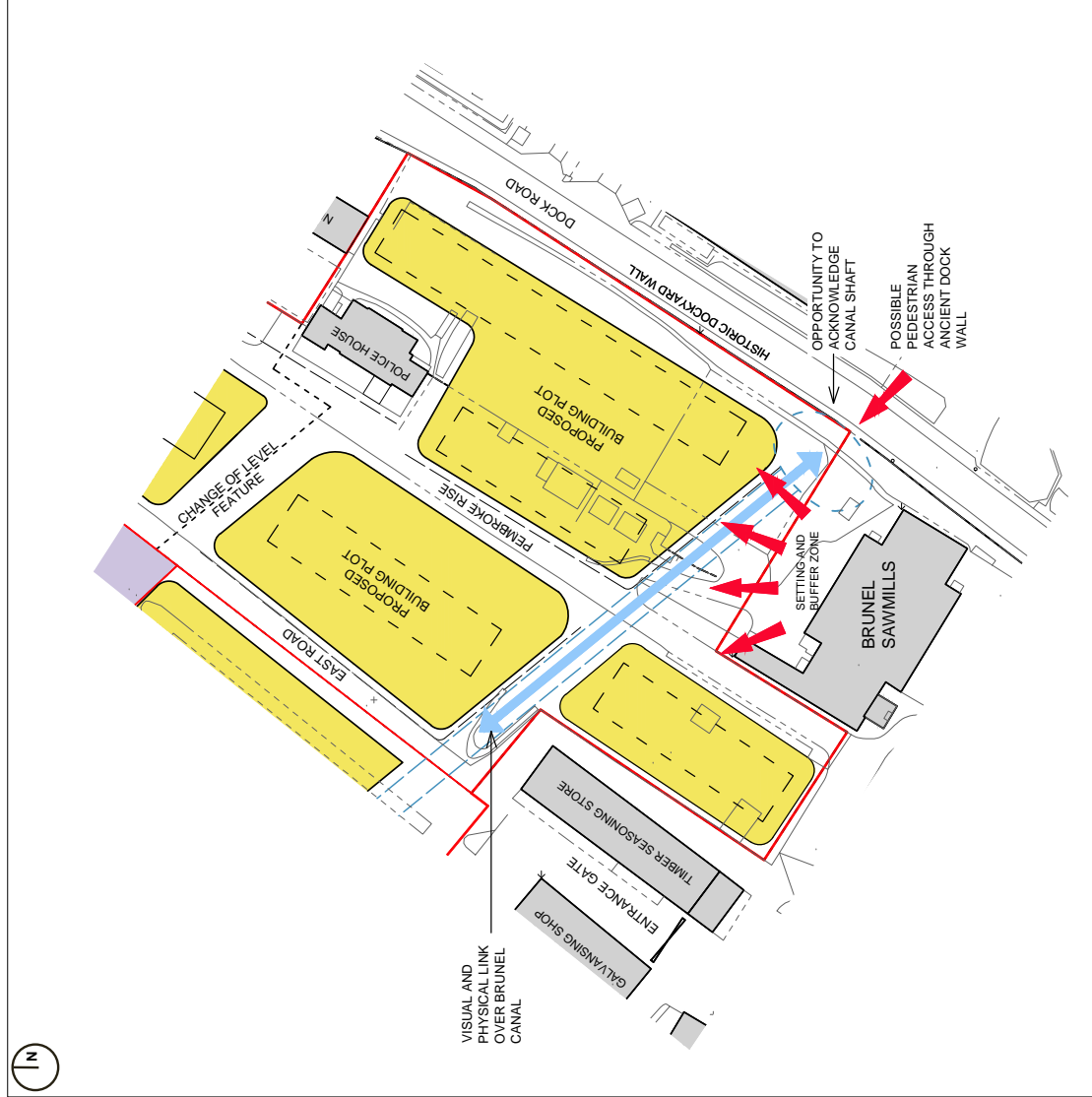
2.52 The listed Police House, which is located to the north of this zone, has never historically had a formal setting within the dockyard and should be carefully assimilated into future development proposals. There are important views from the building and the integrated cupola across the dockyard that should be considered in future masterplanning exercises.

2.53 The zone will be a largely residential area and provide a mix of dwelling types. It falls within 1 in 1000 year extreme flood extent and this would need to be considered as part of the design of the buildings.



Indicative section through the Pembroke Rise Zone





Development opportunities within of the Pembroke Rise Zone

Parameters

Indicative Land Use	Primarily residential up to 14,000 sq m
Building Heights	Up to three storeys
Massing	Medium to small scale



The Police House



Photo showing Uponor Castle from the raised ground a Pembroke Rise

### Covered Slips Buildings

- 2.54 The last major period of construction of Dockyard buildings and structure took place between 1838 and 1855 when the covered Slip buildings were constructed. The Slips illustrate the Dockyard's transformation to wooden ship building under cover and then the introduction of iron ship construction, and finally work on submarines. They form fine examples of the technological advancements that were achieved at Chatham and represent the transition towards machine-based manufacturing and the Industrial Revolution.
- 2.55 The scale and collective volume of the covered Slip buildings ensure they form the dominant feature of the Dockyard fronting the River Medway. It is important that the setting of these buildings is protected in the context of views from the Dockyard and river.
- 2.56 It is recognised that the future development potential of the Slip buildings is limited. However, due to the potential loss of parking on site through the projected redevelopment of the South Mast Pond, the Covered Slip buildings are proposed to be converted to house those car/coach parking spaces which have been displaced from the South Pond. This will be provided through a tiered parking structure and would also accommodate a proportion of the existing Historic Dockyard parking. This would provide the building with a sustainable use, whilst not detracting from its historic interest.
- 2.57 Any refurbishment within the Covered Slips should not detract from the visible form of the cast iron structure. There is also likely to be archaeological remains and therefore

The design guiding objectives for the Cover Slip buildings are as follows:

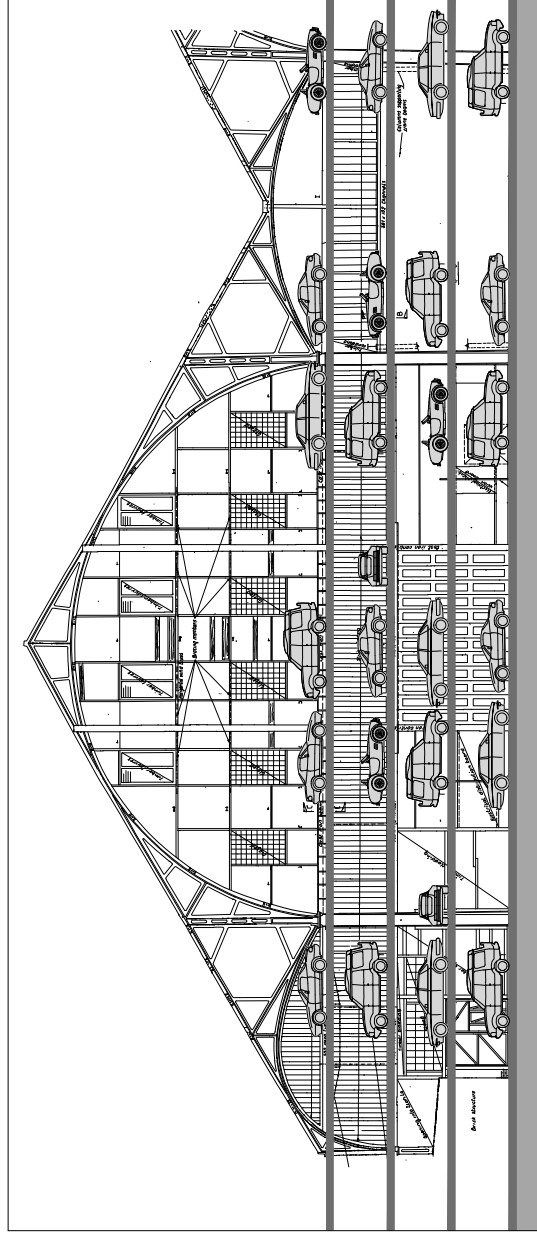
- Any proposed structure should not detract from the scale of the Covered Slip.
- The setting of the Slip buildings should be protected through development guidance for the other zones within the Interface Land, particularly the riverside.
- The view to the Covered Slip buildings should be retained from the principal access along Main Gate Road.
- Flood resistance and resilience measures will need to be considered alongside the future use of the Slip buildings.

foundations could present an issue and should be investigated in greater detail as part of future development proposals.

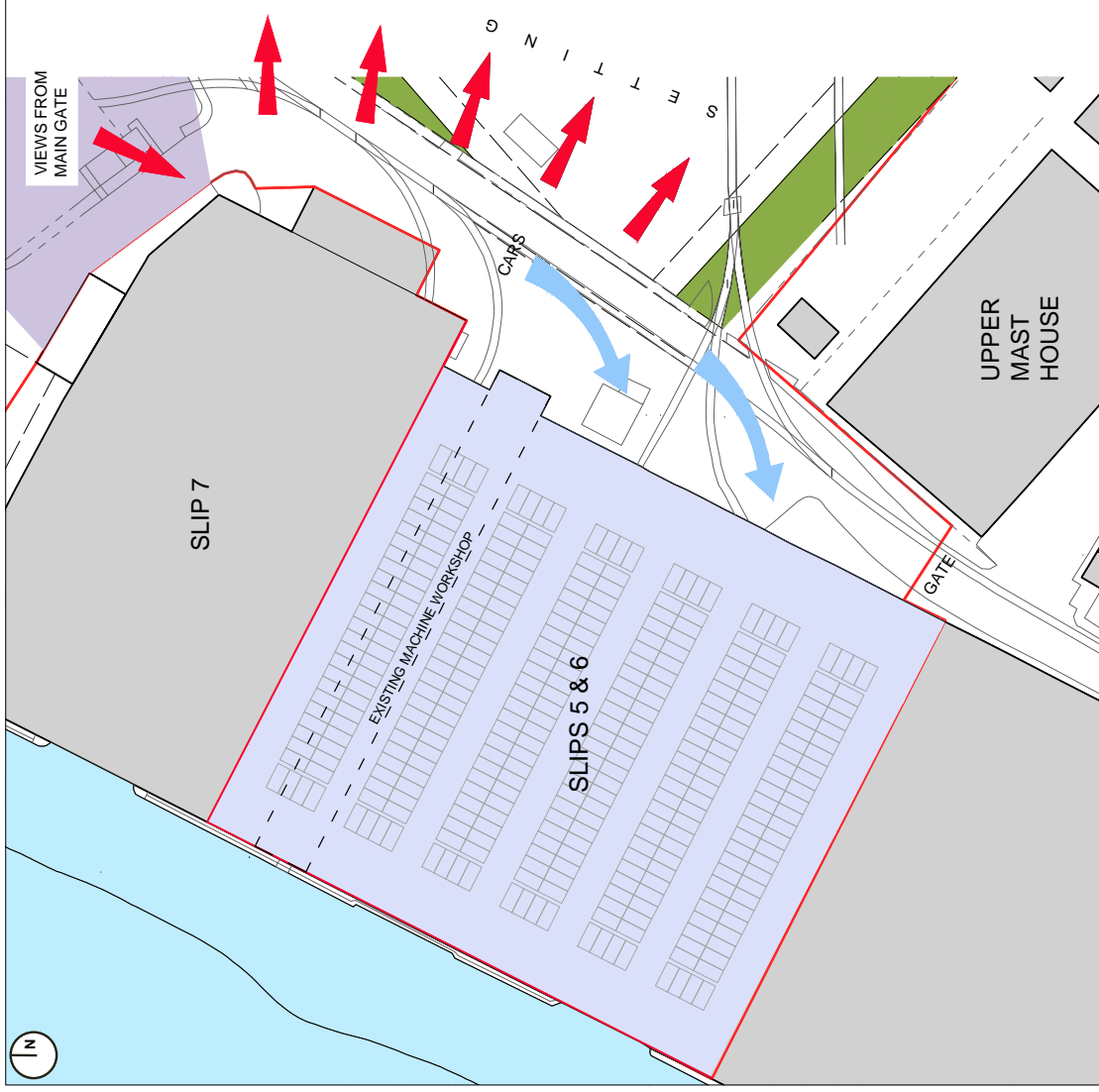
- 2.58 This area falls within 1 in 200 year tidal flood extent and this would need to be considered as part of the design of the structures and refurbishment.

### Parameters

Indicative Land Use	Up to 800 car parking spaces. Parking for twelve coaches
Building Heights	Up to four floors
Massing	Large to medium scale



Indicative section through the Covered Slip



Indicative Layout of the Covered Slips Zone



Covered Slips 4 and 5



Roof structure inside the Covered Slip



Indicative section illustrating tiered car park structure

### Movement Strategy and Parking

#### Development Mix and Travel Plan

2.59 The highway network surrounding the site is under pressure and junctions such as the Wood Street roundabout (south of the site), suffer from congestion at peak times. These would need assessment to determine the extent of improvement required but would need a strong and effective Travel Plan to minimise this impact.

2.60 The Travel Plan will need to demonstrate that significant efforts have been made to minimise the need to travel by car and reduce the impact of car traffic on the surrounding highway network.

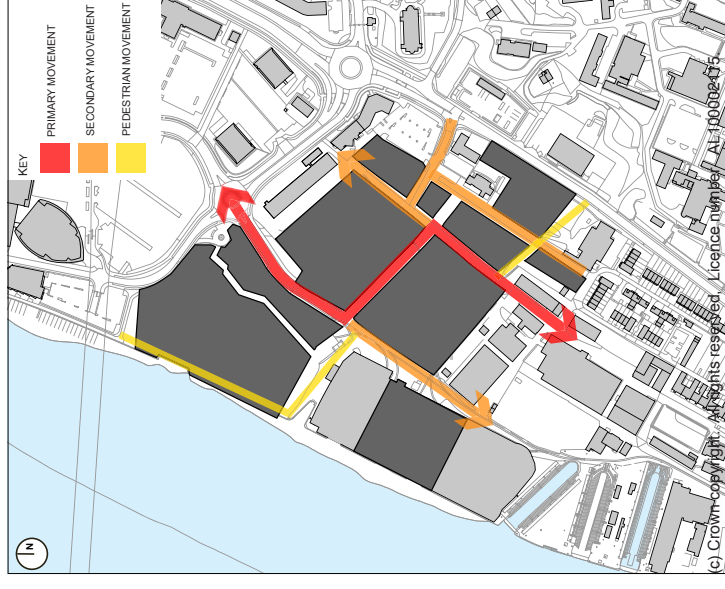
2.61 Should a substantial element of residential development be proposed, a range of complementary uses should be accommodated on the site as listed within the attached Transport Strategy to minimise the need to travel outside the site for most day to day journeys. This should aim to exceed the SEEDA sustainability checklist criteria.

#### Pedestrian and Cycle Access

2.62 Opening an entrance in the Dockyard wall can help to minimise walk distances and maximise the opportunity for walking and cycling from the development. The layout of the site should be designed to accommodate pedestrian and cycle desire lines without significant diversion to maximise opportunities.

Future development should seek to:

- Minimise the need to travel outside of the site.
- Maximise the potential for travel by non-powered modes (foot and cycle) to key local destinations outside of the site.
- Optimise access to sustainable transport networks (bus and rail) and key routes which give access to important nearby areas.
- Minimise the need to travel by car, and especially for single occupancy car journeys.
- Deliver a site layout which provides sufficient parking to meet demand for car ownership at residential uses, and which provides an appropriate amount of car parking at commercial elements of the development and for the existing Dockyard as a visitor attraction, taking into account the other elements of the Transport Strategy.
- Minimise the impact of the proposed development of the surrounding highway infrastructure through the implementation of the Transport Strategy, and mitigation measures to off set residual impacts.
- Due to ownership boundaries, the existing vehicular access points, into both the Interface Land and Historic Dockyard, need to be retained for the existing tenants.



2.63 A review of key pedestrian and cycle routes outside the site should be undertaken and in particular improvements along the Dock Road corridor should be considered to improve access to Chatham and Brompton High Street, which may include improved crossing facilities south of Wood Street roundabout.

#### Site Access

2.64 The main access will be from Main Gate Road with secondary access from Brunel Way and Dock Road. Access through the site from Main



Gate Road to the Historic Dockyard will need to be provided for the significant dwellings and businesses that exist within the area. Access routes should be designed in accordance with Manual for Streets, to have a significant place function, minimising vehicle speeds through integrated design rather than retrofitted calming measures.

**Public Transport**

2.65 Bus access to the site along Dock Road is currently good, and whilst some consolidation or improvement of services may be considered to reduce vehicular travel demand, this is not considered necessary to make the site acceptable for development. It is not expected that services would need to divert into the site. However, improvements to seating and shelters, and consideration of real time information would help increase the attractiveness of this mode. A corridor review of bus priority measures should be considered to identify ways in which public transport services could be made more attractive than car use to access Chatham town centre and the railway station.

**Parking**

2.66 The proposed development should accommodate existing car parking demand together with the needs of new development proposed.

2.67 Local car parking policy is under revision and interim standards advocate minimum rather than maximum standards for residential development. Notwithstanding the interim or any further standards published, a demand based approach to establishing car parking levels should be

adopted on this site to minimise the adverse effects of over or under provision of parking on the quality of the development.

2.68 Parking provision should be convenient to the users so that parking will be used as intended.

2.69 It is anticipated that the Slip building could be used to accommodate replacement parking for some 400 cars and twelve coaches for the Historic Dockyard from the site of the South Mast Pond, along with parking related to development within the Interface Land. Replacement parking for the existing hotel would be required to free up the area to the west of the North Mast Pond.

**Public Realm Proposals**

2.70 A public realm strategy for the Interface Land will ensure that the unique development zones are unified through a similar palette, and that the proposed development will be cohesively linked in with the existing buildings.

2.71 Public realm also offers an opportunity to both fill the voids where buildings once stood and reflect the outline of features that did, or still do, exist underground. This will respond to the historic context of the site. Soft landscaping was not a dominant characteristic within the industrial grain of the Dockyard and should be limited to appropriate locations such as the area to the east.

2.72 Site specific public art should be carefully integrated into the proposals as part of an overall public realm strategy. Also signage should be located at key nodal points within the proposals to enable the user or visitor to orientate themselves within the development.

A series of high level design principles have been developed to inform the development of the public realm strategy and are applicable to each area:

- Adopt a simple, high quality palette of hard landscape materials that balances lifetime costs, aesthetics, durability and sustainability.
- Maximise opportunities to translate the historic features of the Interface Land.
- Create distinctive and incidental spaces defined by and sharing a close relationship with architecture, creating well-defined hard civic spaces and streets whilst integrating green spaces such as courtyards and gardens within the residential and mixed-use areas.

### Design Principles

2.73 In order to achieve the vision for the Interface Land of creating high quality proposals with a distinct character that responds to its surroundings context the following Design Principles have been identified that any proposals should adhere to.

<p><b>Context and Heritage</b></p> <p>The design solutions must be sensitive and respect the historic past of the Interface Land. The study area should also respect the character of the surrounding local context. Where appropriate the design proposals should retain and refurbish buildings rather than rebuild.</p> <p><b>Scale and Proportion</b></p> <p>In order to respect the character of the surrounding area, future development within the Interface Land should respect the industrial heritage by being simple in footprint and outline and being of an appropriate industrial, not domestic, scale. New buildings should form a harmonious group with their older neighbours.</p>	<p>reduce the visual connection to the Covered Slip buildings, which form a predominate feature within the views to the Historic Dockyard; but not at the exclusion of other important views.</p> <p><b>Archaeology</b></p> <p>The study area is of national archaeological importance, and has a number of potential buried remains and archaeology of standing structures. Care needs to be taken to protect any unknown or known underground structures such as the associated tunnel and shafts, the old river wall and the timber Saw Pits associated with the Brunel Saw Mill. For a comprehensive list of the assets within the site, please refer to the various archaeology reports that have been produced for the site.</p>	<p>stage a flexibility in the future use of buildings is encouraged to ensure potential occupiers are not constrained through planning policy and use classes.</p> <p><b>Appearance and Materials</b></p> <p>The need to respect the historic context of the Interface Land will affect the material palette, with the choice and application of materials being an important consideration. The design proposals should reuse and recycle at least 90% of the demolition and construction waste</p>
<p><b>Physical Connectivity</b></p> <p>The Interface Land plays an important part in linking Chatham Maritime and the Historic Dockyard areas. The street network should be legible and connect pedestrians, cyclists and vehicles with surrounding areas, particularly to and from the Historic Dockyard. Shared surfaces should be used, where appropriate.</p> <p><b>Visual Connectivity</b></p> <p>The redevelopment of the Interface Land must not</p>	<p><b>Highest Design Quality</b></p> <p>Design proposals should preserve and enhance the setting of Interface Land. They will have to be of the highest design quality, be valued enough to keep for future generations and must be adaptable to a variety of uses over time.</p> <p><b>Uses</b></p> <p>Suitable levels of a compatible mix of uses will be encouraged within the Interface Land to create a high-density, sustainable development. At this</p>	<p><b>Flexibility and Inclusive Design</b></p> <p>New development should be designed to allow maximum flexibility to ensure that it can accommodate the changing needs of its users over time. Also it should meet the standards of accessibility and inclusion.</p> <p><b>Open space</b></p> <p>The open space areas within the Interface Land, including the North and South Mast Ponds, should connect with the surrounding green infrastructure to encourage both public access and biodiversity links. This could be addressed through the use of green roofs and walls.</p>



### Sustainability

To reduce the impact of the development good practice sustainability principles should be applied to the Interface Land. This will include achieving high sustainability standards, innovative design, technology and construction solutions to climate change challenges.

### Secure by Design

Security provision for internal and external environments needs to be appropriate for scheme locations and building types and should reflect advice from the Police Architectural Liaison Officer prior to the detailed planning stage. Active frontages and other natural surveillance measures should be considered as part of the future development of the Interface Land.

### Reuse of Historical Buildings

The reuse of historic buildings is promoted within the site to provide a clear link to the past and help reinforce a sense of place, local identity and character. This site will need to change in future in ways that we cannot yet predict and so individual heritage assets, where appropriate, and the site in general should retain maximum flexibility in order to increase the chances of having a sustainable future.

### Development Framework Summary

- 2.74 The development framework provides guidance on the spatial arrangement and form the proposal should take within the Interface Land. It also addresses how the identified vision for the Interface Land can be achieved through the consideration of the identified constraints, the overarching organising principles, the character zone typologies and the design principles.

## The Need for a Delivery Strategy

- 3.1 The Supplementary Planning Document (SPD) provides the opportunity to ‘kick-start’ the development process and that it can be used by the Council and its partners as a promotional tool in addition to its use in determining planning applications through the Development Control process.
- 3.2 This part of the SPD sets out the delivery strategy for Chatham Dockyard Interface Land. The purpose of such a strategy should be mentioned:
- to ensure that common parts of the public realm necessary for the success of the whole scheme are delivered at the appropriate time;
  - to safeguard and bring back into use heritage assets as a part of the overall development.
  - to ensure co-ordinated flood risk mitigation measures?
- 3.3 An Implementation Plan is required in order to ensure that the Spatial Masterplan contained in this SPD is achieved on the ground.

## Highlighting the Key Roles and Responsibilities of all Key Public and Private Sector Organisations

- 3.4 While the identification of appropriate delivery mechanisms is clearly an important issue, it is equally (if not more) important to ensure the commitment of the key stakeholders within the regeneration sector. As part of the process of securing the commitment of the key stakeholders

it will be fundamental to identify their respective roles and responsibilities.

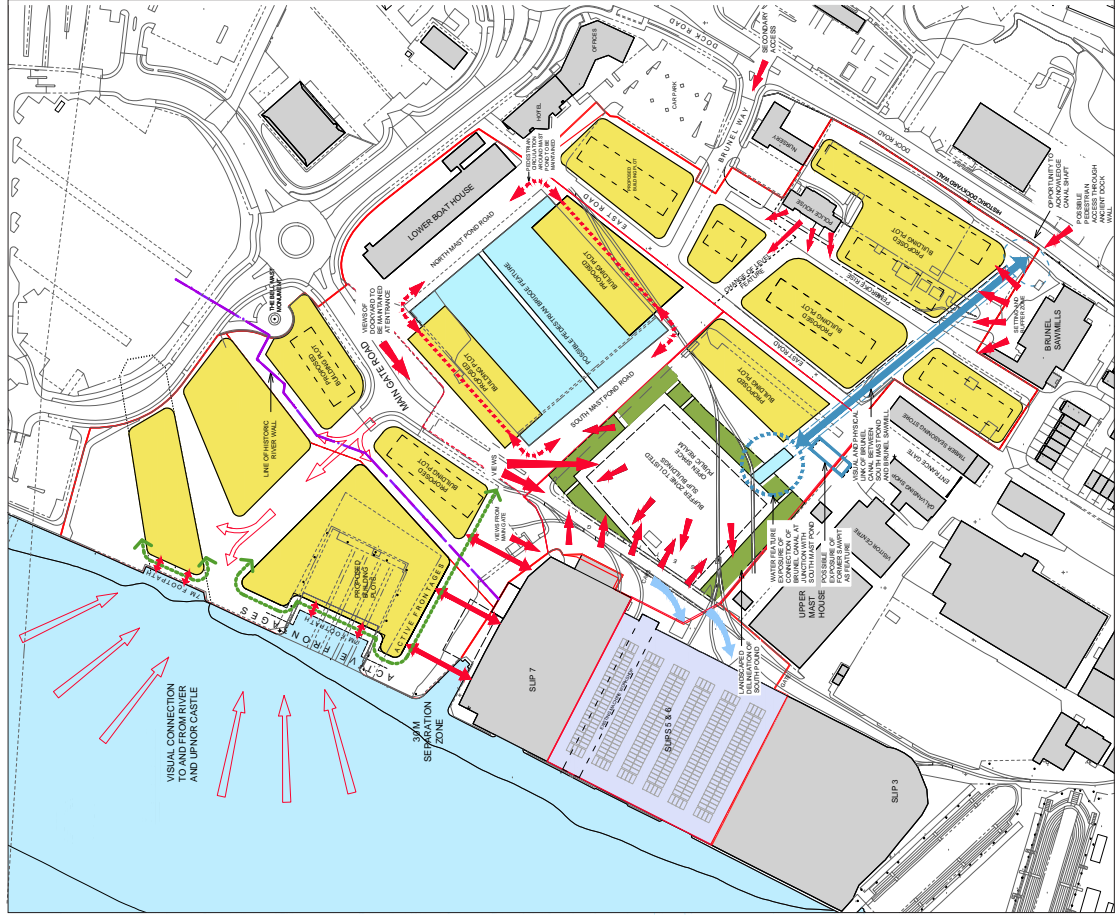
- 3.5 As local planning authority, Medway Council has an integral part in the implementation of future development proposals.

## Statutory Consultees

- 3.6 English Heritage has a particular interest in the Interface Land, as the site encompasses a location of international maritime heritage significance. English Heritage has taken a positive and proactive role in the recent strategic masterplanning exercise and the preparation of this SPD. The close involvement of English Heritage will continue with regard to the emerging detailed proposals.
- 3.7 CABE also has a key role to play regarding the Interface Land. This SPD has been influenced by the CABE guidance: Creating Successful Masterplans – Guide for Clients. It is also expected that significant development proposals for the Interface Land will be the subject of CABE and local Kent Architectural Panel’s Design Reviews.
- 3.8 The Environment Agency clearly has a role with regard to the redevelopment of this significant riverside site. The EAs guidance has been sought with regard to this Masterplan, and should also be engaged with at the pre-application stage.
- 3.9 Natural England should also be consulted by applicants at the pre-application stage.

## Phasing Strategy

- 3.10 The Council supports the principle of a phased approach to the delivery of comprehensive development for the Interface Land. It is unlikely that a single ‘one size fits all’ delivery mechanism will be appropriate and effective across all the sites within the strategic masterplan area. As set out in the preceding sections, the sites have a number of discrete physical or ‘character’ areas which may face different regeneration issues and challenges, and which may require the intervention and support of a range of different organisations or partners.
- 3.11 Future development proposals should be subject to a detailed comprehensive phasing strategy and implementation plan. Where possible we would like sites bought forward in parallel, to ensure investment is shared and an enabling development principle is established to ensure successful public realm and heritage protection. Full details of this will be included within the phasing and implementation strategy.
- 3.12 This strategy should be approved with Medway Council prior to the submission of any planning application for the development of any part of the site.
- 3.13 As the Pembroke Rise zonal plan illustrates, the proposals for the Brunel canal fall within the Pembroke Rise character area as well as the southern Mast Pond, and should be designed to be delivered in a comprehensive manner
- 3.14 The connection of the existing Riverside Walk to the north into the Site is an important part of the reconnection of two parts of the former Dockyard needs to be guaranteed as part of the Implementation Plan. Whilst it is recognised this will be delivered as part of the Riverside



Site wide development plots plan

area, should other zones come forward earlier, solutions towards this path should be sought.

3.15 The Riverside land is commonly regarded as being the most valuable in development terms, however the Riverside Development Zone will not achieve its full potential unless the public realm through which residents and visitors alike must enter the Riverside land has been brought to the necessary standard. The Mast Pond Drain runs through from the Northern Mast Pond to the river, and its continued function is vital for the Mast Pond. The Riverside Character Area should therefore be developed only after the Mast Ponds Character Areas have been developed.

3.16 In the interim, as detailed design proposals are developed, it is recommended that flexibility is applied to allow temporary uses within the site. In the past the site has been used for a number of income generating uses such as Park and Ride and parking for external events within Medway. These uses should be continued to ensure the landowners generate additional income which cross subsidise other costs including security.

### Developer Contributions

3.17 Planning obligations are normally entered into in accordance with Section 106 of the Town and Country Planning Act 1990 and:

- restricting the development or use of land;
- requiring specified operations or activities to be carried out in, on, under or over the

land;

- requiring the land to be used in a specified way; and
- requiring a sum or sums to be paid to the local planning authority on a specified date or dates, or periodically.

3.18 Government policy is set out in Circular 05/2005. The Circular reiterates previous guidance that planning obligations should only be sought where they are:

- relevant to planning;
- necessary to make the proposed development acceptable in planning terms;
- directly related to the proposed development;
- fairly and reasonably related in scale and kind to the proposed development;
- and reasonable in all other respects.

3.19 Following the Community Infrastructure Levy Regulations 2010, planning obligations must be directly related to development. The Regulations state that any Section 106 is:

- necessary to make the development acceptable in planning terms;
- directly related to the development; and
- fairly and reasonably related in scale

and kind.

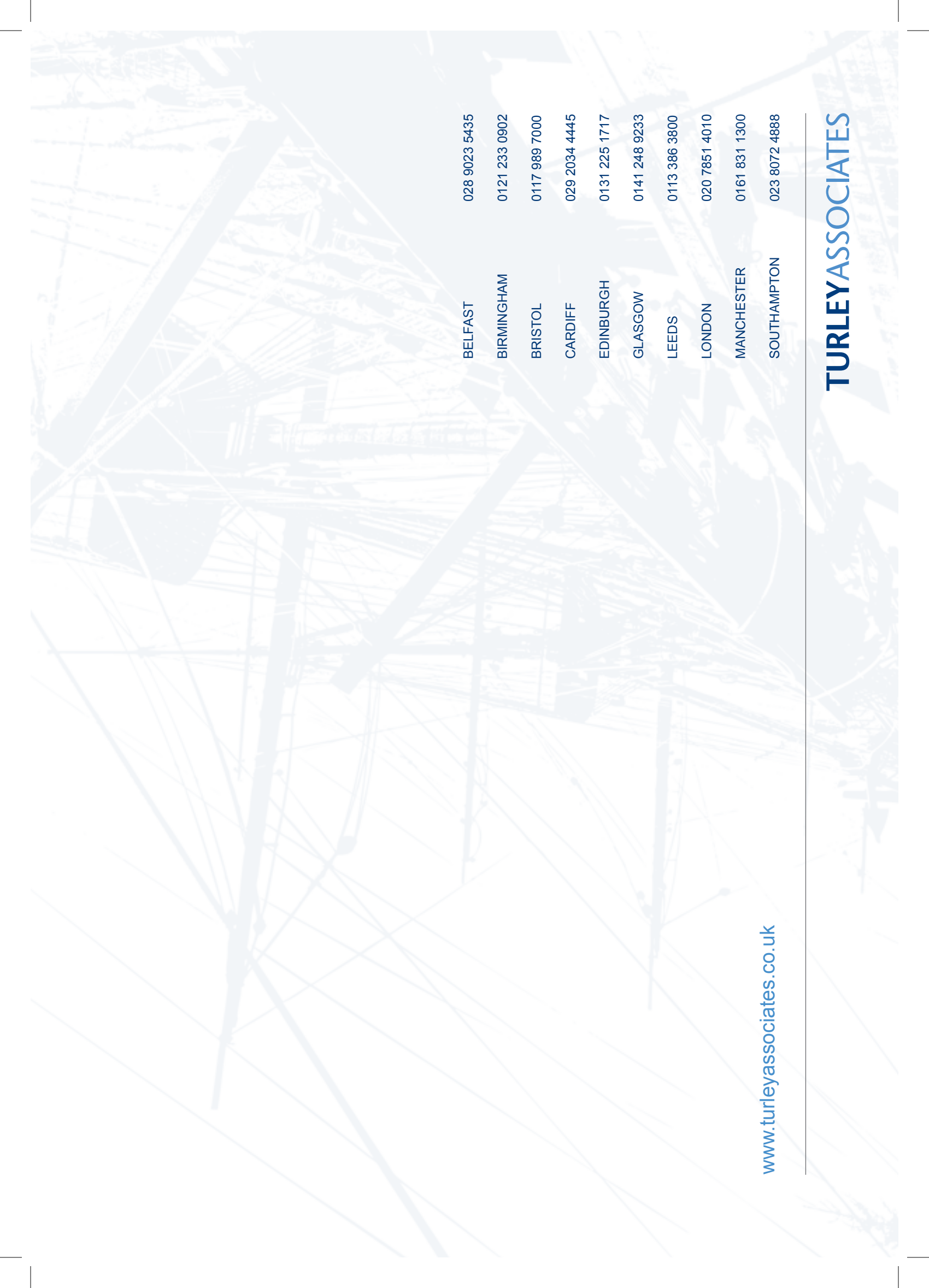
3.20 Medway Council accept that the region is currently experiencing major growth and development through a number of regeneration projects. This has resulted in the Council publishing a Guide to Developer Contributions; a Supplementary Planning Document which provides comprehensive advice on how to determine contributions and identify infrastructure (social and physical) for which contributions may be sought.

3.21 Future development proposals for the Interface Land are expected to take account of the requirements of this document and any other public benefit brought about through the redevelopment of the site before submitting any planning applications to the Council.

In addition, an holistic Section 106 strategy document will need to be prepared for the Interface Land prior to any major development proposals being approved. This strategy document must be endorsed by Medway Council as the planning authority.





The background of the page is a light blue, semi-transparent image of a ship's complex rigging system, showing various masts, stays, and cross-arms. The rigging is dense and intricate, with many lines and structural elements visible. The overall tone is professional and technical.

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