

REGENERATION COMMUNITY AND CULTURE OVERVIEW AND SCRUTINY COMMITTEE

29 SEPTEMBER 2010

THE INTERFACE LAND DEVELOPMENT FRAMEWORK

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Summary

This report provides an update on the preparation of a Supplementary Planning Document (SPD) for the Interface Land, Chatham Maritime and the proposed adoption of this SPD by Cabinet on 19 October 2010. A four-week programme of public consultation on a draft SPD for the Interface Land, Chatham was carried out between 19 August 2010 and 14 September 2010. The consultation has now been completed and the results are set out in this report. **Copies of the draft Supplementary Planning Document are enclosed with the agenda.**

1. Budget and Policy Framework

- 1.1 Chatham Historic Dockyard Trust and the South East England Development Agency (SEEDA) jointly own the site known as the Interface Lands and have funded the preparation of the SPD. The cost of consultation has been met by the Council's existing budget with the assistance of the landowners. It is proposed that the Development Framework is adopted as a Supplementary Planning Document (SPD) prepared in conformity with the provisions of the Planning and Compulsory Purchase Act 2004 and the saved Medway Local Plan Policies S8 (Chatham Maritime) and S9 (Chatham Historic Dockyard).
- 1.2 The development framework contributes towards achieving the targets relating to three Local Area Agreements (LAA) indicators – the target for new homes, supported by appropriate infrastructure, the delivery of affordable and student housing and access to services and facilities by walking, cycling and public transport.

2. Background

- 2.1 The closure of Chatham Naval Base, Chatham in 1984 left an area in excess of 400 acres (162 hectares) of largely unoccupied buildings, land and water. The interface lands form the last major undeveloped site in the former Naval Base. The site is identified in the 2004 Medway Waterfront Renaissance Strategy as '*presenting an opportunity for a stunning mixed development that complements rather than competes*'.
- 2.2 The site straddles the boundary of Chatham Historic Dockyard and Chatham Maritime. Both Local Plan Policies S8 and S9 promote high quality design with Policy S9 stating '*the standard of urban design must be of the highest order*'.
- 2.3 The Medway Local Plan allocation (Policy S9) states that a framework to guide development will be prepared jointly with Chatham Historic Dockyard Trust. This development framework will ensure that the site is developed in a manner that complements the existing Historic Dockyard and is of a design appropriate to its sensitive historic setting.
- 2.4 The site is owned partly by SEEDA and partly by Chatham Historic Dockyard Trust.

3. Options

- 3.1 The Medway Local Plan allocation of the site establishes the principle of development. The Medway Local Plan states that a development framework is required to underpin the shared ambitions of Chatham Historic Dockyard Trust and various agencies.
- 3.2 Adoption will also help to 'de-risk' the site by providing landowners and developer/s with greater certainty regarding what is expected by the Local Planning Authority. Developers bidding for the site will be in a position to make more informed estimates of development costs. This, in turn, mitigates the risk of pressure on the Council as Local Planning Authority to accept sub-standard proposals because design quality was not integrated into the project finances from inception or because the site capacity was over estimated.
- 3.3 Not to produce a development framework would mean a lack of guidance to developers of the site and increase the likelihood of unsuitable development proposals coming forward and reduce the Council's ability to robustly respond to unsuitable proposals.

4. Advice and analysis

- 4.1 The development framework establishes guidance to:
 - Promote development appropriate to the scale and character of the historic environment;

- Promote development that reconnects the original Naval Base and Dockyard whilst preserving the Historic Dockyard's secure boundary;
- Maintain and enhance existing vehicular and pedestrian routes;
- Create a sense of arrival at the Historic Dockyard; and
- Create a mix of uses to enhance and sustain Chatham Maritime/Historic Dockyard destination.

4.2 It achieves this by setting clear limits to the extent of the developable area of the site, including parameters that control the height of the development. The development framework includes provision for public realm improvements and a street network that will encourage walking, as well as providing platforms for social interaction and facilitate the safe integration of pedestrians, cyclists and cars.

4.3 The development framework also takes into account environmental and heritage constraints such as flood risk, ecology and archaeology. The emphasis on design and creating an inclusive community aims to ensure that this is a development of enduring quality and is therefore inherently sustainable.

4.4 To ensure longevity, the development framework has been produced in a way that avoids designating sites for single or restricted uses. Instead the document plans and facilitates a supply of land suitable for residential, business and employment needs but is flexible enough to be responsive to a changing economy or new business requirements.

4.5 The proposals build successfully on Medway Waterfront Renaissance Strategy 2004. Developing the site is likely to take a number of years and the site's different character areas are likely to face different regeneration issues and challenges. With this in mind, the development framework will include a requirement for the preparation of a comprehensive, detailed phasing strategy and infrastructure implementation plan.

4.6 The development of the Interface Land is a considerable challenge but the site also offers enormous potential and the development framework reflects this.

5. Risk Management

5.1 There are considered to be limited risks associated with this project at this stage. The main risks are considered to be associated with a failure to progress the work and the Chatham World Heritage Bid as indicated below.

Risk	Description	Action to avoid or mitigate risk
Chatham World Heritage Bid	<p>The site lies within the core area of the proposed Chatham World Heritage site and its development has the potential to harm the setting of the Historic Dockyard and its defences including all of its scheduled ancient monuments and listed buildings.</p> <p>The UNESCO World Heritage Committee would not look favourably upon a bid where permission has been granted for development that may damage the Outstanding Universal Value of the potential World Heritage Site.</p> <p>This risk will be triggered by publication of a development framework, which pays significant regard to the possible impact of development. A possible consequence is weakening of the bid.</p> <p>This risk is rated D-2</p>	<p>The development framework has been developed in collaboration with English Heritage, the World Heritage Site Steering Group and the Chatham Historic Dockyard Trust. English Heritage has had significant involvement endorsing the development framework approach and influencing the content of document in relation to the heritage assets.</p> <p>The development framework makes provision for the retention and protection of archaeological features of national importance as well as the setting of existing scheduled ancient monuments and listed buildings.</p>
Failure to Progress	<p>Would lead to uncertainty in development decisions and failure to attract investment/development.</p> <p>This risk is rated E-2</p>	<p>Strong support from the Council, Chatham Historic Dockyard Trust, SEEDA, the World Heritage Site Steering Group and the local community will provide investor/developer confidence.</p>
Quick sale of SEEDA owned part of site as SEEDA is dis-banded	<p>Potential new owner proposes inappropriate development of the site.</p> <p>This risk is rates D2</p>	<p>Development Framework provides support to the Council to refuse planning permission for such proposals.</p>

6. Consultation

- 6.1 Collaborative work and involvement with key stakeholders, including SEEDA, Chatham Historic Dockyard Trust and English Heritage has informed the preparation of the development framework.
- 6.2 In preparing development planning documents to be adopted, the Council is required to meet a minimum level of public involvement under the Town & Country Planning (Local Development) (England) Regulations 2004, which requires representations to be made within a period of not less than four weeks or more than six weeks from the date of the start of consultation.
- 6.3 The public consultation commenced on 17 August 2010 and ended on 14 September 2010. This met the minimum level of public involvement required in order to ensure a level of engagement that is adequate to fully address the concerns of local residents.
- 6.4 The draft SPD, accompanying Sustainability Appraisal and also all the supporting appendices were available to view at Medway Council's Headquarters at Gun Wharf, Chatham.
- 6.5 All of the documentation was published on the Council's website:
<http://www.medway.gov.uk/index/environment/developmentplan/ldf/intefacedevelopmentframework.htm>
- 6.6 A notice of the consultation was placed in the Medway Messenger on Monday 30 August 2010. A letter and summary leaflet was also circulated by Chatham Historic Dockyard Trust to all residents and tenants of the Historic Dockyard giving notice of the consultation.
- 6.7 Further publicity of the availability of the draft SPD was made in the following ways:
 - A press release was issued by Turley Associates to industry trade press.
 - A press release was issued by Medway Council to local newspapers, radio and Television.
 - Press releases on the Chatham Historic Dockyard Trust and South East England Development Agency websites.
 - A letter giving notice of the consultation and a copy of the summary leaflet was sent by Turley Associates to the following organisations:
 - St Mary's Island Residents Association;
 - Brompton Residents Association;
 - Upnor Residents Association;
 - Defence Estates;
 - Chatham Maritime Trust;
 - Peel Holdings (owners of commercial Chatham Dockyard);
 - Medway Tourism Association;
 - Fort Amhurst Heritage Trust;
 - Disability Access Groups;
 - Medway Youth Parliament.

- Advertisements for the consultation on the SPD were erected on site.
- On Thursday 19 August and Monday 6 September 2010, a consultation afternoon/evening and exhibition for the community was held at the Royal Dockyard Church. The exhibition remained in situ between these dates allowing general visitors to the Dockyard to review the SPD and comment in a formal response book left with the exhibition.
- A presentation was made to the World Heritage Group at a specifically convened meeting on 8 September 2010.

6.8 The consultation/summary leaflet specifically asked for comments on key issues. **A copy of the form is enclosed with the agenda for members of the committee.**

6.9 The main issues raised during the consultation process were as follows:

- The proposed form and occupancy of future development;
- The development of tall buildings within the site and the siting of these buildings;
- The security and access to the existing residential and tenanted locations of the Dockyard;
- The phasing of any future development;
- Good support for the draft SPD.

7 Financial and legal implications

7.1 The costs of preparing the development framework and conducting the programme of public engagement has been met by Chatham Historic Dockyard Trust with the assistance of SEEDA and the Council's existing budget.

7.2 The development framework will only be given full weight in planning decisions if it forms part of the Local Development Framework or is an adopted supplementary planning document. It is proposed that the development framework is adopted as a Supplementary Planning Document. Government advice in Planning Policy Statement 12 stresses the importance of proportionate public consultation during the preparation stage of local development documents.

7.2 A supplementary planning document must be in conformity with policies in development plan documents (or saved policies in the Development Plan). The relevant saved policies in this case are Policies S8 (Chatham Maritime) and S9 (Chatham Historic Dockyard) in the Medway Local Plan, 2003. Supplementary planning documents must be prepared and consulted upon in accordance with the Town & Country Planning (Local Development) (England) Regulations 2004 (as amended).

8. Recommendations

- 8.1 That the Committee note the results of the consultation and the proposal for Cabinet to adopt the Development Framework on 19 October 2010.

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Background papers

- Medway Local Plan 2003
- Local Development Framework Statement of Community Involvement.

How can you get involved?

Consultation is taking place from 17th August to 14 September 2010.

We would like your views on:

1. **the principle of regenerating the site through appropriate mixed use development that reconnects the parts of the former Naval Base**
2. **the vision and objectives for the appropriate development of the Interface Land**
3. **the proposed character areas**
4. **proposals for the conservation of the above ground heritage features and archaeology**
5. **below ground archaeology and its consideration**
6. **the practical development of the site considering existing and future users**

Consultation Exhibition

An exhibition regarding the draft SPD will be held between 16.00 and 20.00 on two days, **Thursday 19 August** and **Monday 6 September**, at The Royal Dockyard Church, The Historic Dockyard, Chatham.

Where you can find out more?

To find out more you can view the documents at www.medway.gov.uk

A copy of the SPD is available to view during normal office hours at Medway Council's Civic Headquarters at Gun Wharf, Chatham.

How can you comment on the SPD?

You can send comments to:

Email: chathamdockyard@turleyassociates.co.uk

Fax: 020 7851 4020

Post: Turley Associates, 25 Savile Row, London W1S 2ES

The Interface Land Chatham

Consultation on draft Supplementary
Planning Document

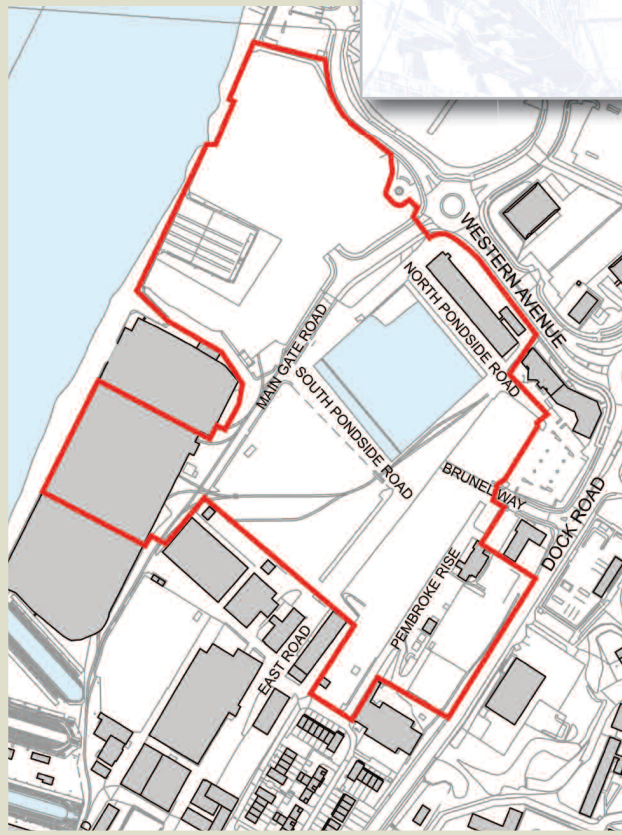
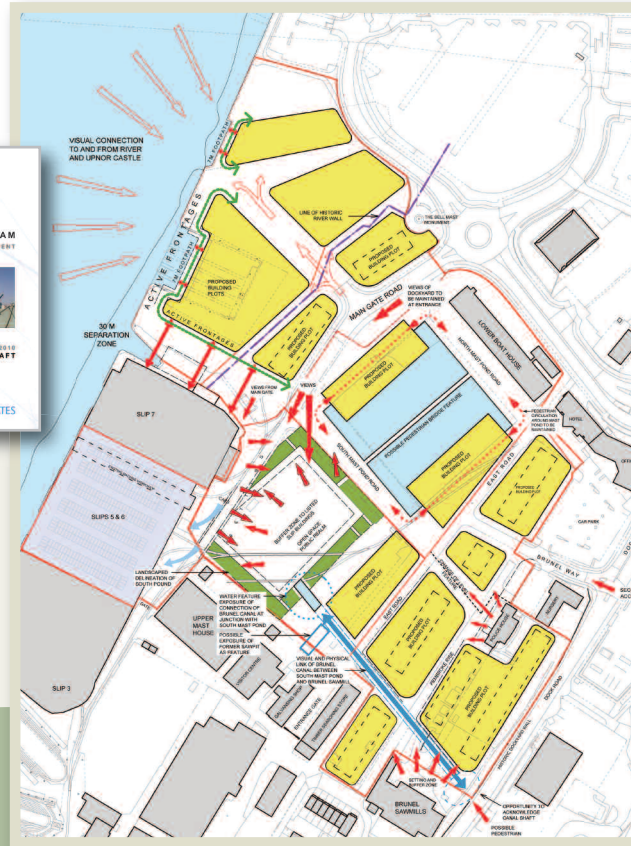
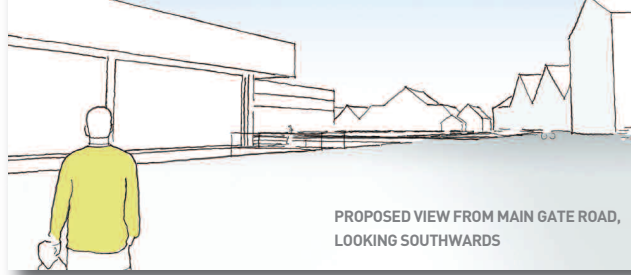
August 2010



The purpose of this leaflet

The Interface Land forms the last major undeveloped site in the former Royal Naval Dockyard, Chatham. The land sits between the earlier Georgian Dockyard to the south and the large nineteenth century extension to the north and is historically associated with the age of sail.

A draft Supplementary Planning Document (SPD) has been prepared to promote and manage future development of the site and advocate a development appropriate to the scale and character of the adjacent area.



The Vision for the Interface Land

The vision is to create a sustainable development that provides the opportunity to link Chatham Maritime with The Historic Dockyard, whilst reflecting and respecting the heritage sensitivities of the study area. In bringing forward the vision, future development of the Interface Land should seek to:

Provide high quality built form and public realm

Introduce innovative sustainable design solutions

Preserve and/or enhance the special character and appearance of the heritage assets both within and outside the site

Reinstate cohesion and connections with the surrounding area

Introduce a range of complementary, compatible and sustainable land uses

Create a distinct identity but respect its setting

The emerging development framework provides guidance on the acceptable spatial arrangement and form of development proposed to regenerate the area.

sustainability
innovation vision future
heritage

THE INTERFACE LAND, CHATHAM

SUPPLEMENTARY PLANNING DOCUMENT



AUGUST 2010
CONSULTATION DRAFT

TURLEYASSOCIATES

FOREWORD

The closure of HM Naval Base Chatham in 1984 left an area in excess of 400 acres of largely unoccupied buildings, land and water. This included:

- a poorly maintained 80 acre estate to the south with a diverse collection of historic building representing the finest example of a dockyard of 'the age of sail' in the World;
- a substantial range of fine Edwardian brick barrack buildings;
- a 1960/1970s built two steam nuclear submarine refit complex with an extensive assortment of large low grade 20th century buildings on contaminated land; and
- three factory basins each of some 20 acres providing deep water moorings, one with dry docks and the most northerly operating as a commercial port with locks to the river Medway.

All of this with huge potential but with extraordinary challenges and associated costs.

Since that date there has been significant investment in the location by the Chatham Historic Dockyard Trust (the 'Trust'), a succession of Government regeneration agencies, most significantly the South East England Development Agency ('SEEDA'), and the private sector. This has resulted in the southern area, the Historic Dockyard, becoming a successful tourist destination and mixed use site, the establishment of the Universities and Chatham Maritime becoming one of the top three office locations in Kent. In addition, over 1,000 homes have been built alongside a marina, a large retail and entertainment complex with a cinema, bars, restaurants and hotels.

As investment continues in Chatham Maritime and The Historic Dockyard we, as landowners, are delighted to jointly bring forward a Supplementary Planning Document (SPD) to guide future development for the Interface Land, Chatham. The land forms the last majority undeveloped site of the former Royal Navy Dockyard and together we share a common aim in seeking the regeneration of this major site at the heart of Medway.

The landowners' and local planning authority's aspirations are the development of a high quality mixed use scheme providing scale and significance against the Covered Slips and other historic buildings and features. SEEDA and the Trust are committed to overseeing the long term development of the Interface Land in their role as landowners. This important commitment will drive the delivery of the proposals, whilst ensuring consistency and high standards are maintained in the quality of design and construction.

By adopting a joint working approach, the proposals set out in this SPD will complement and support the key regeneration objectives of the area and help to address additional long-term challenges to ensure that the potential of the Interface Land is realised and future development complements the investment to date.

Bill Ferris
Chief Executive
Chatham Historic Dockyard Trust

Robin Cooper
Director of Regeneration, Community and Culture
Medway Council

Jonathan Sadler
Development Director Thames Gateway
South East England Development Agency

This document is an initial draft of the Chatham Interface Land Supplementary Planning Document (SPD). Consultation at this stage provides you with the opportunity to get involved early on in the process of formulating the document enabling your feedback to help us shape the final document.

The aim of this document is to provide a legal planning framework for the development of the area and to relate our aspirations for the Interface Land with both national and city council planning policy.

Public consultation on this SPD will commence **on Tuesday 17 August 2010 and will close at 5.00pm on Tuesday 14 September 2010.**

Should you have any comments, please forward these by post to Turley Associates, 25 Savile Row, London W1S 2ES, by fax to 020 7851 4020; or by email to:

chathamdockyard@turleyassociates.co.uk

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Appendices (forming separate reports)

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Appendix 2 - Planning Policy Review

Appendix 3 - Building Heights Assessment

Appendix 4 - Flood Risk Investigation

Appendix 5 - Sustainable Transport Strategy

Appendix 6 - Geo Environmental Baseline

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London
W1S 2ES
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What is a Supplementary Planning Document?

- 1.1 Supplementary Planning Documents (SPDs) are so titled because they add details to policies set out in development plans in order to explain how the local authority's policies can be taken forward. They may take the form of design guides, area development briefs, master plans or issue-based documents. These documents can use illustrations, text and practical examples to expand on how the authority's policies can be taken forward.
- 1.2 A Supplementary Planning Document is a Local Development Document that may cover a range of issues, thematic or site specific, and provides further detail of policies and proposals in a 'parent' Development Plan Document.
- 1.3 SPDs are used to assist with determining priorities, guiding investment and deciding planning applications.
- 1.4 The current development plan for Medway comprises the South East Plan (2009), minerals and waste policies at county level, and the Medway Local Plan, which was formally adopted in 2003. The Local Plan will soon be succeeded by the Local Development Framework (LDF) which will plan for the future of Medway up to 2026. This SPD will form part of that framework.
- 1.5 Local authorities are required to involve local communities in the preparation of SPDs before they can be adopted as formal planning policy. Various key stakeholders were involved during the preparation of this SPD.



Covered Slip Buildings looking north

- 1.6 A Sustainability Appraisal, attached at Appendix 1, incorporating the requirements of the Strategic Environmental Assessment Directive, has been prepared alongside this SPD. Medway Council, Natural England, the Environment Agency and English Heritage were consulted on the requirement for a Sustainability Appraisal of the SPD in November 2009. A Pre-Assessment Report was consulted on in February 2010 to ensure the most up to date policies and baseline data available were identified. The SPD was appraised to predict and evaluate the likely effects. The Sustainability Appraisal has helped to ensure that this SPD will promote sustainable development.

Role and Objectives of SPD

1.7 This SPD sets out an overview of the development requirements for the site following a strategic baseline review of existing framework documents; sets out the overarching spatial design principles and components to deliver successful and sustainable change and concludes with the recommendation of a delivery strategy to ensure the successful implementation of the plan.

1.8 The Interface Land is a strategic site with the ability to reconnect the former elements of the Naval Dockyard - The Historic Dockyard and Chatham Maritime. The Historic Dockyard is the world's most complete historic dockyard from the age of sail. There are a number of key objectives for the future development opportunities within the Interface Land including:

- promoting a development appropriate to the scale and character of the historic environment and which enhances that environment and the setting of its buildings, most of which are Scheduled Ancient Monuments (SAM);
- establishing a development that reconnects the formerly integrated elements of the original Naval Base and Dockyard whilst preserving the Historic Dockyard's secure boundary;
- maintaining and enhancing existing vehicular and pedestrian routes, with the addition of a riverside walk to the north side of Covered Slip 7. Existing parking



Covered Slip Buildings looking south



Covered Slip 7

numbers are to be retained;

- the creation of an appropriate sense of arrival at the Historic Dockyard; and
- creating a mix of uses likely to enhance and sustain the emerging Chatham Maritime/Historic Dockyard destination.

1.9 The main role of this SPD is to set out a Spatial Masterplan that forms a framework for the future redevelopment of the Interface Land. The Masterplan should be read in conjunction with all supporting technical documents set out in Appendices 1 to 6; these set out the background, technical requirements and review emerging guidance, all of which are considered as part of this SPD.

1.10 The Chatham Maritime Interface Land Supplementary Planning Document is currently in consultation status supplementary to the planning policy.

Vision statement

- 1.11 The vision for the Interface Land is to create a sustainable development that provides an opportunity to link Chatham Maritime with the Historic Dockyard, whilst reflecting and respecting the heritage sensitivities of the study area. The proposals will create a distinctive character that combines high quality buildings and public realm and respond well to its context to attract and retain investment.
- 1.12 The emerging development proposals should reflect the historic age of sail, industrial past and pioneering technology developed at Chatham. It will contain a mix of uses that complement the study area and its surroundings. It will also support the creation of a more attractive, accessible, safe and sustainable environment.
- 1.13 In order to bring forward the vision, future development of the Interface Land should seek to:
 - provide high quality built form and public realm;
 - introduce innovative sustainable design solutions;
 - preserve and/or enhance the special historic character and appearance of the heritage assets both within and outside the site;
 - reinstate cohesion within the surrounding area;
 - introduce a range of complementary and compatible land uses; and
 - create a distinct identity but respect its setting.



North Mast Pond



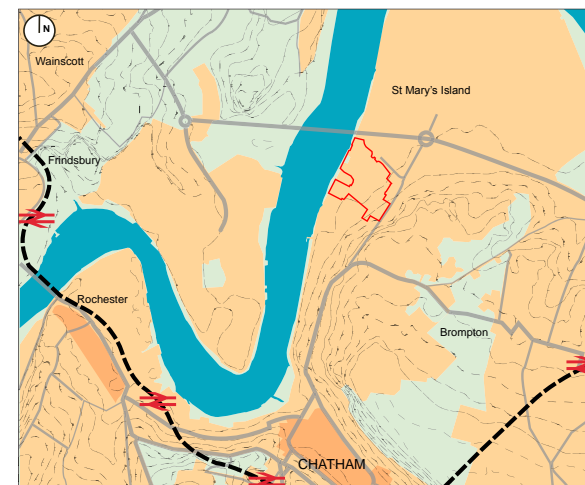
Mast House and Mould Loft

- 1.14 The Government’s objective is that the historic environment and its heritage assets should be conserved and enjoyed for the quality of life they bring to this and future generations. To conserve assets in a manner appropriate to their significance, it is important that:
 - wherever possible, heritage assets are put to an appropriate and viable use that is consistent with their conservation;
 - the positive contribution of such assets to local character and sense of place is consistent with their conservation;
 - consideration of the historic environment is integrated into planning policies, promoting place-shaping.
- 1.15 The site’s location in the river valley and historic features dictate the massing of future built form. In addition, there is the need to protect the identified strategic views and connections to the ridgelines, which limits the scale of the future built form in relation to the wider setting. There is a need to protect views to the north from within the Historic Dockyard.
- 1.16 In order to respond to this vision the Interface Land will need to be subject to detailed masterplanning work.

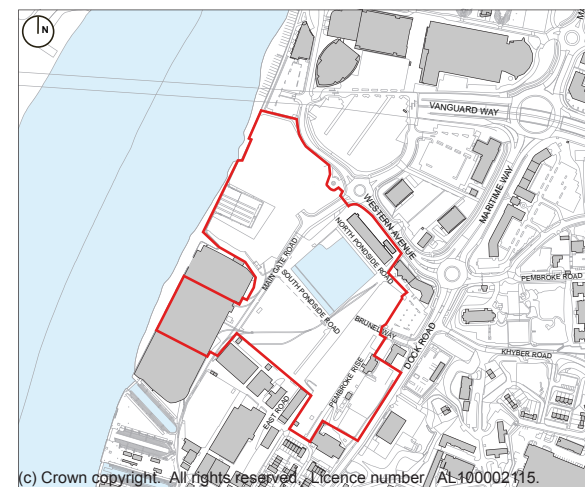
Location and Context

- 1.17 The Historic Dockyard is located to the north of Chatham town centre on the southern bank of the River Medway. Occupying an area of some 32 hectares (80 acres) the Dockyard was in continuous occupation by the Royal Navy from 1618 to 1984.
- 1.18 The Interface Land forms the last major undeveloped site in the former Royal Naval Dockyard, Chatham, and is in the ownership of South East England Development Agency (SEEDA) and the Chatham Historic Dockyard Trust (a Registered Charity established by government following the Dockyard's closure in March 1984. The land sits between the earlier Georgian Dockyard to the south and the large nineteenth century extension to the north and is historically associated with the age of sail.
- 1.19 The Interface Land extends to 10ha in size, however there are a number of heritage buildings and designations both above and below land which form significant constraints to future redevelopment. The land is currently disused or partly used for parking associated with the tourist elements of the Historic Dockyard.
- 1.20 Since the closure of the Royal Naval Dockyard in 1984 a programme of repairs and refurbishment has created a mixed-use site, which is a successful tourist destination in its own right attracting up to 170,000 visitors per annum. The Historic Dockyard has a thriving community of over 140 small businesses and organisations employing approximately 1,000 people; in addition there are around 400 residents. Approximately 2,000 people use the site daily, equivalent to 700,000 per year.

- 1.21 A conservation management plan (2004 – 2009) for the Historic Dockyard Conservation Area has been prepared. This document has helped to inform the preparation of this SPD by identifying the conservation issues affecting the wider Dockyard and providing appropriate guidance.
- 1.22 Over the past decade, the wider Chatham Maritime location has attracted a number of large corporate occupiers, a new university campus, an outlet centre and a residential quarter. At Chatham Maritime it is estimated there are 3 million visitors per annum, whilst the destination provides 3,000 jobs, accommodates 3,000 residents and also comprises around 7,000 students.



Plan showing the site in relation to its wider context



Plan showing the site boundary

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Historic Context

- 1.23 The first documented evidence of the Royal Navy's use of the River Medway can be found in 1547 with the rental of two storehouses on 'Jyllingham Water', whilst the first war ship constructed was the Sunne in 1586. In 1613 the dockyard moved downstream to the present location of the Historic Dockyard and by 1618 storehouses and a ropewalk had been built.
- 1.24 The Interface site lies south of the 17th century dockyard and, developed the first mast pond (south) in 1696 with a series of arches and racks integrated within this for underwater timber storage. The second pond followed circa 1700. The dockyard grew significantly in the 1720s

within a new boundary wall with tower houses that crossed the Interface site. The Mast House and Mould Loft were completed in 1755 and today provide an example of timber framed construction by the dockyard workforce.

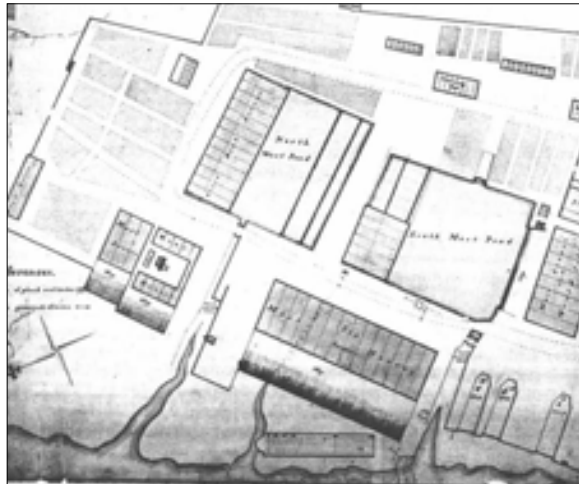
- 1.25 The 1770s saw the construction of a number of sawpits, some in saw houses and, subsequently timber seasoning stores of which two survive. This activity relates to a 1773 decision taken under the Earl of Sandwich to use the dockyard primarily as a building and repair yard. Timber processing was then the main purpose of the Interface site and a 1795 plan shows a series of Mast Houses and a Boat House, that developed adjacent to the river, and Store Boat Houses that were constructed over the North and South Mast

ponds. This plan also clearly shows the then alignment of the river front with wharfage made out of massive vertical timbers.

- 1.26 The next significant development within the dockyard was the development of Marc Brunel's steam powered sawmill in 1814 as part of the demands imposed for ship building by the Napoleonic wars. A canal was developed to transport logs from the south mast pond to an elliptical shaft at the sawmill. After cutting these, timbers would be moved by overhead crane to timber stacking yards to the east and south of the Police House. Expansion for the sawmill saw the early 18th century dockyard wall taken down and replaced by the present example.



Plan showing the layout of the dockyard in the early 18th century with the two mast ponds and the sea wall clearly visible.

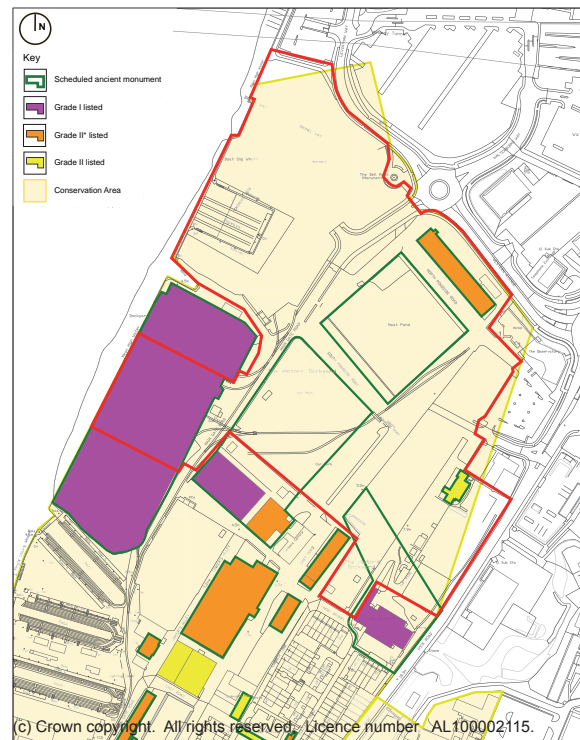


Plan showing the layout of the dockyard in 1795 with the 18th century dockyard wall visible.



Plan showing the layout of the dockyard in 1879 with ... visible.

1.27 Land reclamation westwards into the River Medway occurred after 1817 on which new covered building slips were then built. No 3 Slip in timber from 1838 and subsequently Slips 4, 5 and 6 from 1846. Slip 7 was constructed from 1851. With the introduction of steam power and iron hulls the existing dockyard built for a wooden sailing navy was too small and a major extension was carried out after 1860 on St Mary's Island. This left the Interface site as an area between a new Victorian steam facility and the core of the older dockyard. New uses were found for the site as the importance of timber declined and land reclamation westwards into the river increased the space available and left the 18th century wharfage fossilised behind the present day river wall. The last Chatham built battleship was built on the former open air Slip no. 8 in 1905, with the covered slips then predominantly being used for submarine building.



Plan showing the Historic Designations

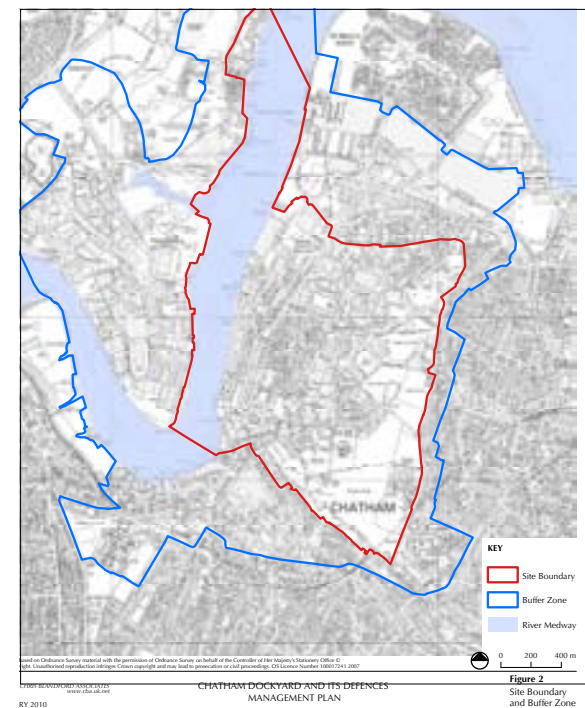
1.28 In 1984 the dockyard closed with many of the undesignated buildings in the northern part of the site being demolished. Today, a number of structures which were retained form an integral element and backdrop to the Interface site. The importance of the surviving dockyard buildings and some buried structures has been recognised through statutory listing or designation as Scheduled Ancient Monuments. Archaeological buried remains of equal importance are not designated. All these heritage assets form a part of the conservation area.

1.29 The current conservation area designation recognises that the significance of the Dockyard is the sum of the individual designated assets and those which cannot be documented by individual designations.

- 1.30 Following recent public consultation on the future of World Heritage, the Department for Culture, Media and Sport has announced that it will be drawing up a new UK Tentative List of possible nominations for World Heritage status, with the view of submitting a new List to UNESCO in 2011. Chatham Dockyard and its defences were previously on the Government's shortlist of sites and will apply under the new guidance to reaffirm this position.
- 1.31 The site has been proposed as a future World Heritage Site because:
- Chatham is the world's most complete Historic Dockyard from the age of sail - a time when Dockyards were the industrial centres of Europe.
 - Chatham was instrumental in securing and maintaining Britain's worldwide influence, and in shaping the world we know today.
 - Chatham Dockyard boasted an advanced and exemplary array of facilities to build, repair, and deploy the fleet.
 - The completeness and survival of the Dockyard and its defences – Fort Amherst, Upnor Castle and the Great Lines – is unrivalled.
 - A series of historic barracks and military installations helps make Chatham an outstanding garrison town.
- 1.32 Under current legislation, it is hoped that Chatham will be nominated for World Heritage status in 2012.

World Heritage Site Nomination

“Chatham Dockyard is the supreme example of a Royal Dockyard largely unaltered from the age of sail.... The importance of the Dockyard is enhanced by its close association with contemporary military establishments. This combination of a substantially intact 18th century Dockyard with its contemporary massive landward defences is unique” from ‘World Heritage Sites: the UK Tentative List’ (Department for Culture, Media and Sport, 1999).



Plan showing the proposed World Heritage Site boundary and buffer

Archaeological Statement

- 1.33 There is substantial evidence within the Interface Land for timber and masonry structures of the Georgian Dockyard (of the age of sail) dating to the late 18th and early 19th centuries. These structures can now primarily be found underground. However, it is essential that these are acknowledged, recorded and in some cases protected as they form an integral part of the Historic Dockyard and demonstrate the historical functional relationships of the structures in the working Dockyard.
- 1.34 The principal features include the former timber, river wall illustrating the form and location of the 18th century Dockyard prior to any land reclamation, active and redundant conduits linking the mast ponds with the Medway and a series of saw pits which were integral to wooden ship building. In addition a canal linked the South Mast Pond and the Sawmill.

Understanding Heritage Values

- 1.35 English Heritage provides guidance relating to how inter-related heritage values may be attached to a place. It is important that these values are considered in relation to future development of the Interface Land. Alterations and new work to a significant place should normally be acceptable if:
- there is sufficient information comprehensively to understand the impacts of the proposal on the significance of the place;
 - the proposal would not materially harm



Plan showing key Archaeological Features

- the values of the place, which, where appropriate, would be reinforced or further revealed;
- the proposals aspire to a quality of design and execution which may be valued now and in the future; and
- the long-term consequences of the proposals can, from experience, be demonstrated to be benign, or the proposals are designed not to prejudice alternative solutions in the future.

- 1.36 This guidance is set out in the English Heritage Conservation Principles, Policies and Guidance dated April 2003. The document can be downloaded at www.english-heritage.org.uk.

Policy Context

- 1.37 SPDs must be consistent with national, regional and local planning policies. A review of relevant planning policies has been carried out and is provided at Appendix 2 of this document. However, below is a summary of some key policy documents that have been identified as a result of the planning policy review and which need to be considered for future development.
- 1.38 Planning Policy Statement 1 (PPS1) sets out sustainable development as the core principle to inform planning. Medway Council considers it essential that the redevelopment of the Interface Land embodies the principles of PPS1 including:

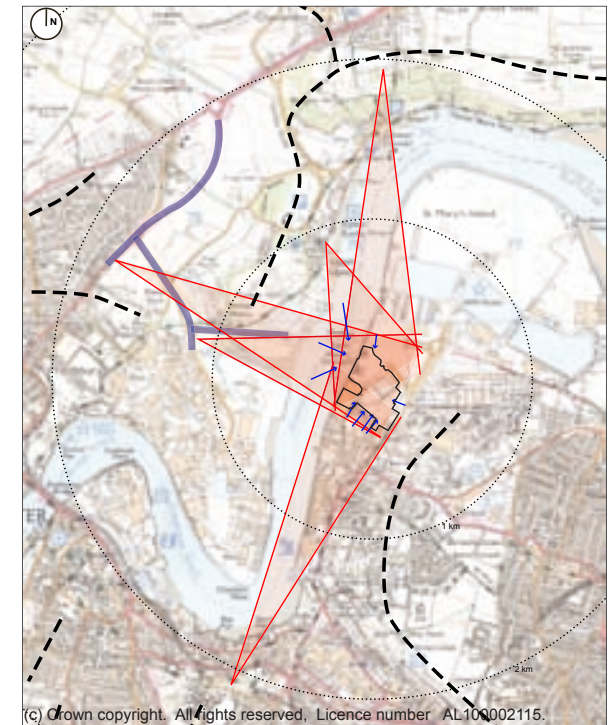
- making suitable land available for development in line with economic, social and environmental objectives to improve people's quality of life;
- contributing to sustainable economic development;
- protecting and enhancing the natural and historic environment and existing communities;
- ensuring high quality development through good and inclusive design and the efficient use of resources; and

- ensuring that development supports existing communities and contributes to the creation of safe, sustainable, livable and mixed communities with good access to jobs and key services for all members of the community.

- 1.39 Planning Policy Statement 5 (PPS5) is now the policy basis for all plan making and assessing of heritage-related applications in England and is the first major step in the reform of the heritage planning system ahead of a new Bill. The Statement encompasses any 'heritage asset' considered to have some degree of significance, this includes all nationally designated and registered heritage assets (listed buildings, Scheduled Ancient Monuments, Conservation Areas, registered parks and gardens, battlefields, archaeology, World Heritage Sites), plus all locally designated heritage assets (locally listed buildings, sites of local archaeological interest etc).
- 1.40 Through the Statement, local authorities are encouraged to identify and assess the significance of any element of the historic environment that may be affected by development proposals. This significance includes the value of an asset for this and future generations.
- 1.41 Heritage assets are defined in the Statement. Of particular relevance are those assets with archaeological interest that are not currently designated as Scheduled Monuments, but are of equivalent significance. The Statement states that the absence of designation does not indicate lower significance and should be treated importantly.
- 1.42 The Statement states that not all elements of a World Heritage Site or Conservation Area

will necessarily contribute to its significance. Therefore, when considering proposals local planning authorities should take into account the relative significance of the element affects and its contribution to the significance of the area as a whole.

- 1.43 Sustainable development is given a high priority by PPS5, with local authorities encouraged to seek the reuse and sympathetic adaptation of historic buildings to reduce carbon emissions.
- 1.44 A review of the Medway Local Plan identified that the Interface Land is not covered by any adopted policies that would prevent development within it. It falls within two strategic policies (Policy S8) Chatham Maritime and (Policy S9) Chatham Historic Dockyard. These both promote high quality urban design and townscape with the latter stating that "development that respects the historic character of the site will be permitted."
- 1.45 The Medway Waterfront Renaissance Strategy, 2004, identified the Interface Land for regeneration with housing, commercial and leisure uses, and offices uses. This mixed use development is seen as integrating the Historic Dockyard, to the south, and the Dockside outlet shopping centre, to the north.
- 1.46 The Interface Land falls within the University and Leisure Quarter and 3.11 of Part 2 of the Building Height Policy for Medway SPD and states that:
- "The area immediately to the north of the Historic Dockyard is known as the 'Interface Land'. It is largely redundant at present. However, it has the potential to be a major new leisure and tourist destination. Distinctive landmark buildings could help reinforce this role. This may justify carefully designed and placed higher buildings. However, it*



Plan showing the strategic and local views

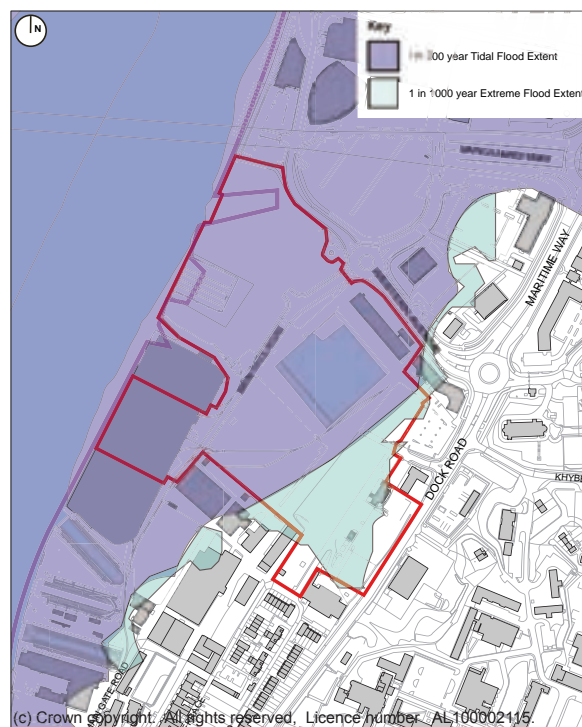
is important that the Covered Slips at Chatham Historic Dockyard remain a key focal point of riverside views and that the backdrop of the Brompton and the Lines remains prominent as a key part of the setting of the Dockyard. This will be an important factor in shaping design proposals for the area".

Conservation Area

- 1.47 The majority of the Interface Land falls within the Chatham Historic Dockyard Conservation Area.

A Building Heights Policy

- 1.48 An SPD relating to tall buildings in Medway was published by the Council in May 2006 to ensure that any proposals for tall buildings within the region are properly considered in the context of a clear long term vision. As a key regeneration site within Medway, the Interface Land must adhere to this document and conform with its recommendations.
- 1.49 The SPD comprises two sections. Part 1 sets out general location and design policy criteria for formulating and assessing high building proposals, whilst Part 2 identifies locations where taller (referred to as higher in the document) buildings are not appropriate within the defined Medway Waterfront Renaissance Strategy Area. Appendix A to Part 2 sets out the identified strategic and approach views, and the important features within each view.
- 1.50 As part of the contextual analysis, Turley Associates undertook a Building Heights Assessment to determine the appropriate building heights for emerging development proposals for the Interface Land. The complete assessment can be found at Appendix 3.
- 1.51 The policy and contextual assessment undertaken for the Building Heights Assessment concludes that any new development within the site would need to have limited visibility from within the Historic Dockyard, and have particular regard to the setting of the heritage assets.



Plan showing the Flood Risk within the Interface Land

- 1.52 The visual assessment focuses on the primary location for tall buildings within the land adjacent to Slip 7 and the River Medway to the northwest of the site.

Flood Risk

- 1.53 The Interface Land is primarily located within tidal Flood Zone 3 (ie high probability flooding - > 0.5% annual probability of flooding from the sea), however the eastern end of the site is within Flood Zone 2 (ie medium probability of flooding - annual probability of flooding between 0.5% and 0.1%) and a small element Flood Zone 1 (ie low probability of flooding - annual probability of flooding lower than 0.1%).
- 1.54 PPS25 recommends that development should be directed towards those areas at a low probability of flooding (ie Flood Zone 1), only where there is no reasonable available site in Flood Zone 1 can Zones 2 and then 3 be considered. Also, if development within the floodplain is necessary, it should be sequentially allocated putting the less vulnerable development (eg commercial spaces) within the areas in which the expected probability of flooding and hazard are higher and the more vulnerable development (eg residential units) within the areas in which the expected probability of flooding and hazard are lower.
- 1.55 However it should be recognised that areas in Flood Zone 3 along the River Medway are also most desirable for residential use and the viability of any future development could be compromised in promoting alternative uses in these areas. Proposals will recognise and address the flood risk issues and ensure adequate mitigation measures are implemented in discussion with the Environment Agency and following the advice of PPS25.

- 1.56 A flood risk investigation report prepared as part of the SPD is included at Appendix 4.

Transport Infrastructure/Movement

- 1.57 It is recognised that any development at the Interface Land will generate additional vehicular traffic, therefore the key importance of transportation and access within the site and linkages to Chatham, Medway, Kent and London should be addressed. Sustainability principles are essential and development proposals should, where possible, demonstrate how they will reduce reliance on the private car, enhance public transport usage and improve walking and cycling facilities.
- 1.58 The quality of the bus services to Chatham is excellent due to a frequent servicing linking Chatham Maritime retail outlets to the town centre. The proposed scheme should seek to optimise access to these services, around half of which are available from Dock Road by Western Avenue, and all of which are available from Dock Road by Wood Street. The desirability of Chatham has also recently been further enhanced by the introduction of high speed rail services to London St Pancras.
- 1.59 The principal access route to the Interface Land will be from Western Avenue. It is important that all existing or proposed internal roads can accommodate lorry movements as these are required to service the businesses located within the Historic Dockyard.
- 1.60 At present, car parking for the adjacent Historic Dockyard is provided on the site, totalling some 400 spaces; these must continue to be accommodated within the site. The proposed



Figure ground plan showing the existing urban grain

development should seek to deliver a car parking strategy which accommodates the existing demand for car parking on the site, together with the demand generated by the additional development. In addition, parking facilities need to be provided for twelve coaches and provision of parking for the hotel should also be accommodated.

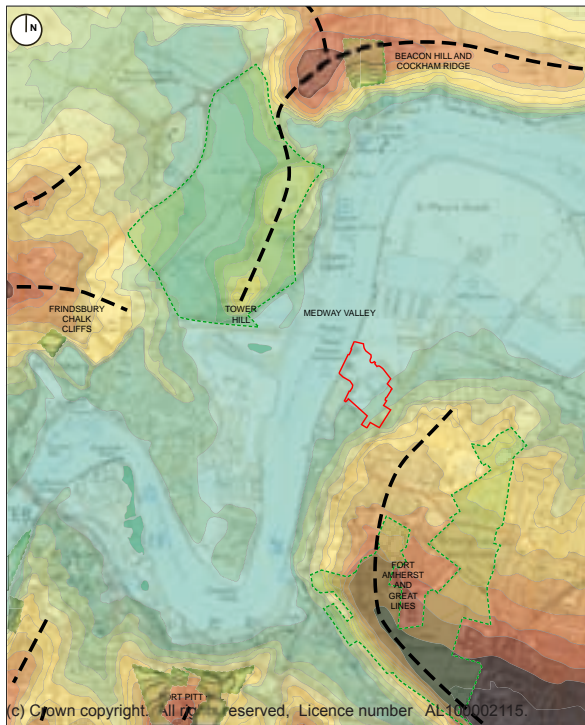
- 1.61 A sustainable transport strategy, prepared as part of this SPD is included at Appendix 5.

Ecology

- 1.62 Surveys will be required to confirm the presence of any protected species prior to any development being undertaken. These surveys should take place in consultation with Natural England to ensure compliance with relevant legislation and guidance.
- 1.63 Any tree works would require permission from the Council due to the Conservation Area designation.

Urban Grain

- 1.64 The figure ground study shows that the Interface Land has only a number of buildings within it and currently consists of predominately open land. It illustrates that the area to the south east of the Interface Land (the Historic Dockyard) boundary has a tight urban grain with medium to small buildings defining blocks and open spaces. There is a series of simple large building footprints adjacent to the River Medway to the southwest,



Plan showing the existing topography

formed by the Covered Slip buildings. To the north are a number of medium to large buildings that are set within open land which form car parks to the large retail centre.

Topography

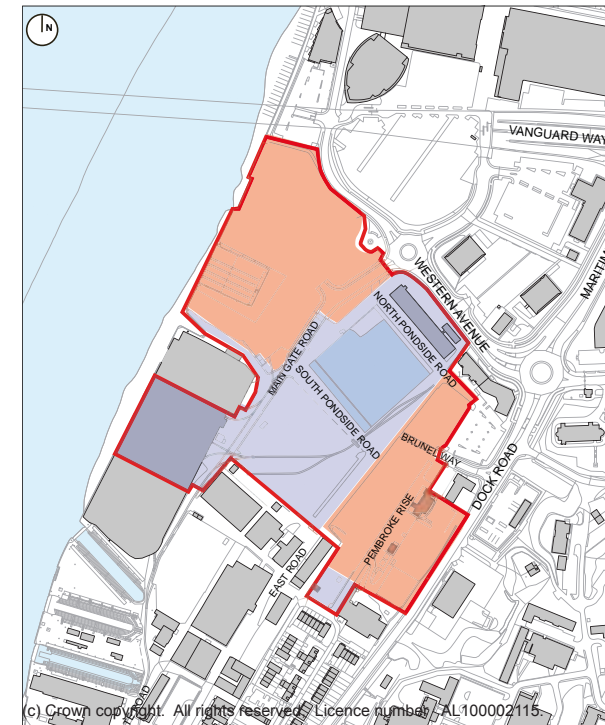
1.65 The Interface Land is located within the lower ground of the Medway Valley. It is framed by the surrounding raised topography of Beacon Hill and Cockham Ridge to the north, Tower Hill to



Plan showing the building heights

the west, and Fort Amherst and Great Lines, to the south and east. The Building Height Policy SPD highlighted that the bowl, green hill slopes and ridges are important components of the Medway landscape.

1.66 Within the Interface Land the topography rises from 4.2 metres AOD to the northwest, dropping to 3.8 metres within the centre of the site between the Mast Ponds. Then rising up to 12.2 metres to the southeast area of the site, adjacent to the Brunel Saw Mill.



Plan showing the Interface Lands ownership boundaries

Building Heights

1.67 The building heights plan illustrates that the area to the southeast is primarily one to four storeys in height (approximately 6 to 15 metres) and the medium to large footprints adjacent to the River Medway to the southwest and north are around five to eight storeys in height (approximately 18 to 27 metres). Further to the north (and not illustrated on this plan) there are two towers known as the Quays that are 20 and 16 storeys in height and to the southeast is Melville Court flats, another tall building.

Development Constraints

1.69 The preceding strategic framework has highlighted the complexities of developing this large, largely vacant, site having regard to the Historic Dockyard to the south and the Chatham Maritime development to the north. Set out below is a summary of the constraints to development that must be considered as part of a future development strategy. A number of the constraints relate to the historic features of the site, therefore English Heritage is an important statutory consultee for any future development proposals.

Heritage Considerations

1.70 The site falls within the Chatham Historic Dockyard Conservation Area and includes a number of Scheduled Ancient Monuments, Grade I, Grade II* and Grade II buildings.

Archaeological Considerations

1.71 Underground features include the saw pits, former Brunel Canal, 18th century river wall and conduits linking the Mast Ponds and River Medway.

Land Ownership

1.72 The complexity of boundary issues means building blocks or associated uses must not cross land ownership boundaries.

1.73 Slip 7 is privately owned, therefore it is important that its continued use is considered in emerging masterplan documents.

Building Heights and Massing

1.74 The volume, scale of the historic Slip buildings should be acknowledged and any new proposed development respect their industrial heritage and setting in the context of the River Medway.

Flood Risk

1.75 A large part of the Interface Land falls within the 1/200 year flood level area, whilst the majority of the remain land is 1/1,000.

Access

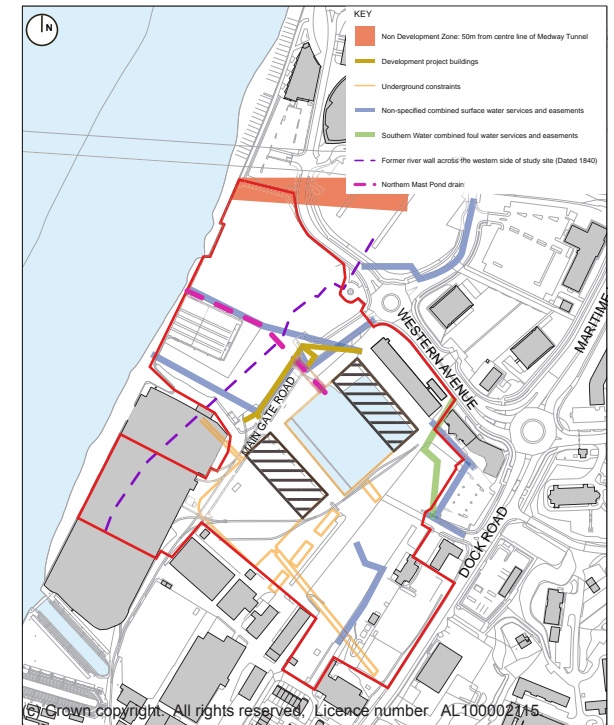
1.76 Main vehicular access must be from Western Avenue; access from Dock Road via Brunel Road is limited and can not be guaranteed.

No Build Zone

1.77 There is a non-development zone which applies 50 metres from the centre line of the Medway Tunnel. In addition, there is a 7 metres non development zone adjacent to the River Medway that will permit future maintenance and rebuilding of the river wall. This area should accommodate the extension of the Chatham Maritime Riverside Walk.

Services

1.78 A number of surface water, foul water and easements exist across the Interface Land.



Plan showing the Interface Lands constraints

1.79 From the other side of the River Medway the Covered Slips are visible landmarks that stand out against a relatively low tree ridgeline. Any future development would need to be of a sufficient scale to complement the Slips and future development should be at an appropriate distance to preserve their setting and overall integrity.

Ownerships

1.80 There are two principal landowners for the Interface Land; SEEDA, shown in orange on the plan on page 15, and the Chatham Historic Dockyard Trust, shown in blue. The Trust's interest relates to the Mast Ponds and surrounding land, whilst SEEDA owns the riverside land and that adjacent to Brunel Way/ Pembroke Rise. A considerable constraint to any future development is the condition that built development (ie buildings) can not be sited over ownership boundaries.

Character Areas and Zonal Planning

1.81 As a result of the preceding constraints, it has become apparent that a series of distinct areas exist within the Interface Land. These areas are set out below and form the basis of the emerging development framework.

- Riverside;
- North and South Mast Ponds;
- Brunel Way;
- Pembroke Rise; and
- Covered Slip.

Riverside

1.82 Comprises land reclaimed from the River Medway circa 1840 and historically housed no substantial buildings. Due to its location to the north west of the site and distance from the Historic Dockyard, this area contains valuable development land which can optimise the proximity to the River and the views afforded.

North and South Mast Ponds

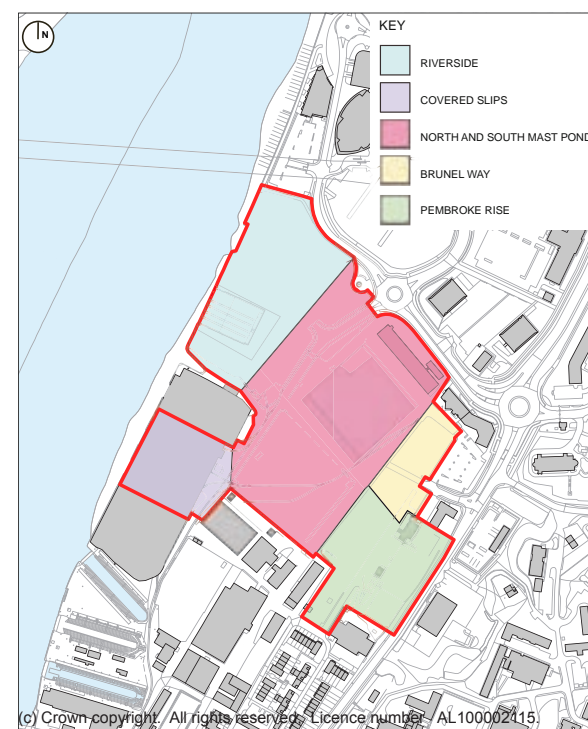
1.83 Currently a hostile open space disconnected from the Historic Dockyard. There is the opportunity to make this location the heart of any new development and a future landmark for visitors and occupants alike.

Brunel Way

1.84 In recent history, this location housed a large office complex associated with the Naval Dockyard. New commercial development has since been reintroduced on the periphery and the opportunity exists to create a commercial centre adjacent to the historic buildings and features.

Pembroke Rise

1.85 This part of the Historic Dockyard was closely linked to the age of sail and the introduction of new technologies. Now largely vacant the land offers the opportunity for re-use and the introduction of new forms of development which



Plan showing Character Zones

utilise the raised land and vistas.

Covered Slips

1.86 These historic, dominant features present the opportunity for conversion to a range of uses.

Future Development Requirements

- 1.87 It is important to promote development of an appropriate scale and character. However, due to the setting of the Interface Land it is essential that flexible and responsive policies are included to enable economic and site-specific circumstances to be fully taken into account. This strategic approach and need for flexibility reflects the balance between achieving objectives of massing through mixed-use development and acknowledging heritage constraints and the heritage assets.
- 1.88 In order to respect and enhance the urban environment and the character of the surrounding area, the Interface Land buildings should respect the industrial heritage by being simple in footprint and outline and being of an appropriate scale.
- 1.89 Flexibility is important in considering the form of residential accommodation. In parallel with market and affordable housing student accommodation should also be considered to support the growth of the higher education sector adjacent to, and within the Dockyard.
- 1.90 This document has been prepared to make the most efficient and effective use of land and buildings. It has been developed to avoid designating sites for single or restricted Use Classes wherever possible, instead planning for and facilitate a supply of land which can cater for residential, business and employment needs but is flexible enough to be responsive to a changing economy or new business requirements.
- 1.91 High quality urban design and architecture must be achieved to ensure integration with the, and protection of the setting of the Historic Dockyard.

Residential Development

- 1.92 In order to create a sustainable and inclusive community, it is essential that any residential development proposed for the Interface Land provides the appropriate quantum of affordable housing; the Council will seek to achieve its target policy of 25%. It is important to ensure that there is an appropriate mix of tenure and both house types and sizes provided to meet the needs of different types of households and to encourage the development of mixed communities.
- 1.93 A strategic approach will be taken towards the provision and location of affordable housing within the Interface Land. T.

Economic and Commercial Development

- 1.94 It is important that the design of new commercial development addresses the challenges of climate change and the pressures on the natural and historic environment.

Social Infrastructure

- 1.95 To minimise the need to travel and to create inclusive communities, the proposed development should seek to include key ancillary facilities which are not available in the immediate vicinity of the site, this includes convenience retail facilities and supporting community facilities.

Contaminated Land

- 1.96 Contaminated land is a material consideration under the land use planning process. Given the site's industrial past, there is the potential for the site to be affected by the presence of contamination. Where contamination is present, it is recommended that the land be capped prior to any new development.

Purpose of the Development Framework

- 2.1 This Development Framework provides legislative guidance to direct future development and investment for the Interface Land. It provides certainty for the future renewal of the area by presenting clear guidance on the acceptable spatial arrangement and form of redevelopment for the study area. It fulfils the landowner's requirements to enable a gradual transition from the successful new developments of Chatham Maritime to the north and the Historic Dockyard to the south, whilst reflecting heritage awareness and respect for the sensitivities and development constraints of the site.
- 2.2 The Development Framework is a three-dimensional proposal affecting physical, economic and social development. It is expressed as a series of plans, illustrations and through descriptive analysis of the proposed design approach to the future redevelopment.
- 2.3 The Development Framework reflects and responds to the study area's character and historic functions. This will ensure the development of a sense of identity for the place and contribute to the character and richness of any new development.



Photo showing the Brunel Saw mill



Photo showing the Covered Slip No. 7



Photo showing Wheelright's Shop, the Old Forge and the Mould Loft in front of Covered Slips 4, 5 and 6.



Photo showing the Police House

How the Development Framework has been Evolved

- 2.4 The Development Framework has been evolved as a collaborative exercise by a team including: CZWG who are the principal Masterplanners; WSP who advised on transport, flood risk and environmental elements and also carried out the Sustainability Appraisal, published in parallel to this SPD; and Turley Associates dealing with Planning, Urban Design and Heritage issues.
- 2.5 SEEDA, Chatham Historic Dockyard Trust, Medway Council, and English Heritage have been consulted on the proposals, with the latter advising on all heritage issues. There have been a series of workshops with these bodies to develop the Development Framework and respond to issues as part of an iterative process.
- 2.6 The entire Historic Dockyard is a site of national archaeological importance and a number of key features, have been identified within the Interface Land.

Key Features of the Interface Land

Internal

- South and North Mast Ponds
- Covered Slips 5 and 6
- Brunel Canal route and features
- Lower Boat House
- Police House
- Timber saw pits and houses
- Former timber and stacking yards
- Original 18th century sea wall
- Original 18th century dockyard wall
- 19th century boat slip
- Existing dockyard wall
- Visual connections

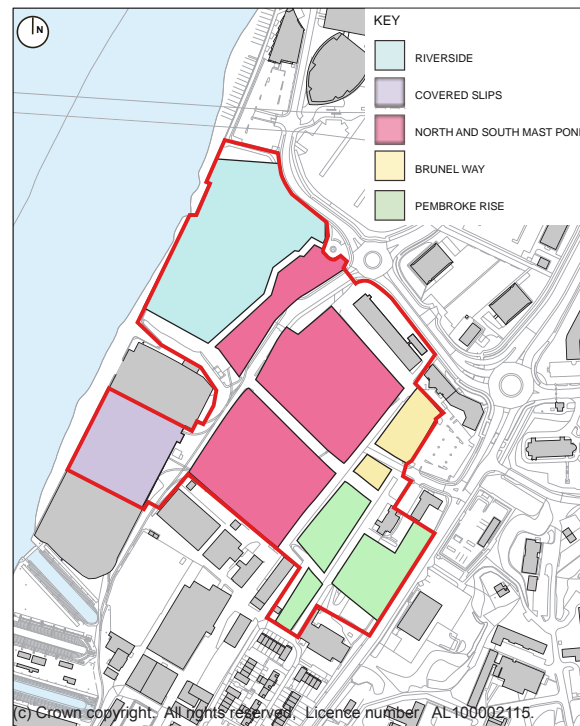
External

- Covered Slips 3, 4 and 7
- Brunel Saw Mill
- Mast House and Mould Loft
- The wider Historic Dockyard
- Visual connections

- 2.7 The Development Framework reflects and responds to the infrastructure of the heritage buildings, the industrial past and the development of pioneering technology. It also fixes the key urban design principles that will shape the form and layout of the site.
- 2.8 Any future planning applications for development within the Interface Land will be tested against this Development Framework. Failure to comply with this SPD or to demonstrate that the design solution is equal or superior to the Development Framework is likely to result in a refusal of planning consent. To this end any planning application for this site must be accompanied by a thorough design statement that explains how it complies with, or departs from the SPD, the Development Framework and the design parameters.

Overarching Organising Principles

2.9 The Development Framework Plan provides structure that demonstrates how the development blocks for the site are arranged. These have been shaped by the existing constraints, identified in the previous section, and proposed organising principles. The latter is defined as principles that when applied will contribute towards achieving the SPD's vision for the development proposals within the Interface Land.



Plan showing the emerging Development Framework

In summary the key organising principles are:

- enhancing of the heritage features;
- protection of the strategic and local views;
- block scale, massing and height parameters;
- permeable movement and parking requirements;
- public realm treatment; and
- drainage and flooding management .

These are explained in the following sections.

Heritage Enhancement

- 2.10 The 'Chatham Dockyard and its settings World Heritage Site Management Plan' provides a framework for understanding how the development site relates to the proposed World Heritage status. The heritage enhancement of the Interface Land has the opportunity to bring a significant derelict part of the dockyard back in to positive use and can provide an exemplar framework of how contemporary developments can enhance the proposed World Heritage Site.
- 2.11 Future development should consider the setting of heritage assets both within and outside the Interface Land and also reflect and respond to identified underground historical features. Where appropriate these features should be used to create focal points. This will help to reinforce the sense of place and local distinctiveness.

Protecting Strategic and Local Views

- 2.12 The strategic views and sensitive local views are a significant factor in recommending the design solution for the site. These views are assessed in the Building Heights Assessment (and illustrated on page 12 of this document, appended to this SPD. It must be recognised that every view across the site cannot be protected but needs to be considered through future design.

Block Scale, Massing and Height

- 2.13 Development within the Interface Land should relate to the scale, height and massing that exists within the surrounding area. Block resolution, namely shape and size of blocks, has not been

defined through the Development Framework. This will allow a degree of flexibility over time, and it is hoped will allow future developers the freedom to explore imaginative solutions that fit.

- 2.14 The contextual analysis has concluded that the Development Framework should be developed as a series of character zones. Future buildings must meet the defined scale set out in the character zone parameters. Buildings should also face onto the streets to define block edges. It is also important that heights are not consistently applied across the site or across development blocks in order to provide a varied roofscape.

Movement and Parking

- 2.15 The street network should be legible and connect with surrounding areas. The existing road network will not change greatly with the primary access from Main Gate Road, from Western Avenue, and limited access from Brunel Way and Dock Road.
- 2.16 Streets should be designed to encourage walking, provide platforms for social interaction and facilitate the safe interaction between pedestrians, cyclists and cars. Service vehicles should be subservient to pedestrians and cyclists.
- 2.17 Parking needs to be carefully located within the development and tightly controlled. This will ensure that vehicles do not become the dominant feature within the street. On this sensitive site a demand based approach to establishing car parking levels should be adopted to minimise the adverse effects of over or under provision of parking on the quality of the development. Parking provision should be convenient to the

users so that parking will be used as intended.

Public Realm

- 2.18 Public realm treatments will have the opportunity to enhance the Interface Land development. An integrated strategy for the public realm, including lighting, street furniture, signage and hard and soft landscaping should be developed for the whole of the Interface Land. Innovative public realm treatments will be expected in order to complement the specified high quality buildings.

Drainage and Flooding

- 2.19 As has been previously identified, parts of the site are within Flood Zones 3 and 2, therefore the development will need to ensure that flood risk on and off site is mitigated in line with advice in PPS25 and from the Environment Agency. The drainage of the site should follow environmentally sound principles including, where possible, the use of Sustainable Urban Drainage Systems (SUDS) as, for example, permeable paving, green roofs, rainwater harvesting and grey water recycling.

Riverside

- 2.27 The Riverside area is located to the northwest of the Interface Land, adjacent to the primary entrance off Western Avenue and adjacent to the River Medway. The land, which was reclaimed in the 18th century, has no tradition of substantial buildings or built form, therefore offers an opportunity for development that does not have a close historical precedent to the Dockyard.
- 2.28 The riverside presents the opportunity to animate a current dormant waterfront edge through a viable residential led mixed-use development. This use, along with the Riverside Walk which will connect with the rest of the Chatham Maritime Riverside Walk, will provide activity and open up the site around the river's edge whilst reaffirming the historic relationship between the river and the waterfront of the original Dockyard. Through the orientation of buildings the area also has the opportunity to define the Main Gate Road and create a principal entrance to the Dockyard for visitors and occupants alike.
- 2.29 Any development within this zone must not harm the current dominance of the Covered Slips, especially in views from across the River Medway, Upnor and Upnor Castle. The mass and scale of the buildings must also allow views from the west to the ridgeline to the east, whilst respecting the grain of the area.
- 2.30 The alignment of any future development is a key design consideration, and the orientation of buildings must be encouraged to maximise natural light. In addition, the North Mast Pond drain must be retained and linked to the River Medway.
- 2.31 The 18th century Dock wall should be retained in situ and recognised through alignment

The design guiding objectives for the Riverside area are as follows:

- Strong gateway into the Historic Dockyard from the Interface Land.
- Proposed development must respect the scale of the Covered Slips as the dominant feature within current views. In addition, pedestrian access must be retained along the River Medway.
- The river is fundamental to the historic use of the Dockyard and therefore should retain a connection with the other development zones.
- The North Mast Pond drain should be protected and where necessary upgraded to ensure the River Medway continues to drain the pond.
- Provision of a Riverside Walk to link in with the existing footpath.

and design. It is recommended that the wall is protected from below-ground piling and development, and waterlogged conditions are retained to avoid the future unseen decay of the wall.

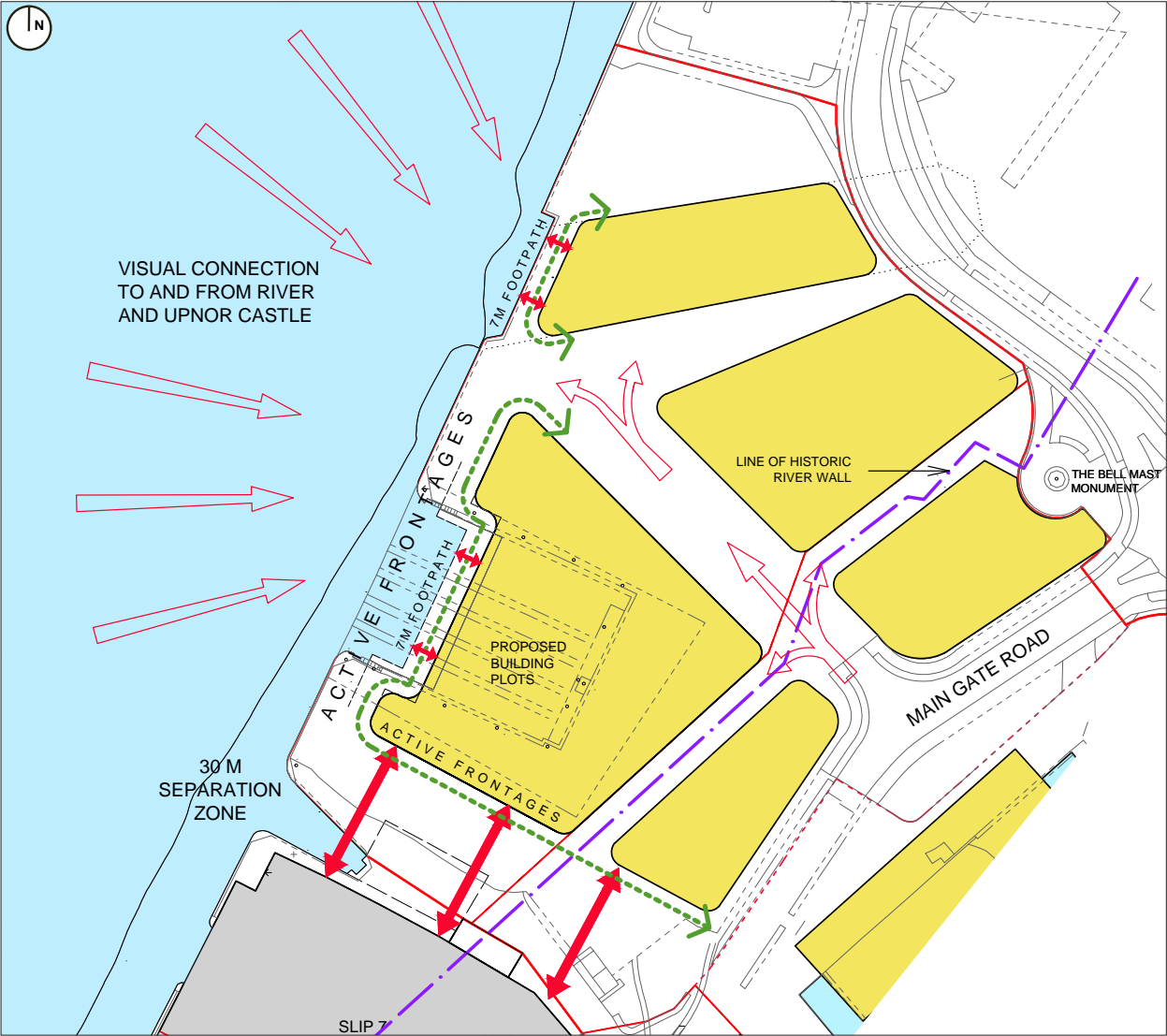
- 2.32 This area will accommodate the densest form of development within the site, it is therefore essential to ensure the built form does not deter from the existing views, both internal and external and is sympathetic to adjacent slip buildings.
- 2.33 A building height assessment undertaken as part of this SPD exercise concluded that the setting of the covered slips should be protected by a no build buffer zone of 30 metres and recommended that development should step up away from the Slip buildings.

Parameters

Land Use	Residential led mixed use development of up to 65,000 sq m
Building Heights	Up to ten storeys
Massing	Large to medium scale



Possible view into the Riverside Zone from the River Medway



Development opportunities within the Riverside Zone



Proposed view from the Medway onto the Riverside Zone



Late 19th century slip



View from the south west corner of the Riverside Zone to Upnor/Upnor Castle

North and South Mast Ponds

- 2.34 Chatham is unique in having two Mast Ponds and whilst it is not a requirement to restore the South Pond to its original sunken and wet state; its historic use needs to be acknowledged within the future development proposals.
- 2.35 The North Mast Pond will be retained in its existing setting and will form a dominant feature of the Interface Land. It is important that it continues to use the River Medway for drainage.
- 2.36 New buildings will respect the scale of the adjacent Covered Slips and Mould Loft and boat houses. Views to the Slips should be retained, where possible, over the building lines and any new development.
- 2.37 The Lower Boat House is the only timber framed storage shed remaining in close proximity to water and should be recognised accordingly. It should retain its current relationship with the North Pond.
- 2.38 Car parking must be removed from the site of the South Pond to improve the setting of the historic environment. The re-introduction of water is not essential in conserving the South Pond. However future development will acknowledge its previous form.
- 2.39 There is the opportunity to have an area of flexible space within the South Pond. This will retain open views onto the façade of Covered Slip 7, Mast House, Mould Loft and the Galvanising Shop. This space could accommodate public events/summer festivals which will act as a stimulant for visitors to the Dockyard.
- 2.40 The line of the Brunel Canal should form a dominant feature within future development

The design guiding objectives for the North and South Mast Ponds are as follows:

- The two Mast Pond areas should be considered together, but detailed separately.
- Future development should be on a north/south axis.
- Reintroduction of water within the South Pond is not essential, however its historical use must be recognised through design.
- The setting of the Lower Boat House should be protected.
- There should be a visual representation between the Saw Mill and the South Mast Pond, and the river.
- Removal of the Western Avenue fence to encourage connectivity with Chatham Maritime.



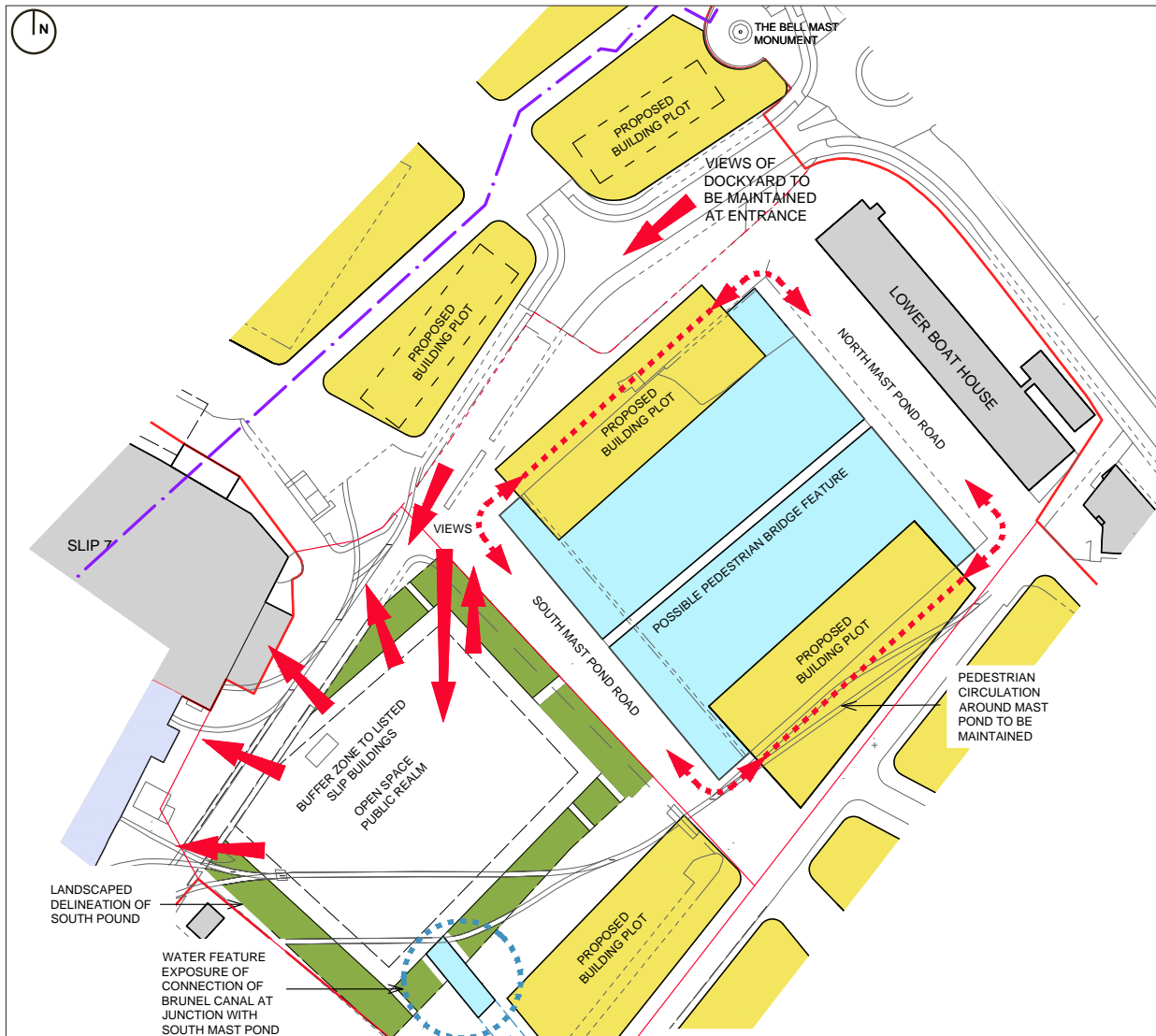
Section through the North Mast Pond (looking north)

proposals. This feature and the Saw Mill have a historic functional relationship with the South Mast Pond and therefore should be visually linked. This should include opening/displaying the shaft of the Canal at the point where it meets the Mast Pond and also the shaft where it links to the Saw Mill.

- 2.41 The proposals should illustrate the Dockyard's timber related past. Buried archaeology, including saw pits, should be investigated and if this cannot be preserved in situ, eg beneath new buildings, it should be recorded. A representative example of saw pits should be revealed and interpreted. Policy HE12 of PPS5 relates to recording of heritage assets which, here, means more than just buried remains
- 2.42 Future development is recommended to be built on a north/south axis to promote visual linkages with the historic dockyard buildings and the core of the historic dockyard. Pedestrian circulation around the water must be retained through recognised pathways or setting the buildings back from the water's edge. This would ensure that the historic movement pattern and function of the pond is reflected through future design.
- 2.43 This area falls within 1 in 200 year tidal flood extent and this would need to be considered as part of the detail design of any future development.

Parameters

Land Use	Mixed use development of up to 12,000 sqm
Building Heights	Up to three storeys
Massing	Medium scale



North and South Mast Pond Zone - inspirational development blocks



Proposed view from Main Gate Road, looking southwards



Proposed view from South Mast Pond Road, looking westwards



Thames Barrier Park, illustrating a form of public realm that could be developed for the South Mast Pond

Brunel Way

- 2.44 Brunel Way is located to the northeast of the Interface Land. This area is primarily accessed from the Main Gate Road and the South Ponds Road, with limited access from Brunel Way.
- 2.45 It is important that any built development at Brunel Way does not infringe the setting of the North Mast Pond. A further constraint to guiding development is the ownership boundary, across which development can not take place as highlighted in the strategic framework. At this stage, there is therefore no guarantee of unimpeded access from Brunel Way to the Mast Pond area.
- 2.46 The zone should be developed with a largely commercial focus and link in with the adjacent existing office and hotel buildings.
- 2.47 This area was historically used for timber stacking and therefore future development should acknowledge this part of the Dockyard's timber related past. Historically this land also accommodated a number of saw pits. It is therefore recommended that a set of saw pits is conserved and restored as a historic feature of the site (alternatively this could be done within Pembroke Rise).
- 2.48 This area falls within 1 in 1000 year extreme flood extent and this would need to be considered as part of the design of the buildings.

The design guiding objectives for the Brunel Way are as follows:

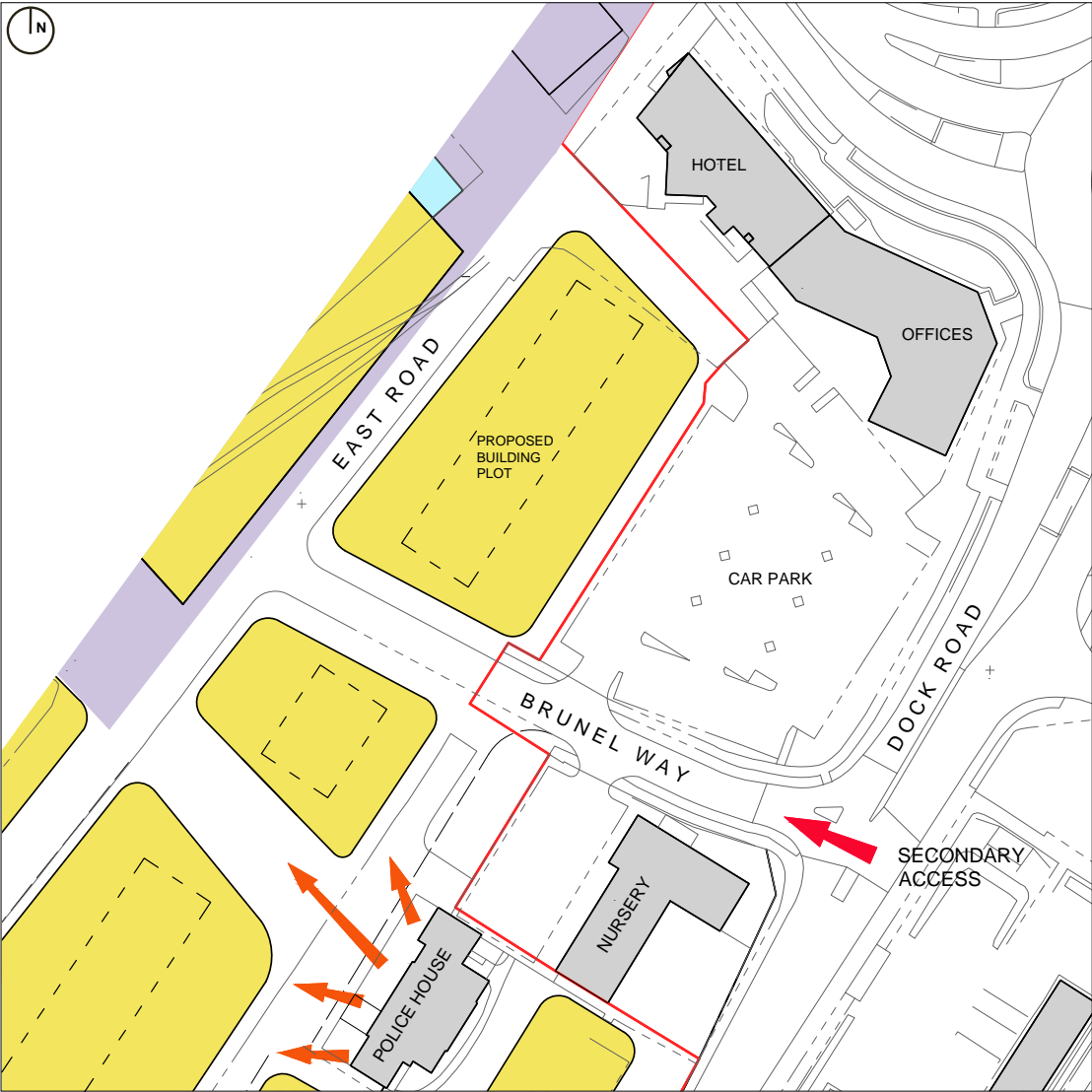
- Predominately commercial in land use and medium scale massing.
- Acknowledge in some form the historic use of the site. This could be represented through design or materials.
- The Police House forms a major landmark in this location; it is important that it is reused as part of any future development proposal.

Parameters

Land Use	Commercial of up to 6,000 sq m
Building Heights	Up to four storeys
Massing	Medium scale



Brunel Way, looking southwards



Development opportunities within the Brunel Way Zone showing proposed development blocks



Watermans Place, Leeds and Black Lion Street, Brighton - precedent images



Existing view of the Lower Boat House, the Hotel and Office Block

Pembroke Rise

- 2.49 Pembroke Rise is located in the southeast corner of the Interface Land and includes the highest land within the Site. This zone is closely related to the South Mast Pond and has a finer grain to the other zones, to reflect the adjacent southern grain.
- 2.50 Brunel Sawmill, along with the associated saw pits and timber stacking yards, must be considered together to reflect their historic use (timber would be floated through the canal and tunnel to the shaft at the base of the Sawmill. When the wood was sawn into planks, it was moved by carriage to the stacking yards and seasoning sheds). It is important to integrate the building with the Canal and adjacent stacking yard to maintain its historic illustrative and associative significance.
- 2.51 The early 18th century Dockyard Wall (with tower houses) forms an underground feature within the site located to the south of the Police House. Whilst, this is not a Scheduled Ancient Monument it is an archaeological historic area of significant interest and illustrates the early Dockyard boundaries. It will be important to highlight this through design features. The existing dock wall will be retained and illustrates the growth associated with the Dockyard during the historic age of sail.
- 2.52 This area was historically used for timber stacking and therefore future development should acknowledge this part of the Dockyard's timber related past. Historically this land also accommodated a number of saw pits. It is therefore recommended that a set of saw pits is conserved and restored as a historic feature of the site (alternatively this could be done within Brunel).

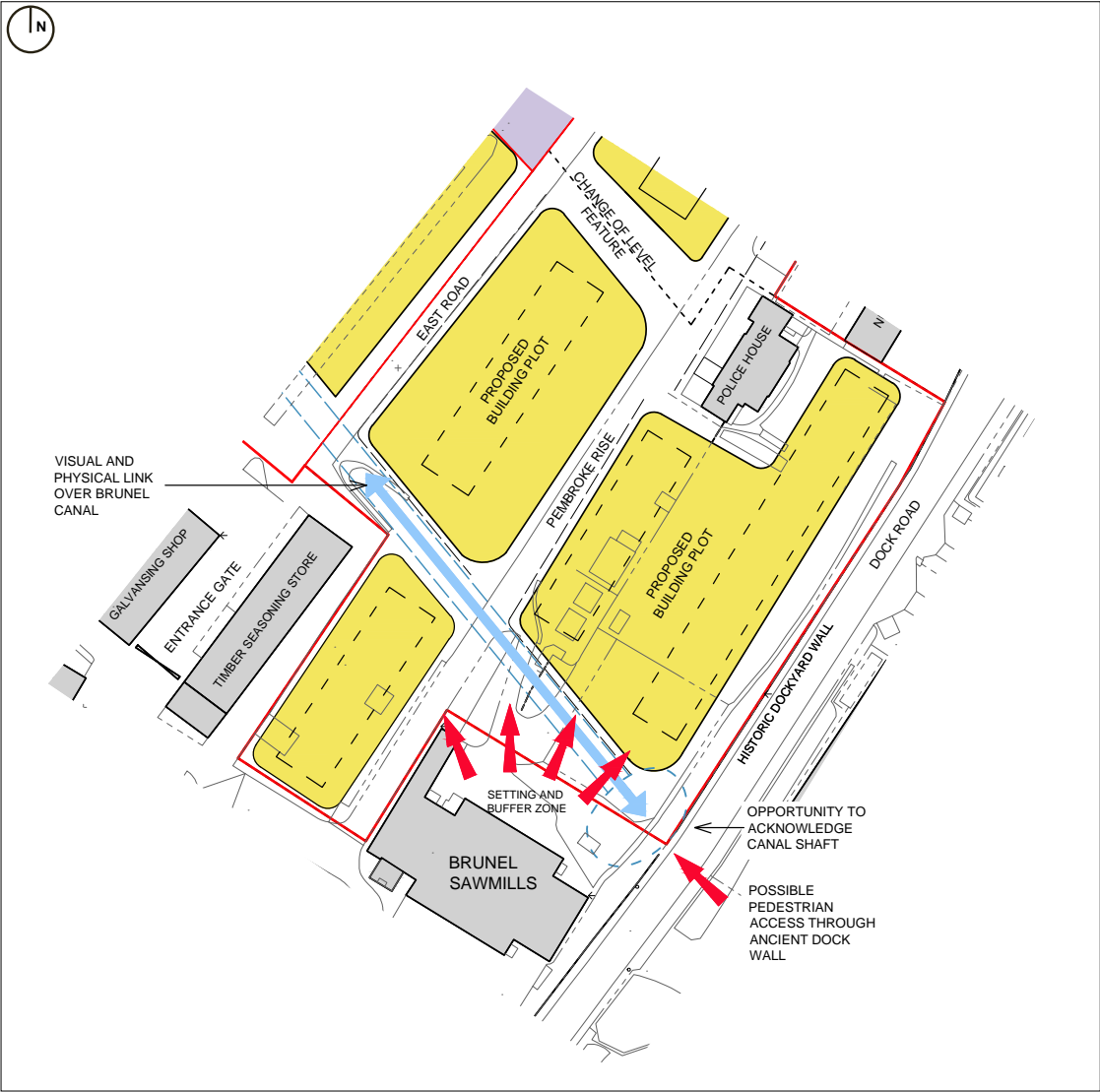
The design guiding objectives for Pembroke Rise are as follows:

- Predominately residential in use presented using traditional housing models.
- The urban form should define the high quality residential in character public realm areas.
- An access point could be established through the existing Dockyard wall.
- Acknowledge the historic timber stacking use of the zone through design and materials.
- Retain the visual link between the Brunel Canal, the Saw Mill and the South Mast pond.
- Consider views from the Police House and how future development could impact upon these.

- 2.53 There are a number of existing bricked up doorways punctuating the existing Dock Wall. These offer the opportunity to provide a pedestrian link onto the Dock Road without harming the setting of the wall. It is envisaged that only one access point will be necessary and that this could be on the alignment of the Brunel Canal, therefore utilising views through the site to the riverside and historic Slip buildings.
- 2.54 The listed Police House, which is located to the north of this zone, has never historically had a formal setting within the dockyard and should be carefully assimilated into future development proposals. There are important views from the building and the integrated cupola across the dockyard that should be considered in future masterplanning exercises.
- 2.55 The zone will be a largely residential area and provide a mix of dwelling types.
- 2.56 This area falls within 1 in 1000 year extreme flood extent and this would need to be considered as part of the design of the buildings.



Aspirational section through the Pembroke Rise Zone



Development opportunities within the Pembroke Rise Zone

Parameters

Land Use	Primarily residential up to 14,000 sq m
Building Heights	Up to three storeys
Massing	Medium to small scale



The Police House



Photo showing Upnor Castle from the raised ground a Pembroke Rise

Covered Slips Buildings

- 2.57 The last major period of construction of Dockyard buildings and structure took place between 1838 and 1855 when the covered Slip buildings were constructed. The Slips illustrate the Dockyard’s transformation to wooden ship building under cover and then the introduction of iron ship construction, and finally work on submarines . They form fine examples of the technological advancements that were achieved at Chatham and represent the transition towards machine-based manufacturing and the Industrial Revolution.
- 2.58 The scale and collective volume of the covered Slip buildings ensure they form the dominant feature of the Dockyard fronting the River Medway. It is important that the setting of these buildings is protected in the context of views from the Dockyard and river.
- 2.59 It is recognised that the future development potential of the Slip buildings is limited. However, due to the potential loss of parking on site through the projected redevelopment of the South Mast Pond, the Covered Slip buildings are proposed to be converted to house those car/coach parking spaces which have been displaced from the South Pond. This will be provided through a tiered parking structure and would also accommodate a proportion of the existing Historic Dockyard parking. This would provide the building with a sustainable use, whilst not detracting from its historic interest.
- 2.60 Any refurbishment within the Covered Slips should not detract from the visible form of the cast iron structure. There is also likely to be archaeological remains and therefore

The design guiding objectives for the Cover Slip buildings are as follows:

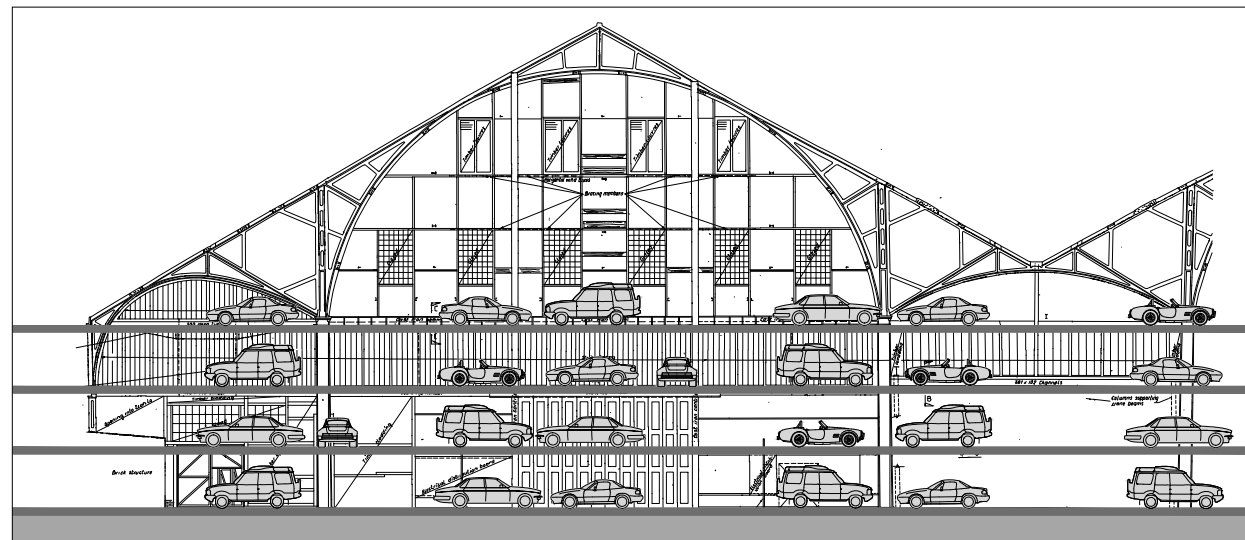
- Any proposed structure should not detract from the scale of the Covered Slip.
- The setting of the Slip buildings should be protected through development guidance for the other zones within the Interface Land, particularly the riverside.
- The view to the Covered Slip buildings should be retained from the principal access along Main Gate Road.

foundations could present an issue and should be investigated in greater detail as part of future development proposals.

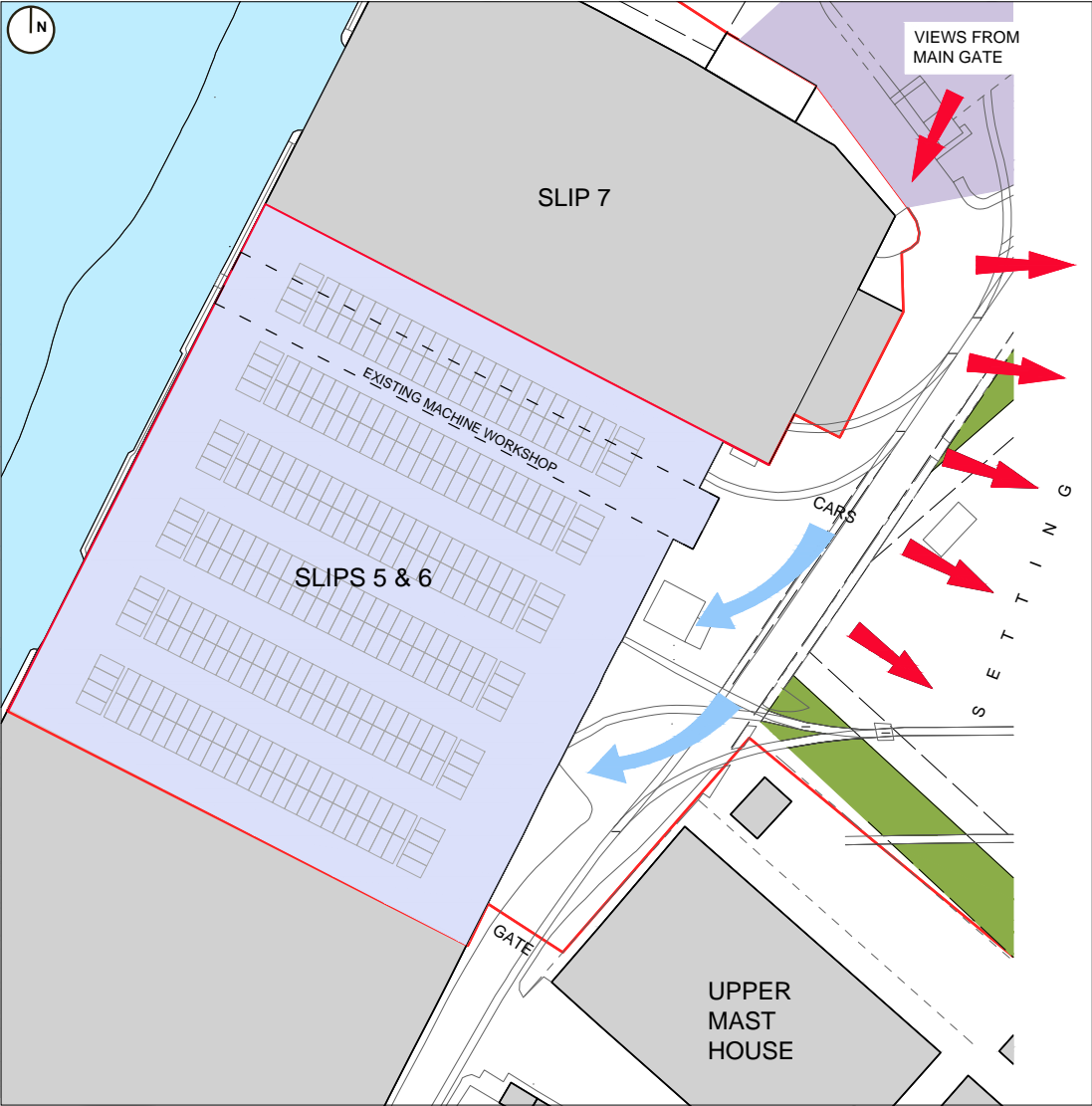
- 2.61 This area falls within 1 in 200 year tidal flood extent and this would need to be considered as part of the design of the structures and refurbishment.

Parameters

Land Use	Up to 800 car parking spaces. Parking for twelve coaches
Building Heights	Up to four floors
Massing	Large to medium scale



Proposed section through the Covered Slip



Indicative Layout of the Covered Slips Zone



Covered Slips 4 and 5



Roof structure inside the Covered Slip



Section illustrating tiered car park structure

Movement Strategy and Parking

Development Mix and Travel Plan

- 2.62 The highway network surrounding the site is under pressure and junctions such as the Wood Street roundabout (south of the site), suffer from congestion at peak times. These would need assessment to determine the extent of improvement required but would need a strong and effective Travel Plan to minimise this impact.
- 2.63 The Travel Plan will need to demonstrate that significant efforts have been made to minimise the need to travel by car and reduce the impact of car traffic on the surrounding highway network.
- 2.64 Should a substantial element of residential development be proposed, a range of complementary uses should be accommodated on the site as listed within the attached Transport Strategy to minimise the need to travel outside the site for most day to day journeys. This should aim to exceed the SEEDA sustainability checklist criteria.

Pedestrian and Cycle Access

- 2.65 Opening an entrance in the Dockyard wall can help to minimise walk distances and maximise the opportunity for walking and cycling from the development. The layout of the site should be designed to accommodate pedestrian and cycle desire lines without significant diversion to maximise opportunities.

Future development should seek to:

- Minimise the need to travel outside of the site.
- Maximise the potential for travel by non-powered modes (foot and cycle) to key local destinations outside of the site.
- Optimise access to sustainable transport networks (bus and rail) and key routes which give access to important nearby areas.
- Minimise the need to travel by car, and especially for single occupancy car journeys.
- Deliver a site layout which provides sufficient parking to meet demand for car ownership at residential uses, and which provides an appropriate amount of car parking at commercial elements of the development and for the existing Dockyard as a visitor attraction, taking into account the other elements of the Transport Strategy.
- Minimise the impact of the proposed development of the surrounding highway infrastructure through the implementation of the Transport Strategy, and mitigation measures to off set residual impacts.

- 2.66 A review of key pedestrian and cycle routes outside the site should be undertaken and in particular improvements along the Dock Road corridor should be considered to improve access to Chatham and Brompton High Street, which may include improved crossing facilities south of Wood Street roundabout.

Site Access

- 2.67 The main access will be from Main Gate Road with secondary access from Brunel Way and Dock Road. Access through the site from Main Gate Road to the Historic Dockyard will need to be provided for the significant dwellings and businesses that exist within the area. Access routes should be designed in accordance with Manual for Streets, to have a significant place function, minimising vehicle speeds through integrated design rather than retrofitted calming measures.

Public Transport

- 2.68 Bus access to the site along Dock Road is currently good, and whilst some consolidation or improvement of services may be considered to reduce vehicular travel demand, this is not considered necessary to make the site acceptable for development. It is not expected that services would need to divert into the site. However, improvements to seating and shelters, and consideration of real time information would help increase the attractiveness of this mode. A corridor review of bus priority measures should be considered to identify ways in which public transport services could be made more attractive than car use to access Chatham town centre and the railway station.

Parking

- 2.69 The proposed developments should accommodate existing car parking demand together with the needs of new development proposed.
- 2.70 Local car parking policy is under revision and interim standards advocate minimum rather than maximum standards for residential development. Notwithstanding the interim or any further standards published, a demand based approach to establishing car parking levels should be adopted on this site to minimise the adverse effects of over or under provision of parking on the quality of the development.
- 2.71 Parking provision should be convenient to the users so that parking will be used as intended.
- 2.72 It is anticipated that the Slip building could be used to accommodate replacement parking for some 400 cars and twelve coaches for the Historic Dockyard from the site of the South Mast Pond, along with parking related to development within the Interface Land. Replacement parking for the existing hotel would be required to free up the area to the west of the North Mast Pond.

Public Realm Proposals

- 2.73 A public realm strategy for the Interface Land will ensure that the unique development zones are unified through a similar palette, and that the proposed development will be cohesively linked in with the existing buildings.
- 2.74 Public realm also offers an opportunity to both fill the voids where buildings once stood and reflect the outline of features that did, or still do, exist underground. This will respond to the historic context of the site. Soft landscaping was not a dominant characteristic within the industrial grain of the Dockyard and should be limited to appropriate locations such as the area to the east.
- 2.75 Site specific public art should be carefully integrated into the proposals as part of an overall public realm strategy. Also signage should be located at key nodal points within the proposals to enable the user or visitor to orientate themselves within the development.

A series of high level design principles have been developed to inform the development of the public realm strategy and are applicable to each area:

- Adopt a simple, high quality palette of hard landscape materials that balances lifetime costs, aesthetics, durability and sustainability.
- Maximise opportunities to translate the historic features of the Interface Land.
- Create distinctive and incidental spaces defined by and sharing a close relationship with architecture, creating well-defined hard civic spaces and streets whilst integrating green spaces such as courtyards and gardens within the residential and mixed-use areas.

Design Principles

2.76 In order to achieve the vision for the Interface Land of creating high quality proposals with a distinct character that responds to its surroundings context the following Design Principles have been identified that any proposals should adhere to.

<p>Context and Heritage</p> <p>The design solutions must be sensitive and respect the historic past of the Interface Land. The study area should also respect the character of the surrounding local context.</p> <p>Scale and Proportion</p> <p>In order to respect the character of the surrounding area, future development within the Interface Land should respect the industrial heritage by being simple in footprint and outline and being of an appropriate industrial, not domestic, scale. New buildings should form a harmonious group with their older neighbours.</p> <p>Physical Connectivity</p> <p>The Interface Land plays an important part in linking Chatham Maritime and the Historic Dockyard areas. The street network should be legible and connect pedestrians, cyclists and vehicles with surrounding areas, particularly to and from the Historic Dockyard. Shared surfaces should be used, where appropriate.</p>	<p>Visual Connectivity</p> <p>The redevelopment of the Interface Land must not reduce the visual connection to the Covered Slip buildings, which form a predominate feature within the views to the Historic Dockyard; but not at the exclusion of other important views.</p> <p>Archaeology</p> <p>The study area is of national archaeological importance, and has a number of potential buried remains and archaeology of standing structures. Care needs to be taken to protect any unknown or known underground structures such as the associated tunnel and shafts, the old river wall and the timber Saw Pits associated with the Brunel Saw Mill. For a comprehensive list of the assets within the site, please refer to the various archaeology structures that have been produced for the site.</p> <p>Highest Design Quality</p> <p>Design proposals should preserve and enhance the setting of Interface Land. They will have to be of the highest design quality, be valued enough to keep for future generations and must be adaptable to a variety of uses over time.</p>	<p>Uses</p> <p>Suitable levels of a compatible mix of uses will be encouraged within the Interface Land to create a high-density, sustainable development. At this stage a flexibility in the future use of buildings is encouraged to ensure potential occupiers are not constrained through planning policy and use classes.</p> <p>Appearance and Materials</p> <p>The need to respect the historic context of the Interface Land will affect the material palette, with the choice and application of materials being an important consideration.</p> <p>Flexibility and Inclusive Design</p> <p>New development should be designed to allow maximum flexibility to ensure that it can accommodate the changing needs of its users over time. Also it should meet the standards of accessibility and inclusion.</p>
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Sustainability

To reduce the impact of the development good practice sustainability principles should be applied to the Interface Land. This will include achieving high sustainability standards, innovative design, technology and construction solutions to climate change challenges.

Secure by Design

Security provision for internal and external environments needs to be appropriate for scheme locations and building types and should reflect advice from the Police Architectural Liaison Officer prior to the detailed planning stage.

Reuse of Historical Buildings

The reuse of historic buildings is promoted within the site to provide a clear link to the past and help reinforce a sense of place, local identity and character. This site will need to change in future in ways that we cannot yet predict and so individual heritage assets, where appropriate, and the site in general should retain maximum flexibility in order to increase the chances of having a sustainable future.

Development Framework Summary

- 2.77 The development framework provides guidance on the spatial arrangement and form the proposal should take within the Interface Land. It also addresses how the identified vision for the Interface Land can be achieved through the consideration of the identified constraints, the overarching organising principles, the character zone typologies and the design principles.

The Need for a Delivery Strategy

- 3.1 The Supplementary Planning Document (SPD) provides the opportunity to 'kick-start' the process and that it can be used by the Council and its partners as a promotional tool in addition to its use in determining planning applications through the Development Control process.
- 3.2 This part of the SPD sets out the delivery strategy for Chatham Dockyard Interface Land. An Implementation Plan is required in order to ensure that the Spatial Masterplan contained in this SPD is achieved on the ground.

Highlighting the Key Roles and Responsibilities of all Key Public and Private Sector Organisations

- 3.3 While the identification of appropriate delivery mechanisms is clearly an important issue, it is equally (if not more) important to ensure the commitment of the key stakeholders within the regeneration sector. As part of the process of securing the commitment of the key stakeholders it will be fundamental to identify their respective roles and responsibilities.

Statutory Consultees

- 3.4 English Heritage has a particular interest in the Interface Land, as the site encompasses a location of international maritime heritage significance. English Heritage has taken a positive and proactive role in the recent strategic masterplanning exercise and the preparation of this SPD. The close involvement of English Heritage will continue with regard to the emerging detailed proposals.

3.5 CABE also has a key role to play regarding the Interface Land. This SPD has been influenced by the CABE guidance: Creating Successful Masterplans – Guide for Clients. It is also expected that significant development proposals for the Interface Land will be the subject of CABE and local Kent Architectural Panel's Design Reviews.

3.6 The Environment Agency clearly has a role with regard to the redevelopment of this significant riverside site. The EAs guidance has been sought with regard to this Masterplan, and should also be engaged with at the pre-application stage.

3.7 Natural England should also be consulted by applicants at the pre-application stage.

Phasing Strategy

3.8 The Council supports the principle of a phased approach to the delivery of comprehensive development for the Interface Land. It is unlikely that a single 'one size fits all' delivery mechanism will be appropriate and effective across all the sites within the strategic masterplan area. As set out in the preceding sections, the sites have a number of discrete physical or 'character' areas which may face different regeneration issues and challenges, and which may require the intervention and support of a range of different organisations or partners.

3.9 Future development proposals should be subject to a detailed comprehensive phasing strategy and implementation plan.

3.10 Infrastructure works to provide open space public realm across the Interface Land should not be the last works to be carried out unless adequate financial controls/penalties are in place to ensure

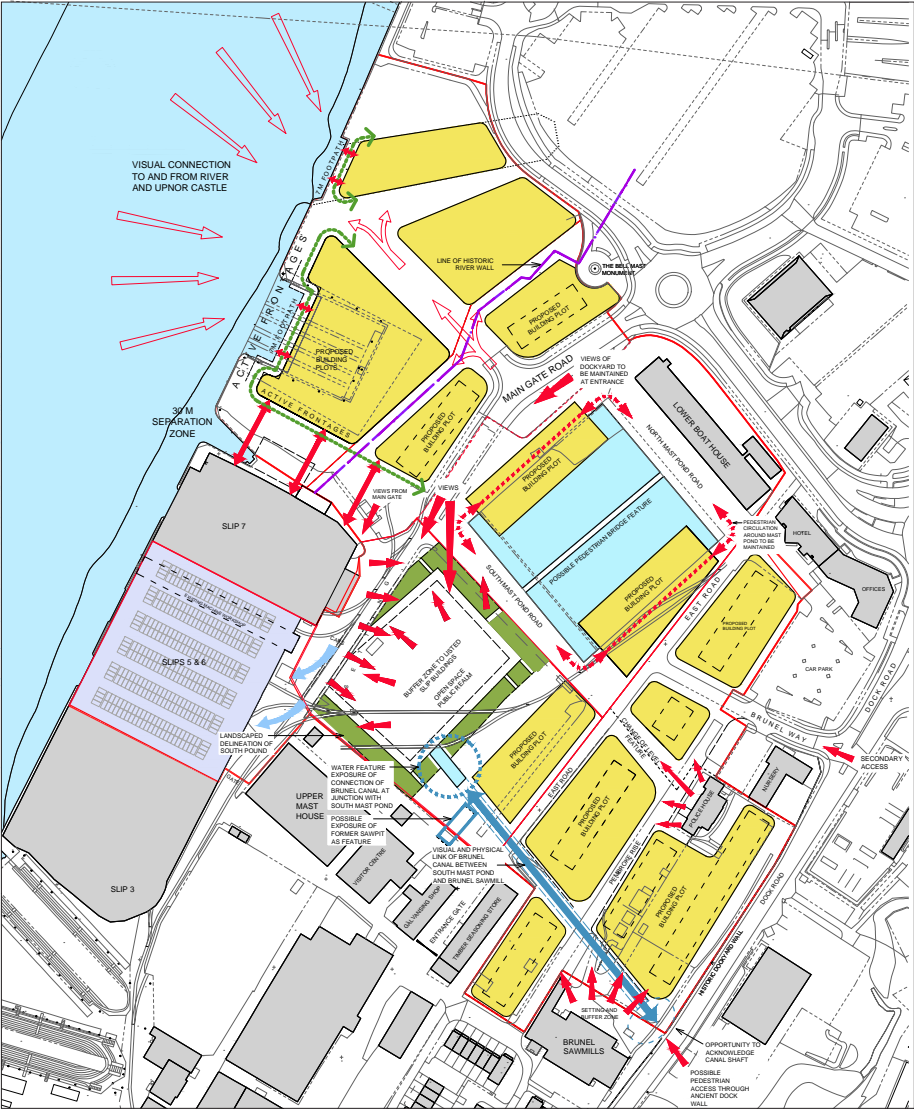
that such works are carried out to the necessary high standard.

3.11 As the Pembroke Rise zonal plan illustrates, the proposals for the Brunel canal fall within the Pembroke Rise character area as well as the southern Mast Pond, and should be designed to be delivered in a comprehensive manner

3.12 The connection of the existing Riverside Walk to the north into the Site is an important part of the reconnection of two parts of the former Dockyard needs to be guaranteed as part of the Implementation Plan. Whilst it is recognised this will be delivered as part of the Riverside area, should other zones come forward earlier, solutions towards this path should be sought.

3.13 The Riverside land is commonly regarded as being the most valuable in development terms, however the Riverside Development Zone will not achieve its full potential unless the public realm through which residents and visitors alike must enter the Riverside land has been brought to the necessary standard. The Mast Pond Drain runs through from the Northern Mast Pond to the river, and its continued function is vital for the Mast Pond. The Riverside Character Area should therefore be developed only after the Mast Ponds Character Areas have been developed.

3.14 In the interim, as detailed design proposals are developed, it is recommended that flexibility is applied to allow temporary uses within the site. In the past the site has been used for a number of income generating uses such as Park and Ride and parking for external events within Medway. These uses should be continued to ensure the landowners generate additional income which cross subsidise other costs including security.



Site wide development plots plan

Developer Contributions

- 3.15 Planning obligations are normally entered into in accordance with Section 106 of the Town and Country Planning Act 1990 and:
- restricting the development or use of land;
 - requiring specified operations or activities to be carried out in, on, under or over the land;
 - requiring the land to be used in a specified way; and
 - requiring a sum or sums to be paid to the local planning authority on a specified date or dates, or periodically.
- 3.16 Government policy is set out in Circular 05/2005. The Circular reiterates previous guidance that planning obligations should only be sought where they are:
- relevant to planning;
 - necessary to make the proposed development acceptable in planning terms;
 - directly related to the proposed development;
 - fairly and reasonably related in scale and kind to the proposed development;
 - and reasonable in all other respects.
- 3.17 Following the Community Infrastructure Levy Regulations 2010, planning obligations must be directly related to development. The Regulations state that any Section 106 is:
- necessary to make the development acceptable in planning terms;
 - directly related to the development; and
 - fairly and reasonably related in scale and kind.
- 3.18 Medway Council accept that the region is currently experiencing major growth and development through a number of regeneration projects. This has resulted in the Council publishing a Guide to Developer Contributions; a Supplementary Planning Document which provides comprehensive advice on how to determine contributions and identify infrastructure (social and physical) for which contributions may be sought.
- 3.19 Future development proposals for the Interface Land are expected to take account of the requirements of this document and any other public benefit brought about through the redevelopment of the site before submitting any planning applications to the Council.

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