

Development Brief: Interface Land, Chatham

Supplementary Planning Document

June 2018





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Foreword

This document describes an exciting regeneration opportunity for two sites within the former royal dockyard at Chatham, an area of the highest historic importance for its role in our national story. The sites lie where the core of the successfully regenerated historic dockyard undertaken by Chatham Historic Dockyard Trust (CHDT), abuts the modern Dockside retail and leisure area, and beyond that the Pembroke University Campus.

The river Medway lies west of the sites and directly adjoins one of these. The sites are well located for road, rail and public transport, footpaths, cycleways, and river access opportunities.

These important and prominent locations have the potential to create a new benchmark for future place-making and residential led mixed use development, providing a key part in the success of Medway as a Smart and Sustainable University Waterfront City for the 21st Century.



Both sites benefit from the outstanding quality of their history and the heritage setting provided by the dockyard and river. The recently completed and award winning development by CHDT has set a standard for and confirmed the level of aspiration required for the future use of the sites, including for the quality of new build elements. Situated inside of what was once part of the 18th century dockyard, the future of each site must respect and complement the wider site as now operated by the CHDT. The historic significance of the place requires a sensitive, considered design but this must not mean that ambitious or innovative responses to the heritage of the dockyard is discouraged; on the contrary we seek ambitious, appropriate and viable proposals that can help write a new chapter in the history of this place.

The legacy of innovation so prevalent at the dockyard was key to the safety and prosperity of our nation over some four hundred years and this has been maintained and built upon by CHDT in its vital work at Chatham. The quality of the conservation of the dockyard, skilfully combining repair of the old with new additions, should now inform approaches to further development here, by giving this context, relevance and a sense of purpose to be considered alongside the undoubted technical and design challenges posed by these sites.

There is a need for contextual solutions that fit with the outstanding heritage of the wider site and its setting. This is an exciting opportunity to design for tomorrow's heritage and to set the standard for future place-making in Medway.

To assist such ambitions, this document has been produced to describe the opportunities and challenges of these sites and by doing so to help de-risk the process of development by confirming relevant policy, design and technical expectations within a flexible framework for future negotiations. As such, it has built upon wide ranging knowledge compiled to date from various agencies and offers clarity on the range of issues crucial to investors and developers in pursuing a successful outcome.

Endorsed by Homes England, Historic England, Chatham Historic Dockyard Trust and adopted by Medway Council it has weight as a Supplementary Planning Document and can be relied upon as a significant material consideration in future planning decisions.



For centuries the Navy enjoyed an unparalleled location on the Medway with views over the river and across its hinterland. Through this exciting opportunity we now look forward to the last chapter for regeneration of land within what was the historic dockyard, offering unparalleled developed views to and from the Medway for centuries to come.



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1 Introduction



1.1 Purpose of the Document

A Development Brief for the 'Interface Land' sites has been produced by Medway Council, working with the landowner, Homes England (HE). The Development Brief has been informed by discussions with Council officers from Planning, Design, Conservation, Flood Risk, and Highways teams, and also statutory consultees Historic England and the Environment Agency. Discussions have also been held with the Chatham Historic Dockyard Trust (CHDT) and Chatham Maritime Trust.

The Development Brief is an adopted non-statutory planning guidance document and is a material planning consideration to be used to inform decisions in relation to proposals for development of the two sites.

1.2 The Development Brief Structure

Following this introductory chapter, the Brief is structured into the following chapters which outline key areas the applicant will need to consider and respond to:

- 2. Site potential and policy context**
- 3. Historic environment**
- 4. Access, connections and parking**
- 5. Flood risk**
- 6. Ecology**
- 7. Land quality, air quality and noise**
- 8. Stand offs and services**
- 9. Developable areas**
- 10. Other considerations**

Each chapter provides technical contextual information about the issues and opportunities relating to the relevant topic, establishes key principles and advises on required design response; outlines planning application requirements, and directs readers to relevant documents where available.

Given the unique nature of the sites and the bespoke design approaches which are required, the Brief has not sought to establish a design solution for the sites, rather it seeks to set the policy, design and environmental parameters which future proposals should address.

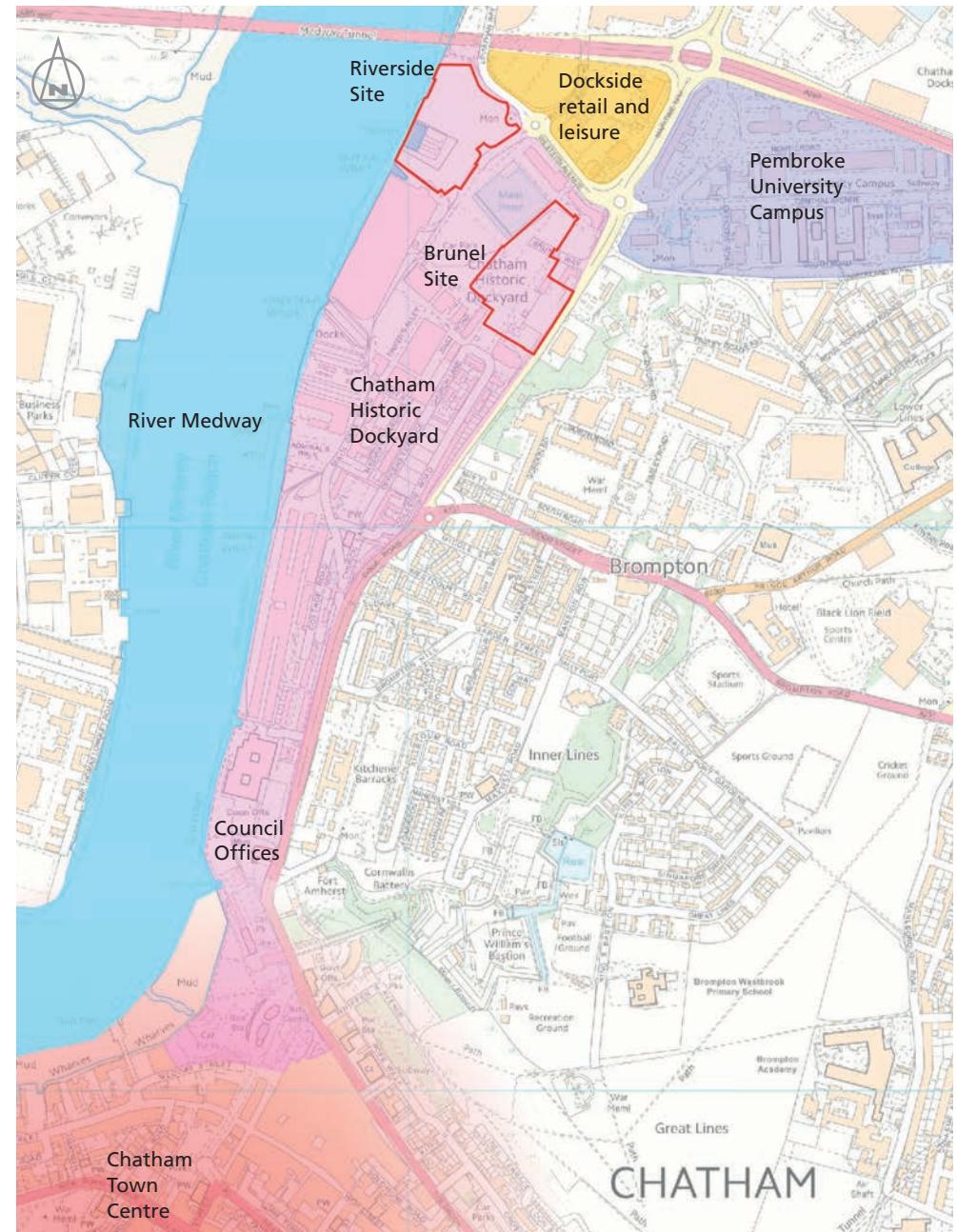
Whilst there is a focus on certain topic areas, development should adhere to the full range of policies set out in the Local Plan, supporting Supplementary Planning Documents (SPDs) and the National Planning Policy Framework (NPPF). Applicants should also refer to national and local validation requirements for submitting planning applications.

1.3 The Sites

The Interface Land Sites (the Sites) are currently in the ownership of Homes England who are looking to facilitate and support appropriate development in collaboration with key stakeholders. The Sites present an opportunity to bring forward exciting mixed-use, residential-led proposals that respond appropriately to the rich historic setting of this part of the former dockyard.

The history of the dockyard in Chatham dates back to 1547. The Interface Land sites are located within the area of the original dockyard, which played a vital role as a fleet base in the sail age navy. Much of this original dockyard area is now managed by Chatham Historic Dockyard Trust (CHDT) with the objectives to restore, preserve and interpret what is widely regarded as the world's best preserved dockyard of the Age of Sail.

Over the years the dockyard has developed with large scale expansion to the north of the original dockyard. This was driven by a focus on shipbuilding and maintenance and the development of large steam powered iron warships. Shipbuilding continued here during the 20th century, but with naval activity increasingly focused on the south coast ports of Plymouth and Portsmouth compounded by the intensification of economic pressures on the shipbuilding industry, the dockyard finally closed in 1984.



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Following closure the former dockyard was divided into three broad areas:

- The southern part, which comprised the majority of the original dockyard, was taken on by CHDT. This area has been the subject of heritage-led conservation and regeneration initiatives by CHDT, creating the popular Chatham Historic Dockyard visitor experience alongside the sensitive conversion of existing buildings to create new residential, commercial, exhibition and educational facilities.
- The eastern most part, around Basin 3, began operation as a commercial dock and is now operated by Peel Ports.
- The remainder was the focus of a major regeneration programme known as Chatham Maritime, which resulted in significant public sector investment in infrastructure and land remediation, enabling the creation of a new residential community at St Mary's Island alongside the delivery of modern business, retail and leisure facilities in the 'South of Basins' area. This area also includes what has become the 'Universities at Medway', a unique partnership which has brought together the University of Greenwich, the University of Kent, Canterbury Christ Church University and Mid-Kent College on a shared campus utilising many of the former naval buildings in the 'Pembroke' area.



The Interface Land Sites are situated at the convergence of the regenerated parts of the historic dockyard and the modern retail, leisure and residential development to the north and the University Campus to the east. This creates a unique opportunity to continue the work of the Chatham Maritime regeneration programme, combining history with modern architectural responses that respect the role these Sites played as part of the original dockyard, improve linkages and connectivity between the areas, and complement the high standards of design and conservation in the Chatham Historic Dockyard.

The Sites are separated by one of the former mast ponds that comprised part of the original dockyard and are surrounded by the following.

- The award winning Command of the Oceans visitor centre and the Chatham Historic Dockyard are located immediately to the south (with Medway Council offices and Chatham town centre further to the south).
- The Dockside retail and leisure area is located to the north of Leviathan Way.
- The shared campus of University of Greenwich, the University of Kent, Canterbury Christ Church University and Mid-Kent College is located to the north/east.
- The River Medway bounds one of the Sites to the west.

For the purposes of this Development Brief, the Interface Land sites are known individually as the 'Riverside Site' and the 'Brunel Site'.



Riverside Site

The Riverside site is 2.7ha and lies to the west of Main Gate Road (the main access to the Historic Dockyard) and south of Leviathan Way, overlooking the River Medway. The site lies within the Chatham Historic Dockyard Conservation Area. The site is cleared of any buildings but hardstanding and self-seeded grassed areas remain. A slipway on site provides access to the River. To the south lies the impressive Slip Buildings which are a scheduled monument, Grade 1 listed and an iconic part of the Historic Dockyard. The Riverside site is accessed off Main Gate Road via Western Avenue.



Brunel Site

The Brunel site is 2.7ha and lies within the Chatham Historic Dockyard Conservation Area. The site is located west of Dock Road and south of The Observatory office premises and Travelodge hotel. A nursery facility lies outwith but adjacent to the site in the east sharing the access to the site off Brunel Way. The Northern Mast Pond (Scheduled Monument) and large areas of car parking lie to the west. The Brunel Sawmill (Scheduled Monument and Grade 1 Listed) lies to the south with a range of mixed uses within the Historic Dockyard beyond. The western portion of the site is cleared of any buildings, the eastern portion contains the Police House which is a mid 19th Century, 3-storey Grade II Listed building and Scheduled Monument. Self-seeded trees and vegetation surround the Police House to the south and east. The Brunel site is accessed off Brunel Way via Dock Road. Rights of access also exist via Main Gate Road and along the southern edge of the northern mast pond into the site. The western part of the site fronting the Northern Mast Pond and the main Historic Dockyard visitors attraction car park is currently utilised for parking (0.45ha). A covenant exists restricting the future use of this part of the site to limited development such as landscaping or parking only.



2

Site Potential and Policy Context

2.1 Site Aspirations

Development of the Interface Land sites presents the following opportunities:

- Delivering mixed-use redevelopment of two significant public sector brownfield sites within a core regeneration area where the principle of development is secure subject to addressing planning requirements.
- Progressing appropriate proposals based on the highest quality design approaches that: maximise the potential of the sites, enhance the heritage of the sites and their setting, and ensure the perpetuation of neighbouring uses. In so doing, delivering an attractive environment for end users, enhancing the experience for neighbouring users, and fulfilling Medway's regeneration and placemaking aspirations.
- Integrating surrounding complementary land uses and good road, rail and sustainable transport connections by strengthening linkages to and through the site via a high quality public realm ensuring footpath, cycle, and river connectivity.

There is a strong commitment within the Council and within statutory agencies such as Historic England to work with applicants to ensure the opportunities outlined above come forward.

If designed sensitively and effectively in collaboration with key stakeholders, particularly the Chatham Historic Dockyard Trust (CHDT), the Interface Land sites can create a viable and vibrant development opportunity for a range of potential uses, ensuring best use of public sector brownfield land, and creating a considerable asset for Chatham's future whilst respecting and conserving the sites' historical past.

The regeneration of the Historic Dockyard owned and led by CHDT has received significant recognition and serves to illustrate how development on sites with important heritage assets can be delivered well whilst also fulfilling significant regeneration benefits for Medway. The sympathetic development of the recent Command of the Oceans project culminated in its shortlisting for the prestigious Stirling Prize for Architecture which recognised the way that 21st Century design and quality standards can be used in enhancing world class heritage assets to the benefit of placemaking. The redevelopment of the Interface Land sites will be able to capitalise from these significant achievements but will also be required to continue to set the standard for place-making in Medway through exemplar design and appropriate uses.

Riverside site context





Brunel site context



2.2 Land Use Potential

Whilst it is not for the Development Brief to define the exact mix of uses on the sites, their location within the original dockyard with its rich heritage, and adjacent to the Historic Dockyard's operations does mean that some uses are more appropriate than others.

Between the two sites lies the recently regenerated mast pond area, which provides an important opportunity to create the “linking hub” that re-connects the Historic Dockyard area as regenerated by CHDT and surrounding multiple users, including the universities and the Dockside retail area, and providing a complementary mix of uses potentially such as housing, commercial, further and higher educational uses, food and beverage, leisure and recreation, open space, riverside activities, and sustainable connections.

Further information on possible suitable uses:

- **Residential development** – anticipated to be the dominant land use on both sites.
 - Housing mix should be in line with Local Plan policy and latest Strategic Housing Market Assessment (SHMA).
 - Affordable housing in line with Local Plan requirements (currently 25% in the adopted and emerging Local Plan) and any future affordable requirements.
 - Potentially suitable for student and staff housing due to proximity to universities.
- **Supporting leisure and commercial uses in appropriate locations** – e.g. café/restaurant, and office uses particularly on the Riverside site, encouraging higher levels of activity around a riverside focus drawing in visitors and employees at the Historic Dockyard, university students, and users of nearby leisure, entertainment, and retail facilities.

- **Further and higher education (Class D1) uses** – given the proximity of the sites to Pembroke University Campus and education facilities within the Historic Dockyard and Chatham Maritime, the sites could support further and higher education uses.
- **Open Space and recreational uses**, particularly encouraging riverside activities.
- **Other uses** may be suitable and should be discussed with the planning authority as development comes forward.



Examples of contemporary style, higher density, riverside developments

2.3 Development Density

The sites lend themselves to potentially different styles of development.

- The **Riverside site** with its prominent river-side location and proximity to the modern Dockside area to the north, presents an opportunity to animate a current dormant waterfront edge **possibly via a more contemporary style of development, potentially of a higher density**, whilst also respecting historic viewlines, the adjacent Slip Buildings, and important below ground archaeological remains.
- The **Brunel site** extends the low density linear line of the eastern portion of the Historic Dockyard. This, along with the presence of the mid 19th Century Police House on site and the early 19th

Century Brunel Sawmill to the south (both Scheduled Monuments) means the site is **likely to be more suitable for a medium density, more traditional development style**.



Examples of medium density, more traditional development style

2.4 Design Integration

Whilst development of the Interface Land sites may progress as one proposal or separate proposals, applicants must fully acknowledge that historically the two sites were physically and functionally part of the former dockyard, and cannot therefore be developed without recognising that the integrity of the adjacent Historic Dockyard is the overriding determinant of any satisfactory development proposals.

The quality of design is paramount. There is a need for a contextual solution that fits with the outstanding heritage of the wider site and its setting.

The sites do, however, have their own individual histories as part of the former dockyard and present different opportunities. As such, they should have their own design characteristics whilst ensuring they integrate with and seek to further enhance the Historic Dockyard as a whole. Along with the on-site and surrounding heritage assets, the new built form of each site will need to respond to opportunities such as viewlines, access points, internal movement and site linkages, ensuring that these are not compromised by an inappropriate design response on one or both sites.



Applicants must discuss site uses, the design approach, architectural proposals, the mitigation of any harm to the historic environment and phasing with the Planning Authority, Historic England, Chatham Historic Dockyard Trust and other stakeholders through regular pre-application discussions as proposals come forward.

2.5 Planning Policy Position

The sites are located within the wider Chatham Maritime Regeneration Area as defined by the Medway Local Plan

(2003) and are therefore deemed suitable in principle for development. Relevant adopted policies relating to Chatham Maritime are shown below:

The emerging Local Plan will support and update the adopted policy with allocations for both sites and the requirement for a high quality, sensitively designed, residential-led mixed use development. The applicant should review all relevant national and local plan policies at the time proposals come forward.

Chatham Maritime Policy S8: Chatham Maritime Mixed Use Zone

Development will:

- Include a factory outlet centre (retail), Class B1 offices, a hotel, land and water-based leisure uses and housing. Tourist facilities and Class A3 uses of a scale commensurate with their location will also be appropriate.
- Create a new transportation framework for the sites, including improved public transport, cycling and pedestrian links to Chatham and Gillingham town centres with a key objective of reducing the need to travel by the private car.
- Promote high quality and innovative design approaches to create a high quality and vibrant environment.
- Promote development which is complementary to the Chatham Historic Dockyard in order to maximise visitor appeal and integrate the site with the wider environment.

Chatham Maritime Policy S9: Chatham Historic Dockyard

- ‘At the Historic Dockyard...development that respects the historic character of the site will be permitted. The standard of urban design must be of the highest order’.

Education Policy CF7: Further, Higher and Adult Education

- ‘Expansion of the existing campuses at Chatham Maritime and Chatham Historic Dockyard will be permitted’.



2.6 Medway 2035

The emerging Medway 2035 vision document will support the new Local Plan and sets out the special qualities of the area that provide a bedrock for regeneration. It identifies eight priorities as the focus for regeneration actions over the short, medium and long-term.

Priority 1 is Destination and Placemaking with the objective being to '*Put Medway on the map as a smart and sustainable waterfront investment city*'. Key sub-objectives of relevance to the redevelopment of the Interface Land sites are set out below. Applicants should review the full document.

The River

The River Medway is a high value nature conservation area and its estuary is a Special Protection Area under international law. It is the historic heart of Medway, providing prime opportunities for sensitive residential estate development and environmental enhancement. It offers multiple benefits for health and wellbeing, for the economy and for nature. Its waterfront has outstanding potential for increased recreational, leisure and commercial uses in appropriate locations.

Heritage and culture – world-class breadth and depth

Medway will embrace and promote the fascinating past which shaped its present and will influence its future. Our world-class heritage landscape is an outstanding setting for a modern city.

Medway will articulate a clear strategy that recognises the significance of our heritage, and its place in building the identity of our future city.

The redevelopment of the Interface Land sites will be required to ensure proposals reflect the Medway 2035 'Destination and Placemaking' priority along with wider priorities. Proposals will be required to be exemplars, meeting Medway's future design aspirations and setting the benchmark for wider development standards.



3 Historic Environment

3.1 Dockyard and Site History

The Interface Land sites were, since the early 18th century, part of Chatham Dockyard and critical to its operation, but have now been vacant for several decades.

Much has been documented and recorded about the Dockyard. The Chatham Historic Dockyard Trust's Conservation Management Plan 2017-2022 summarises the Dockyard's history as follows:

The Historic Dockyard at Chatham is a site of exceptional international maritime heritage significance.

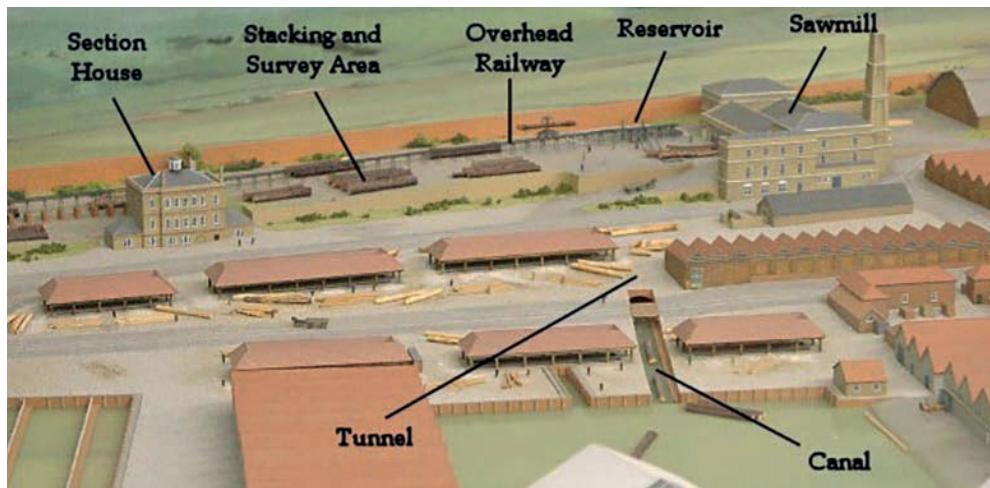
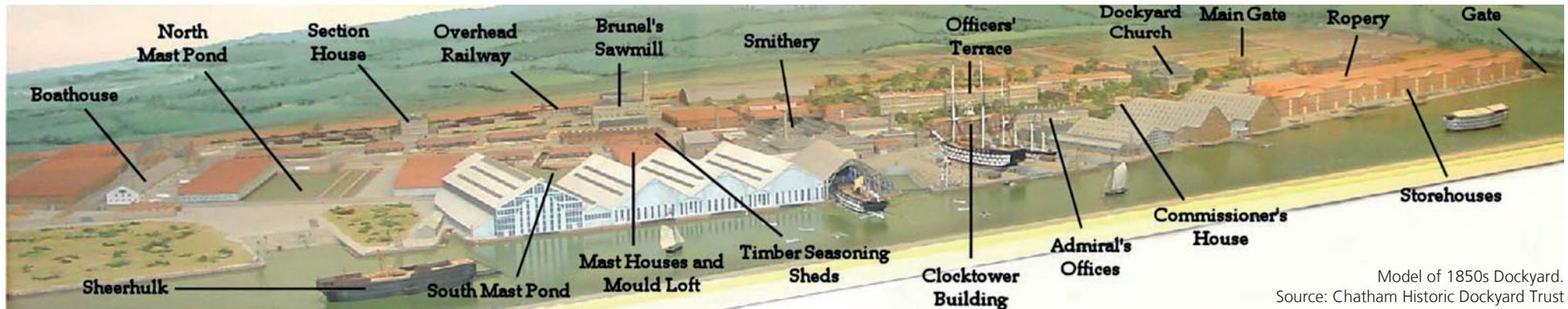
The area was in use as a Royal Dockyard from 1613 to 1984 and is arguably the most complete Dockyard of the Age of Sail to survive in the world. It also contains a number of important later 19th Century structures.

The Dockyard played a highly important and significant role in supporting the Royal Navy for over three hundred years building over 400 ships including HMS Nelson's flagship at Trafalgar and repairing and maintaining thousands more. In that time the Royal Navy achieved and maintained an unrivalled mastery of the seas – a critical factor in the development of Britain's global influence.



1804 Chatham Dockyard, Joseph Farington

Source: National Maritime Museum, Greenwich, London, Greenwich Hospital Collection



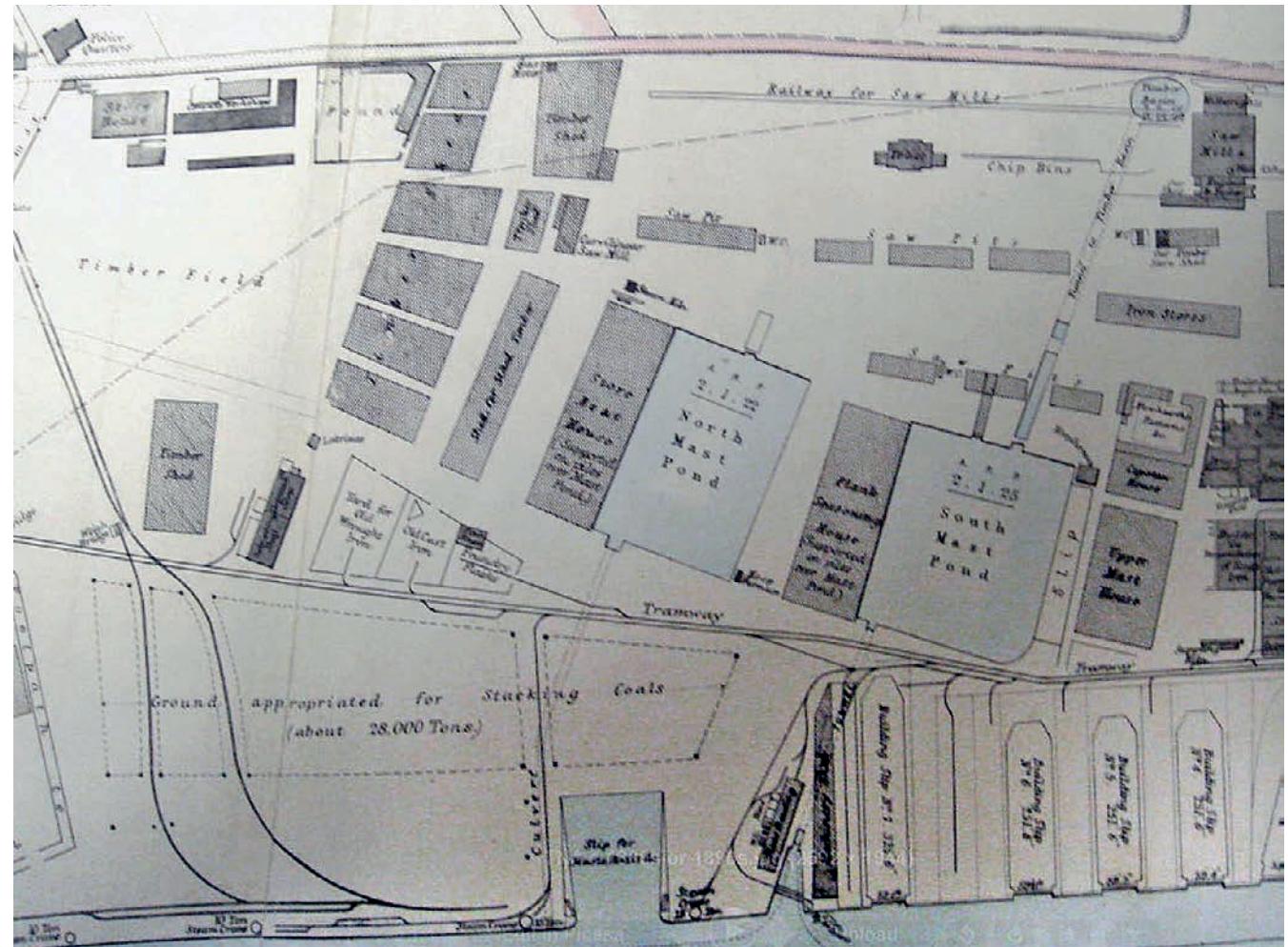
Model of 1850s Dockyard. This shows the mid 19thC layout of the Brunel site.
Source: Chatham Historic Dockyard Trust.

Encompassing an area of 80 acres, the Dockyard contains over 100 buildings and structures, 47 are scheduled as ancient monuments, with 54 listed as Historic Buildings, eleven at Grade I, and thirty two at Grade II*. The site is also designated as a Conservation Area.

Associated with the Historic Dockyard, but not part of it, are the Chatham Lines, an important range of 18th Century fortifications, including Brompton and Kitchener Barracks and Fort Amherst, and on the opposite bank of the Medway, the Elizabethan Castle and ordnance depot at Upnor.

The entire Dockyard is a site of national archaeological importance with potential for the existence of buried remains....Archaeological potential results mainly from 400 years of dockyard activity concentrated in one place. Documentary evidence ... shows the dockyard growing in size through time and once open areas being used for buildings and then sometimes being significantly re-developed. These phases of construction will be reflected by the presence of buried archaeological remains of earlier buildings and structures. In addition, minor structures such as saw pits, cranes, masts etc, not necessarily recorded on maps, can be expected to survive as buried remains. Evidence for the survival of below ground archaeology relating to earlier dockyard buildings and structures has been found across the site.

It is within this context that the Interface Land sites sit. Both sites are within the Historic Dockyard Conservation Area and either include or lie adjacent to designated heritage assets of the highest order (listed buildings and scheduled monuments), some buried archaeological remains are designated through scheduling but most are not. The archaeology is nevertheless of national importance. The sites relate to the historic operation of the dockyard as a whole and are associated with important historical view lines and functional relationships. Proposals for development will need to actively address and respond to these issues in a positive way.



1880s/90s Plan of Dockyard. Source: The National Archives
Riverside site - shows the reclaimed land from River Medway in active use.
Brunel site – clear layout of Police House, saw pits, canal, and overhead railway

Extract from 1879 OS Map

3.2 Designations and Policy

The plan shows the significant number of heritage designations close to the sites. A number of designations exist on the Interface Land sites:

- Both sites lie within the Chatham Historic Dockyard Conservation Area.
- The Brunel site contains Police House (sometimes referred to as the Police Section or Section House). The building is both a Scheduled Monument and Grade II listed. The Listing information refers to the building as being the Expense Account Department, Wages Division.
- The Brunel site lies adjacent to the Brunel Sawmill building and the site contains part of the canal tunnel which was used for floating timber from the South Mast Pond to the sawmill. The Sawmill and below ground canal tunnel are both Scheduled Monuments and Listed.

National and local policies relating to conservation areas, listed buildings, scheduled monuments and undesignated archaeological remains all apply.

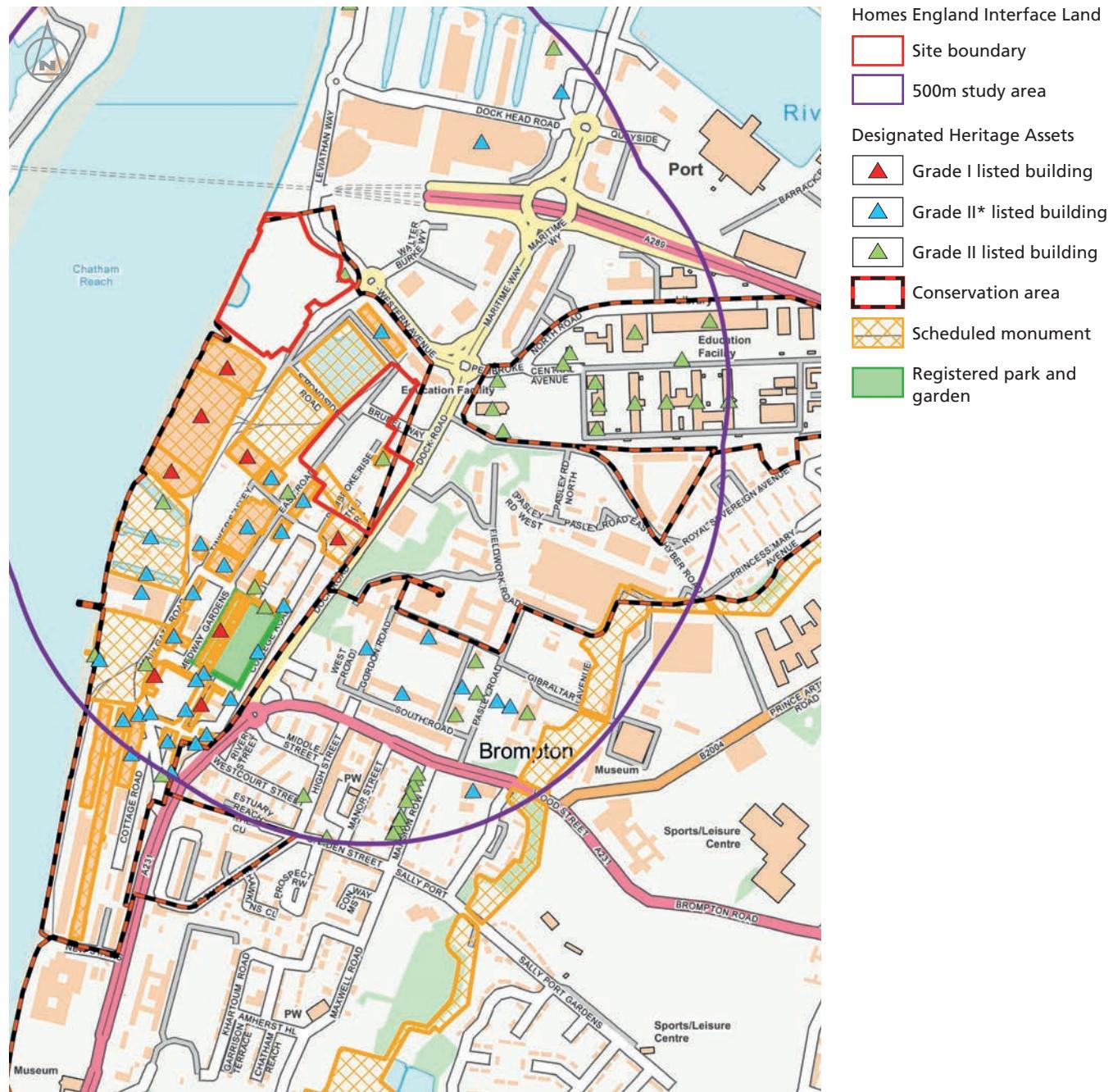


Table 3.1 Summary of Heritage Policies in Medway Local Plan (2003)

Policy	Summary
BNE12 – Conservation Areas	Special attention will be paid to the preservation and enhancement of the character and appearance of Conservation Areas...
BNE14 – Development in Conservation Areas	Development within Conservation Areas, or affecting their setting, should achieve a high quality of design which will preserve or enhance the area's historic or architectural character or appearance. Criteria relating to built form, layout, building materials and appearance, streetscape and hard and soft landscaping are set out in the full policy.
BNE16 – Demolition of Listed Buildings	The demolition of Listed Buildings will not be permitted unless it can be demonstrated that there are exceptional and overriding reasons for demolition, and that all possible methods of preserving the building have been investigated.
BNE17 – Alterations to Listed Buildings	Alterations, extensions, conversions or changes of use affecting a Listed Building will not be permitted if they are detrimental to the architectural or historic character of the building; or unsympathetic in design, scale, appearance and use; or do not retain original features and materials.
BNE18 – Setting of Listed Buildings	Development which would adversely affect the setting of a listed building will not be permitted.
BNE 20 – Scheduled Ancient Monuments	Development affecting Scheduled Ancient Monuments or other nationally important sites will not be permitted if it would damage or destroy such sites, or be detrimental to their setting.
BNE 21 – Archaeological Sites	Development affecting potentially important archaeological sites will not be permitted, unless: <ul style="list-style-type: none"> • The developer, after consultation with the archaeological officer, has arranged for an archaeological field evaluation to be carried out by an approved archaeological body before any decision on the planning application is made; • It would not lead to the damage or destruction of important archaeological remains. There will be a preference for the preservation of important archaeological remains in situ; • Where development would be damaging to archaeological remains, sufficient time and resources are made available for an appropriate archaeological investigation undertaken by an approved archaeological body. Such investigations should be in advance of development and in accordance with a specification and programme of work approved by the council. Resources should also be made available for the publication of the results of the investigation.

The applicant should ensure proposals meet these policy requirements, and any future requirements set out in the emerging Local Plan.

It is important to note that whilst the specific site-based designations shown on the plan on page 19 are highly relevant they do not cover the full historic significance of the sites, National Planning Policy Framework para 139 states:

Non-designated heritage assets of archaeological interest that are demonstrably of equivalent significance to scheduled monuments, should be considered subject to the policies for designated heritage assets.

Local Plan Policy BNE20 further supports this, referring to 'Scheduled Ancient Monuments or other nationally important sites'

Given the national importance of the former dockyard as a whole, the Interface Land sites are deemed historically nationally significant and therefore applicants should assume undesignated archaeological heritage assets on both sites hold an equivalence to Scheduled status and should approach development accordingly.



3.3 Overarching Historic Environment Principles

Based on an appreciation of existing built form and layout, established viewlines, and anticipated below ground assets, the following overarching historic environment principles apply:

- A design response appropriate to the scale and character of the historic environment, being mindful that the sites are located on the interface of the Historic Dockyard and wider Chatham Maritime regeneration area.
- New design should respect and reflect the historical layout of the sites wherever possible, so as to enhance or better reveal the significance of the site.
- An appropriate set back from built heritage assets is required.
- A considered approach to view lines seeking to preserve and or to enhance important view lines wherever possible via the appropriate placement of building plots and through building heights. Proposals should align with Historic England's advised 5-step approach to considering settings and views as set out in their Good Practice Advice Note 3 (GPA3).
- Preservation in situ of archaeological assets with development areas and building plots identified to preserve buried archaeology. Where such assets cannot be avoided, safeguarding measures, such as restrictions on depths of permitted disturbance and or appropriate foundation and services designs may be required. Excavation of archaeology should be a last resort only to be considered if it can be demonstrated that harm has been minimised and that which remains is unavoidable and clearly and convincingly outweighed by the public benefits of permitting the harm to be incurred.
- Preservation in situ of archaeological remains will require maintenance of water logged/anaerobic conditions to ensure below ground timber assets do not deteriorate as a result of development. Any proposals which change the hydrology of the site or the burial environment of archaeological remains, for example by an impact upon groundwater levels, will need to address the potential impact on the long term survival of buried archaeological remains, including timber structures.
- Use of an appropriate palette of materials and finishings which reflect, complement, and enhance the surrounding context.
- Conversion of the Police House should be seen as a positive opportunity. Surrounding development should consider an appropriate response to the setting of the Police House.



3.4 Riverside Site

① Original 18th Century River Wall

Development implications:

- Wall extent and tie backs to be identified and remains preserved in-situ.
- Route to be reflected in design layout with appropriate interpretation.

② Important Dockyard Assets

Development implications:

- Seek to preserve significant archaeological remains via preservation in-situ where possible.
- Foundation design solution required.
- Preservation of water-logged conditions to avoid timber deterioration.

③ Appropriate setback from slip buildings

④ Slipway

Development implications:

- To be retained and actively integrated into proposals.
- Active riverside openspace giving access to River. To be managed and maintained safely and effectively.

⑤ Mast Pond Drain

Development implications:

- Historical and functional mast pond drain to be protected and preserved.
- No build on top of the drain. Appropriate set back required.

⑥ Preservation of historic viewlines

See item 6, table 3.2



Setting of adjacent heritage assets

Proposals must respect the setting of:

- A** Slip Buildings (scheduled)
- B** Bell Mast (Listed)
- C** Northern Mast Pond (Scheduled)
- D** Lower Boat Store (Scheduled)

Table 3.2 Interpretation of Key Assets and Required Design Response

Ref	Asset	Interpretation and Design Response
1	Original 18th Century Dockyard wall	<p>In the mid-19th Century the Chatham Dockyard expanded west reclaiming land from the River Medway securing additional land to facilitate increased operations. The anticipated route of the original wall is shown on the plan.</p> <p>The wall extent and timber tie backs are to be identified and remains preserved in situ. Preservation of water-logged conditions will be required to preserve timber features.</p> <p>The route of the original river wall is to be reflected in the design layout for the site with appropriate interpretation, potentially via a footpath or linear landscape treatment or other means.</p>
2	Important dockyard assets - Lower Mast House, No.8 Slip, and Upper Boat House	<p>The largest concentration of archaeological assets are anticipated to be located east of the original river wall (often referred to as the 'dry side') and this area is likely to be the focus for mitigation of archaeological impacts.</p> <p>The Lower Mast House, No.8 Slip, and Upper Boat House were previously located as shown on the plan.</p> <p>Archaeological remains should be preserved in-situ where possible. Safeguarding measures, such as appropriate foundation design, will be required where assets cannot be avoided. Excavation should be a last resort and only accepted if demonstrated to be unavoidable and outweighed by other benefits.</p> <p>Preservation of water logged/anaerobic conditions will be required to ensure below ground archaeological deposits, such as timber structures, do not deteriorate.</p>
3	Covered Slip Buildings	<p>The Covered Slip Buildings are located to the south of the Riverside site beyond the site boundary. The Slip Buildings are Scheduled Monuments and Grade I Listed and are an iconic and highly visual aspect of the Historic Dockyard.</p> <p>The volume, scale and massing of the historic Slip buildings should be acknowledged and new development must respect their industrial heritage and setting in the context of the River Medway.</p> <p>An appropriate set back area is to be proposed and agreed with Historic England and Medway Council as design proposals come forward.</p>
4	Slipway	<p>The slipway was constructed in the mid-19th Century as part of the Dockyard expansion. It is not designated in its own right but it is a heritage asset and a contributor to the character and appearance of the conservation area.</p> <p>The slipway should be retained and actively integrated into the development proposals for the site.</p> <p>The slipway creates an opportunity for retained riverside access and active open space, creating an interesting focal point within the development whilst respecting the site's historical context. Proposals will need to be managed and maintained safely and effectively.</p> <p>Sufficient space should be provided around the slipway to enable effective use.</p>
5	Mast Pond Drain	<p>Drain routing from Northern Mast Pond to River Medway through the site.</p> <p>The drain is both historical and functional and will be required to be preserved and protected. No build over the drain. An appropriate easement will be required.</p>
6	Historic View lines Views of the Covered Slip Buildings from the River Medway Views to/from Upnor Castle Views to Brompton Barracks and Chatham Lines from the River Medway Views from within the core of The Historic Dockyard	<p>From the western bank of the River Medway the Covered Slips are visible landmarks that stand out against a relatively low tree ridgeline. In addition to the setback referenced at 3 above, proposals should not harm the current dominance of the Covered Slips, especially in views from across the River Medway, particularly from the village of Upnor and Upnor Castle.</p> <p>In addition, views through and past the Interface Land sites to the green backdrop of Brompton Lines beyond should be respected ensuring an element of continued visual connection with the Dockyard's defences from the River Medway.</p> <p>The mass and scale of the buildings should allow views from the west to the ridgeline to the east. A single development mass will not be acceptable. Multiple buildings of varying heights and at an appropriate orientation will enable punctuated views and ensure building mass does not compete with the Covered Slips or overly limit views of the green backdrop of trees which have grown along the Brompton Lines. The impact on views from The Historic Dockyard to the south should also be carefully considered.</p> <p>It may be appropriate for building heights to 'step up' from south to north increasing in height towards the north-western edge of the site fronting the River. Buildings over 6-storeys in height will trigger the Council's Building Heights policy as set out in the Building Heights SPD (2006). Information on viewlines is set out in the Chatham Dockyard and Its Defences SPD (2014).</p>

3.5 Brunel Site

1 Police House

Development implications:

- Restoration and conversion to a number of potential uses.
- Preservation of setting and views to:
 - a. Former Timber Stacking Yard
 - b. Brunel Sawmill
 - c. Western site.

2 Brunel Sawmill and canal to north

Development implications:

- No build zone
- Canal route and shaft to be interpreted
- Visual connection between Sawmill and southern mast pond to be retained

3 Saw pits

Development implications:

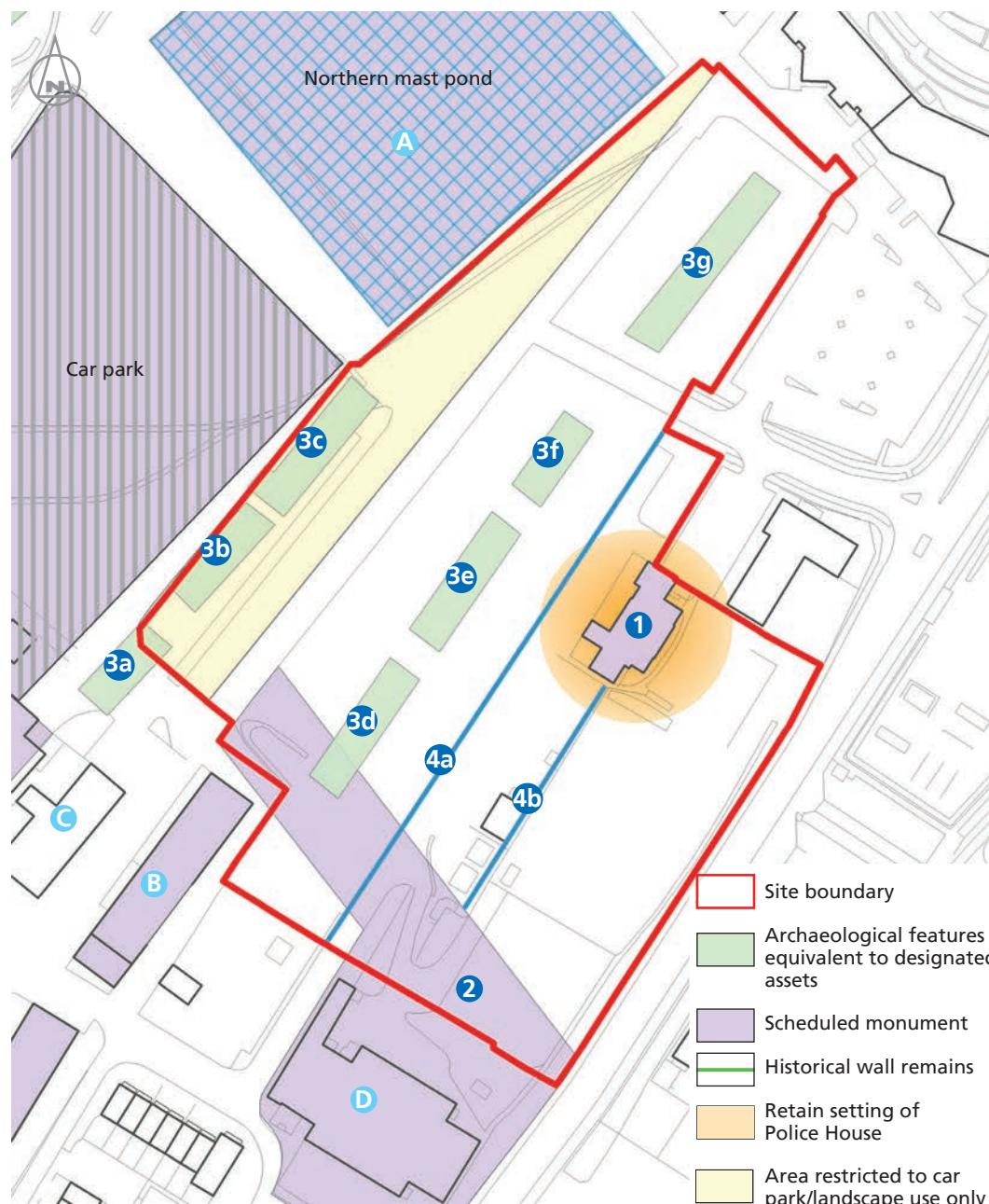
- 3a - c within area limited to car park use by restrictive covenant. No implications.
- 3d - g Foundation design to preserve pits in situ where possible. Potentially interpret within plot layout.

4 Wall remains

- 4a - 18th century dockyard wall.
- 4b - Timber stacking yard wall.

Development implications:

- Timber stacking yard wall to be preserved and integrated into design layout.



Setting of adjacent heritage assets

Proposals should seek to respect the setting of:

- A** Northern Mast Pond (Scheduled)
- B** Timber Seasoning Sheds (Scheduled)
- C** Galvanising Shop (Listed)
- D** Brunel Sawmill (Scheduled)



Table 3.3 Interpretation of Key Assets and Required Design Response

Ref	Asset	Interpretation and Design Response
1	Police Section House	<p>Police House is designated as a Scheduled Monument and Grade II Listed. The building should be sensitively conserved and converted in consultation with Historic England and Medway Council. Police House is in a good condition and its conversion to a viable use(s) should be seen as a positive opportunity and a key part of development proposals for the Brunel site. Initial design testing has indicated that the building can be converted in a number of ways, for example, vertical town house use, horizontal apartment use, and commercial office or business hub use. Other uses may well be feasible.</p> <p>Conversion design should seek to consider the following:</p> <ul style="list-style-type: none"> • Retention of external façade and external features • Preservation of internal plan following the existing three cell divisions • Retention of the staircase within the south cell • If doorways are to be blocked the blocking should be recessed to ensure the original plan can be read • Retention of chimney breasts • The former doorways at ground floor level to the front offer potential access through the primary façade of the building. Their reopening and reuse should be considered. • Use of galleries, pods and internal structures to ensure large scale spaces at first and second floor can continue to be read as such. <p>Whilst an appropriate set back area / setting will be required, Police House has never historically had a formal setting within the Dockyard.</p> <p>The self-seeded trees and vegetation which surrounds Police House in the south and east have no historical significance and could be removed subject to ecological assessment.</p> <p>The small observatory or 'cupola' on the roof of Police House indicates that visual connections across the Dockyard were important. Particularly views across what would have been the timber stacking yard to the south-east as shown on the plan, views of the Brunel Sawmill and the canal, and potentially views to the Riverside site across the mast pond. Preservation of visual connection should be considered as part of the future design layout of the site.</p>
2	Brunel Sawmill, Canal and Stacking Yard	<p>The Brunel Sawmill building lies outwith the site to the south. The site contains part of the canal tunnel which was used for floating timber from the Southern Mast Pond (now the Historic Dockyard's main car park) to the Sawmill. The Sawmill and the canal route as shown on the plan are Scheduled Monuments and Grade I Listed.</p> <p>There should be no buildings developed within the Scheduled Monument designation. Encroachment of development too close to the Sawmill itself needs to be avoided. The canal route and shaft should be interpreted, potentially via use of paving or landscape arrangements. There should be a visual connection between the Sawmill and the Southern Mast Pond.</p> <p>The former timber stacking yard as shown on the plan had a functional relationship to the Sawmill. Proposals should make reference to this relationship, for example by a north to south axis in the design to reflect the past movement of timber by an overhead railway or potentially via the use of timber references through use of materials in architecture, street furniture, landscape edges etc.</p> <p>The footprint of the stacking yard should remain intelligible as a zone between its western edge brick retaining wall and the main dockyard wall to the east.</p> <p>Opportunity to present a pedestrian connection / access via Dock Yard Wall, re-opening one of the now bricked up doorways which provided a previous access point. This could also provide a strong visual connection along the canal route to the mast pond with the Slip buildings beyond.</p>
3	Saw pits	<p>A total of at least 7 saw pits historically existed on the site and were used to store timber.</p> <p>The pits shown as 3a-3c on the plan are located under an established car park. The area shown pale yellow on the plan is the subject of a restrictive covenant limiting built development. Pits 3a-3c are therefore fully preserved in situ and present a linked set of pits.</p> <p>Pits 3d-3g should be preserved in situ where possible. The pits could potentially be reflected in building plot layout. Appropriate foundation design is required where assets cannot be avoided with excavation and interpretation as a last resort.</p>
4	Wall remains	<p>The 18th Century Dockyard wall is shown as 4a on the plan. Its current existence as buried archaeological remains is unproven. Should such remains be identified they should be preserved in situ. The alignment of the early 18th century dockyard wall is reflected in the line of the road and this should be retained in future designs. The early 19th century dockyard wall with its tower houses is scheduled where it exists above ground and any buried parts of it must be assigned a high level of importance.</p> <p>The former Stacking Yard wall shown as 4b is of greater significance and should be preserved, enhanced and integrated into the design layout for the site. The wall could form the western edge of a distinct development zone in this part of the site.</p>

3.6 Application Requirements

Historic England has advised that, as the sites are deemed to be nationally significant, a detailed planning application will be required to be submitted. Scheduled Monument Consent will be required for any works relating to Police House and the Brunel Sawmill and canal route.

It is anticipated that, in addition to investigations undertaken previously, further archaeological investigation is likely to be required on some part of the sites to explore the archaeological implications of emerging development proposals. These are likely to be required prior to application submission. Targeted archaeological investigations as a potential means of mitigating otherwise unavoidable harm may be required prior to construction, dependent on the consented proposals. Building recording and archaeological survey may also be required during the implementation of consented works to specific agreed parts of the sites.

Joint pre-application discussions will be required with Historic England, Kent County Council Heritage Conservation and Medway Council's Conservation Team. Discussions must also be held with Chatham Historic Dockyard Trust.

Relevant documents anticipated to be required to support a planning application include:

- A statement of significance covering both buried archaeological remains and above ground heritage assets.
- A heritage impact assessment to demonstrate how the proposals will affect the significance assessed above and how any harm has been avoided, minimised or, if unavoidable, mitigated.

- Site layout plan reflecting design response to archaeological and heritage assets.
- Detailed elevational drawings and cross-sections.
- Finishings and materials.
- Landscape treatment.
- Visual Impact Assessment with accurate visual representations.

3.7 Available Information

The applicant is expected to draw on the significant amount of available information already collated to date regarding the above and below ground heritage assets for the sites. The applicant should supplement this information with further analysis and investigation as advised by Historic England and Medway Council as proposals come forward. Archaeological investigations prior to planning application submission are likely to be required. Targeted excavations, monitoring and recording will also be required prior to and during construction, dependent on consented proposals and appropriate mitigation arrangements.



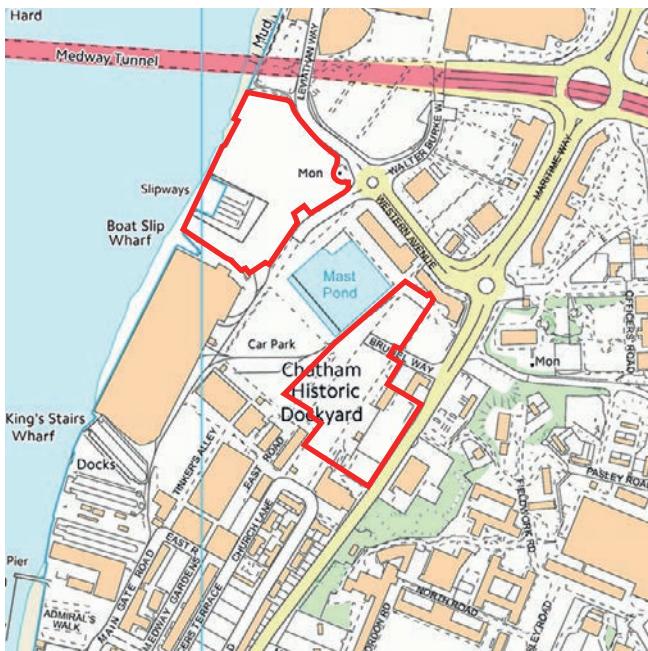
3.8 Relevant Documents

- National Planning Policy Framework: especially Section 12.
- Scheduled Monuments and nationally important but non-scheduled monuments (DCMS 2013).
- Local Plan policies - Adopted policies BNE12, BNE13, BNE14, BNE16, BNE17, BNE18, BNE20 and BNE21.
- Building Heights SPD 2006.
- Chatham Dockyard and Its Defences SPD 2014.
- Background archaeological reports.
- Historic Environment Good Practice Advice in Planning Note 2.
- Historic Environment Good Practice Advice in Planning Note 3.
- Preserving Archaeological Remains: Decision-taking for Sites under Development (Historic England, 2016).
- Piling and Archaeology (Historic England 2015).
- Land Contamination and Archaeology (Historic England 2017).
- Waterlogged Wood Guidelines on the recording, sampling, conservation and curation of waterlogged wood (Historic England, 2010).
- Conservation Management Plan 2017-2022 (Chatham Historic Dockyard Trust 2017).

4 Access, Connections and Parking

4.1 Introduction

The Interface Land sites are located within a sustainable location with strong connections. Development proposals will need to consider access arrangements, sustainable transport connections and linkages to/ through the site, parking, and road and junction capacity. This section sets out requirements necessary to support development proposals.



4.2 Overarching Transport Principles

The following overarching transport principles for both sites have been established:

- Ensuring a functional and safe vehicular access for each site.
- Ensuring pedestrian and cycle links through/ to the sites are well thought through with connections to surrounding facilities, points of interest, and public transport.
- Facilitating access to the River Medway.
- Provision of an appropriate level of car parking to serve the developments whilst seeking to encourage and optimise use of the strong sustainable transport connections available.
- Consideration of capacity of the road network and potential need for mitigation measures to improve capacity.

4.3 Policy Context

Adopted Local Plan transport policies relevant to the Interface Land sites are set out on page 26. The applicant should ensure proposals meet policy requirements, and any future requirements set out in the emerging Local Plan. The emerging Local Plan intends to ensure sustainable travel is central to the design of new developments.



Table 4.1 Summary of Transport Policies in Medway Local Plan (2003)

Policy	Summary
T1 – Impact of Development	In assessing the highways impact of development, proposals will be permitted provided that: <ul style="list-style-type: none"> • the highway network has adequate capacity to cater for the traffic which will be generated by the development, taking into account alternative modes to the private car; • the development will not significantly add to the risk of road traffic accidents.
T2 – Access to the Highway	Proposals which involve the formation of a new access, or an intensification in the use of an existing access, will only be permitted where: <ul style="list-style-type: none"> • the access is not detrimental to the safety of vehicle occupants, cyclists and pedestrians; • or can, alternatively, be improved to a standard acceptable to the council as Highway Authority.
T3 – Provision for Pedestrians	Medway Council will develop a network of safe and convenient footpaths, to link houses, schools, town centres, work places, recreation areas and public transport routes. Development proposals shall provide attractive and safe pedestrian access. In all cases, they should maintain or improve pedestrian routes related to the site.
T4 – Cycle Facilities	Secure cycle parking and associated facilities will be sought in accordance with the council's adopted cycle parking standards.
T6 – Provision for Public Transport	Where of sufficient scale, new developments will be expected to make provision for access by public transport
T9 – River Buses and Piers	Proposals for improvements to existing piers and for the development of new piers on the Medway to provide river bus services will be permitted that are consistent with nature conservation, ecological, hydrological and landscape policies and other river uses.
T11 – Development Funded Transport Improvements	Legal agreements with developers will be sought to secure: <ul style="list-style-type: none"> • off-site improvements to transport infrastructure (which may include rail freight, public transport, pedestrian and cycling infrastructure) • and/or public transport services, the need for which arises from the proposed development; • improved accessibility by all modes of transport
T13 – Vehicle Parking	Development proposals will be expected to make vehicle parking provision in accordance with the adopted standard.
T14 – Travel Plans	Travel Plans will be required for all developments which require a transport assessment

4.4 Access

Riverside Site

Access to the Riverside Site is via Main Gate Road onto Western Avenue and then Dock Road/Maritime Way via a roundabout.

Brunel Site

Access to the Brunel Site is likely to be via Brunel Way onto Dock Road at a ghost island right turn junction. This junction is currently restricted to left out only movements (northbound), with southbound movements achieved via a U-turn at the Dock Road/Maritime Way roundabout. Access to Brunel Way is achievable in from both directions off Dock Road.

The site also benefits from a secondary access via South Pondside Road (southern length of the Northern Mast Pond) onto Main Gate Road and onto Western Avenue past the access to the Riverside Site. Land to maintain this access is secured and controlled by the landowner. The Brunel Way access is anticipated to be the primary point of access subject to development proposals and access discussions with Medway Council.

Access design proposals should be discussed in advance and signed off by Medway Council Highways Team and should be the subject of a Road Safety Audit. Proposals must also be discussed with Chatham Historic Dockyard Trust.

Figure 4.1 Access Points

4.5 Connections

Development proposals should connect into and, where possible, improve existing links connecting the site to

- Medway town centre and beyond.
- Connecting to bus and rail routes.
- Connecting to the universities, Dockside Area, Historic Dockyard – creating a draw for surrounding users and higher levels of activity.
- Connecting to the River Medway.

Pedestrian, cycle, bus and rail

A number of existing pedestrian routes and footways surround both sites. Medway Town Centre, local schools and shops can all be reached within a 20 minute walk distance from the proposed development site.

Cycle route provision also surrounds both sites through a mix of on and off-road designated cycle lanes. National Cycle Route (NCR) 1 routes along Dock Road which borders the Brunel Site. Connections to NCR 1 and other numerous routes on the local network provide good connections to local amenities and Chatham town centre as well as neighbouring towns. The area with an approximate cycle journey time of 20 minutes is extensive and covers the whole of Chatham and Gillingham providing access to rail facilities. The approximate journey time taken to travel between Gillingham/Chatham and London is between 40 minutes to 1 hour 8 minutes.

All required local facilities lie within acceptable cycling and walking distances of the site.

A number of bus stops, comprising high quality bus shelters are situated with 400m of both sites, namely on Dock Road, Riverside Avenue and Central Avenue.

Access to 11 frequent bus routes can be achieved within 400m.

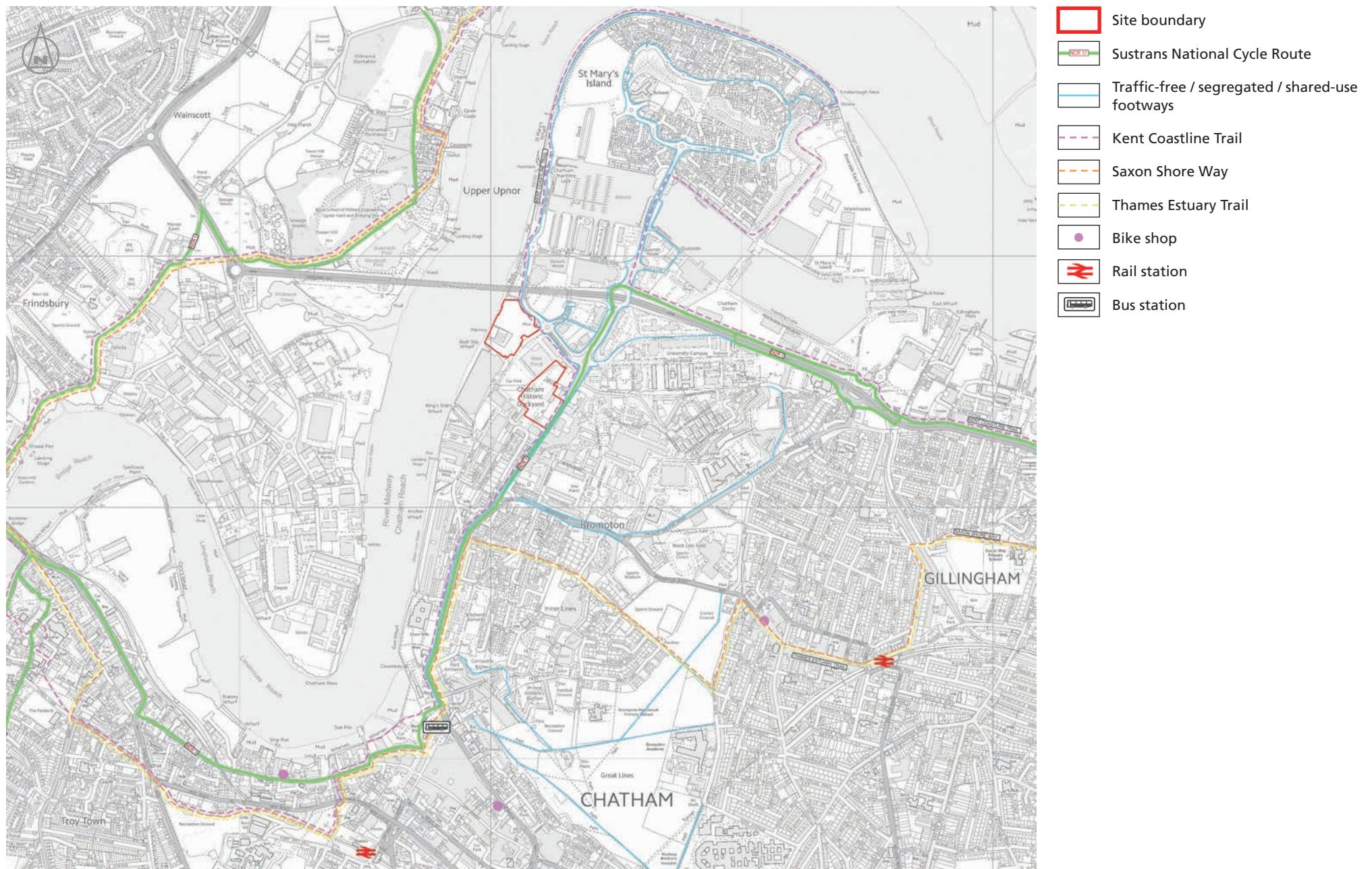
The sites are strategically well connected. Proposals should seek to optimise the sustainable connections available and maximise sustainable development principles where possible demonstrating how they will reduce reliance on the private car, enhance public transport usage and improve walking and cycling facilities.

A Travel Plan will be required to be submitted which should identify how the development will maximise the usage of sustainable modes of transport as opposed to the private motor vehicle.

Applicants should embrace Sport England's Active Design concept which promotes healthy communities through good urban design including maximising pedestrian and cycle connections.

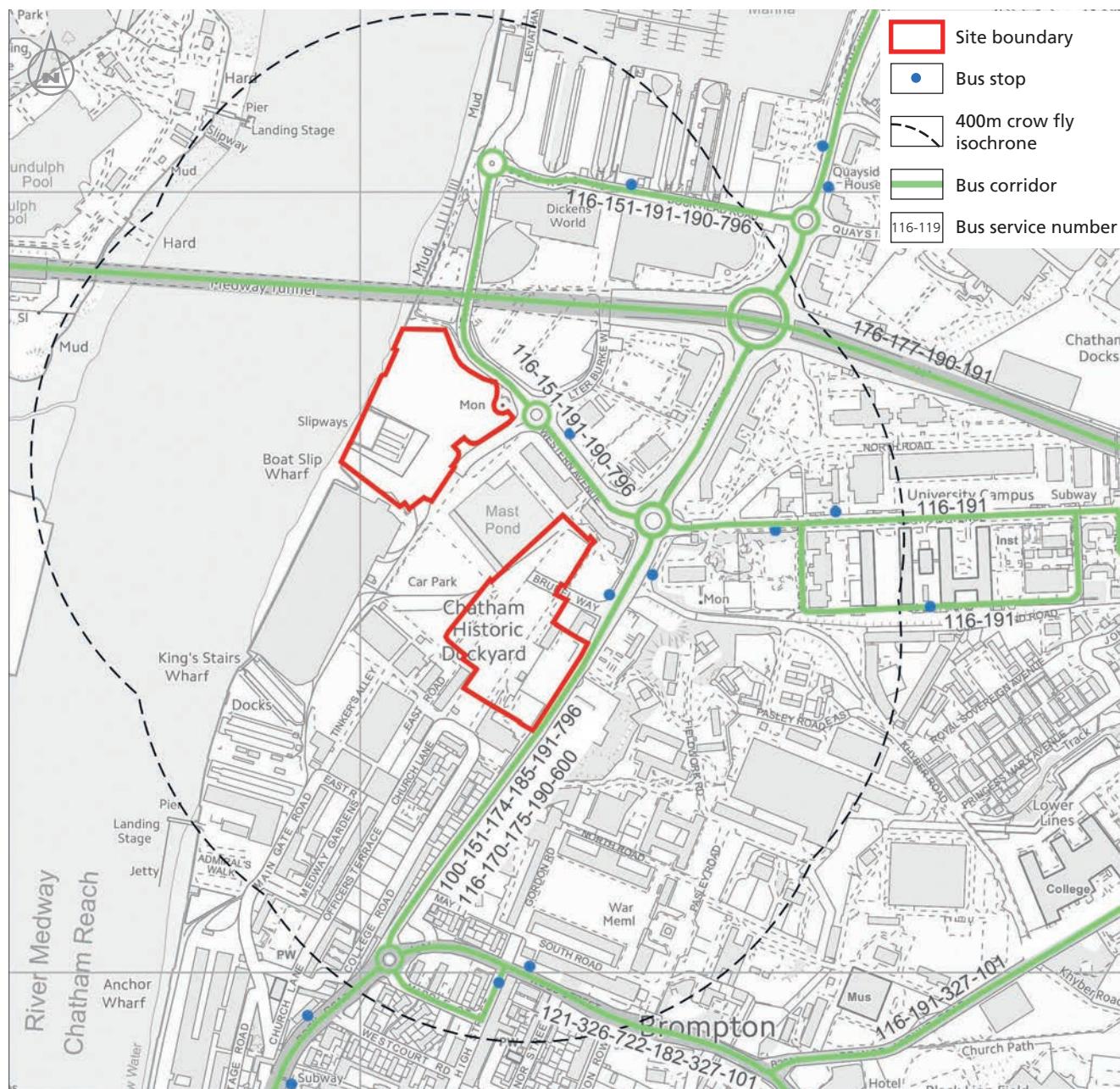


Figure 4.2 Existing Pedestrian and Cycle Network



Based upon the Ordnance Survey Map with the permission of the Controller of Her Majesty's Stationery Office. © Crown Copyright. 100001776

Figure 4.3 Existing Bus Stops and Routes within 400m



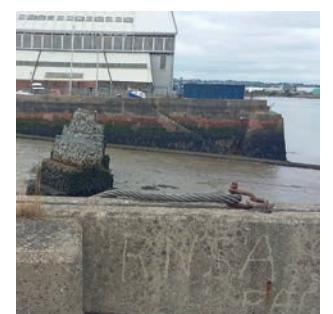
River Access

The slipway on the western side both historically and currently provides access to the River Medway. Proposals should consider opportunities to maintain and enhance this access point with appropriate, safe and effective long term management and maintenance arrangements put in place.

An ambition is ongoing within Medway to optimise access and use of its river. This vital artery which has indeed been so central to the development of Medway from Roman times still acts as a commercial and leisure spine for which adequate public access is vital. This spine exists as a linear network of piers, slips and docks which Medway is looking forward to further energising over the coming years.

The slip on the Riverside Site of the Interface Land is one such access slip offering considerable character and development opportunity. This slip gives access to the river as one of the few remaining public slips, additionally offering links and access to the developing linear marina outside Gun Wharf. Furthermore it offers a perceptual and public realm link between the river and the mast pond of the Historic Dockyard, not least since it was the link historically and benefits from close proximity.

Applicants are encouraged to consider the existing dock and river access as a premium asset of the site that can offer more opportunities than constraints for any development proposed. Indeed its contribution to distinctive character and place making for the western site is considered to be of worthy significance.



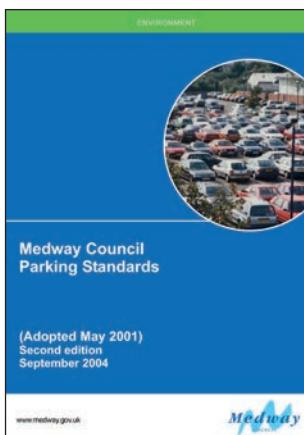
4.6 Parking

Development proposals should seek to effectively integrate parking provision into the scheme layout, providing sufficient spaces for residents, visitors and commercial users, ensuring no overspill parking into neighbouring areas.

Parking provision should be in line with adopted parking standards which are currently set out in Medway Council's Second Edition "Parking Standards" (2004) and Interim Residential Parking Standards" (2010).

Currently required minimum parking standards are set out in the tables opposite.

The Council may consider reducing parking requirements in this location. It is for the applicant to prove the sites are in a sustainable location and to provide strong footpath and cycle connections to and through the Interface Land sites to sustainable transport connections and to negotiate any reductions in parking provision. Dependent on the nature of proposals which come forward, the Council are willing to consider parking reduction in the context of effective green travel provision.



Parking provision should be discussed with the Planning Authority and the Highways Authority at pre-application stage.

Table 4.1 Minimum Residential Parking Standards

C3 - Residential	Car Spaces per dwelling	Cycle Spaces per dwelling*
1 bedroom	1.0	1.0
2 bedroom	1.5	1.0
3+ bedroom	2.0	1.0
Visitor	0.25 spaces per dwelling (unallocated) (rounded up to nearest whole number)	0

*Cycle parking is not required if garage or secure area is provided within curtilage of dwelling.

Table 4.2 Minimum Commercial Parking Standards

Land use	Max. number of car parking spaces	Min. number of commercial vehicle Space	Min. number of cycle spaces
A1 – Retail	1 per 18m ² GFA	1 per 500m ² GFA	1 per 250m ² GFA
A3 – Public Houses / Bars	1 per 4m ² GFA	Assessed individually	1 per 250m ² GFA
A3 – Restaurants / Cafes	1 per 4m ² GFA (+ 1 per member of staff)	Assessed individually	1 per 250m ² GFA
B1 – Business	1 per 30m ² GFA	Assessed individually	1 per 400m ² GFA



4.7 Network Capacity

Development proposals should meet the access needs they generate and should not cause any severe impacts in terms of increased traffic congestion or highway safety.

A Transport Assessment will be required to assess the baseline existing position and the impact of the development on the surrounding and wider road network.

The Highways Authority have advised that capacity assessments should be undertaken at the following junctions:

- Maritime Way/A289 roundabout;
- A289/Anthony's Way roundabout;
- Gillingham Gate Gyratory; and
- Dock Road/A231 roundabout.

The applicant should agree the final extent and scope of junction assessment work with the Highways Authority at the time proposals come forward.

Discussions with the Highways Authority will be required to establish what mitigation measures are to be provided and secured by the development to improve the functionality and capacity of the local highway network. The ultimate form of any junction improvements should be worked up with the Highways Authority as part of a detailed planning application.

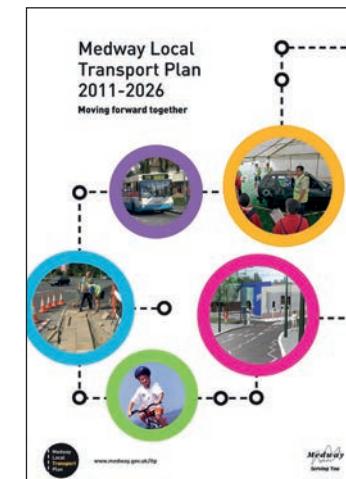
4.8 Application Requirements

Pre-application meetings should be held with the Highways Authority and Planning Authority to determine the scope and nature of transport-related requirements to support a future planning application(s). Anticipated requirements are as follows:

- Access design proposals for both sites supported by Road Safety Audits.
- Transport Assessment - to assess the impact of the development on the surrounding and wider road network including public transport routes and the cycling and walking network.
- Travel Plan - to identify how the development will maximise the usage of sustainable modes of transport as opposed to the private motor vehicle.
- Parking Plan.

4.9 Relevant Documents

- National Planning Policy Framework Section 4: Promoting Sustainable Transport.
- Medway Local Transport Plan (LTP3) (2011-2026).
- Medway Local Plan (2003) Transport Policies – Section 8.
- Medway Council's Second Edition "Parking Standards" (2004) and Interim Residential Parking Standards" (2010).



5 Flood Risk

5.1 Introduction

The Interface Land sites are located on the east bank of the Medway estuary and are within an identified flood risk area, with the Riverside site being of greater risk of flooding. Development proposals will need to respond effectively to flood risk requirements.

5.2 Overarching Flood Risk Principles

- Proposals will recognise and address the flood risk issues and ensure adequate mitigation measures are implemented in discussion with the Environment Agency and Medway Council's flood risk officers.
- Development should seek to facilitate measures to reduce flood risk through a range of potential measures such as improvements to defences and/or site levels, appropriate ground floor land uses, appropriate finished floor levels, and safe emergency planning.
- In addition to tidal flood risk mitigation, consideration of surface run off and use of SuDS should be integrated into proposals where appropriate.

5.3 Policy Context

Medway Local Plan (2003) Policy CF13 Tidal Flood Risk Areas states:

Development will not be permitted within a tidal flood risk area if:-

- It harms the integrity of the flood defences; or
- It fails to provide for a means of escape for people in the event of a flood; or
- It introduces residential living and sleeping accommodation below the estimated flood level.

Section 10 of the National Planning Policy Framework establishes that:

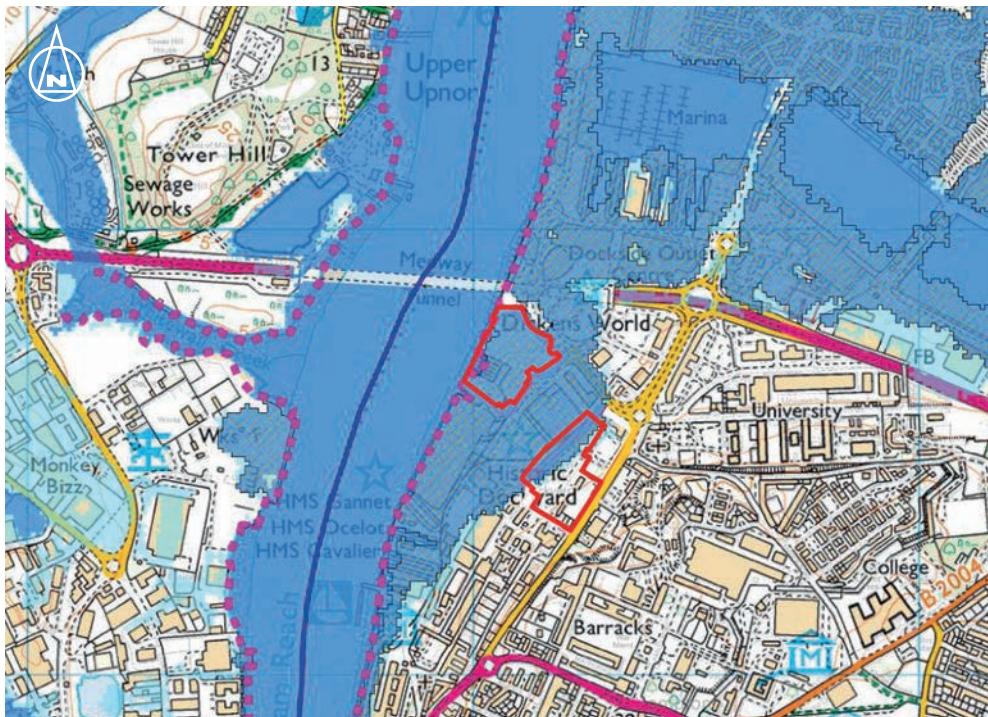
When determining planning applications, local planning authorities should ensure flood risk is not increased elsewhere and only consider development appropriate in areas at risk of flooding where, informed by a site-specific flood risk assessment following the Sequential Test, and if required the Exception Test, it can be demonstrated that:

- Within the site, the most vulnerable development is located in areas of lowest flood risk unless there are overriding reasons to prefer a different location; and
- Development is appropriately flood resilient and resistant, including safe access and escape routes where required, and that any residual risk can be safely managed, including by emergency planning; and it gives priority to the use of sustainable drainage systems.

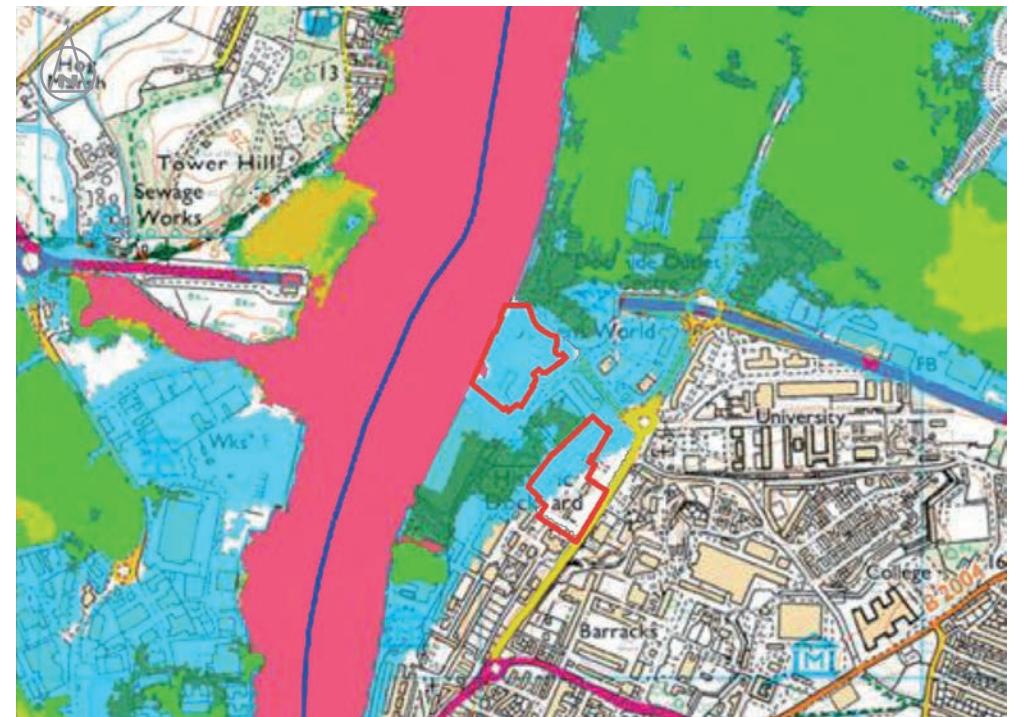
5.4 Flood Risk Context

Both sites are at risk from tidal flooding from the Medway estuary. The Environment Agency flood map shown below indicates that the Riverside site falls entirely within Flood Zone 3 as defined in the National Planning Policy Framework (high probability of flooding). The Brunel site intersects Flood Zones 2 and 3 along its western boundary, but the majority of the site lies outside of the extent of flood risk (within Flood Zone 1 – Low probability area).

The Environment Agency flood map indicates the presence of flood defences along this section of the Medway estuary. The zone of flood risk affecting the sites is therefore shown as "benefitting from flood defences". The river wall which runs along the eastern bank of the Medway through Chatham, and forms the western edge of the Riverside site, provides the flood defences referred to.

Figure 5.1 Environment Agency Flood Map – Extracted Nov 2016

- Site boundary
- Main river
- Flood defences
- Areas benefiting from flood defences
- Flood zone 3
- Flood zone 2

Figure 5.2 Flood Levels for the 0.5% Annual Exceedance Probability (AEP) Flood Event and 2115 Climate Change Scenario

- Site boundary
- Main river
- 5% AEP (2012)
- 1.3% AEP (2012)
- 0.5% AEP (2012)
- 0.1% AEP (2012)
- 0.5% AEP (2070)
- 0.5% AEP (2115)

Historical hydraulic modelling undertaken for the site has indicated that the areas shown as Flood Zone 3 on the Environment Agency's flood map would remain dry for present day flood events as it is protected by flood defences, however the defences would be overtopped when climate change (sea level rise) was considered.

The current policy for flood defences in the vicinity of the site, as set out in the Shoreline Management Plan (2008 and 2010) is to "hold the line". This policy position means that it is the intention of the Environment Agency that the existing line of defence and the level of protection provided should be maintained. The Shoreline Management Plan recognises that the **flood defences from which the site benefits will need to be maintained and improved to protect their integrity, and that crest levels will need to be raised in order to maintain the level of protection in the face of sea level rise.**

A request to the Environment Agency was made in 2016 for Product 4 data. This provides information on the flood levels reached for different flooding events and timeframes. The data included in Product 4 is (currently) the most up to date and therefore should be used for the assessment of flood risks in the area.

In discussions with the Environment Agency it has been recommended that any flood risk assessment for the Interface Land sites should consider flood levels for the 0.5% Annual Exceedance Probability (AEP) flood event and 2115 climate change scenario as shown on the plan below. (Flood levels were extracted from the modelling undertaken in the North Kent Coast Modelling and Mapping Study, completed by JBA Consulting, in August 2015).

5.5 Existing Flood Defences and Site Levels

Defence levels are not constant along the Riverside site boundary. Topographical analysis indicates that the crest levels of the defence line range generally between 5.50 – 5.65m AOD, however there is a significant difference in levels, located at the north-western part of the site where the defence levels increase in height from 5.65m AOD to 6.11m AOD and then remain constant at around 6.12m AOD. Beyond the site boundary the defence line extends over a section of 105m to the north along the River Medway towards St Mary's Island with similar crest levels.

In addition, the ground levels of the site adjacent to the defence line vary between 4.20 – 4.70m AOD. There is therefore a significant drop between existing ground levels and existing defence levels and it is highlighted that this difference will increase if the defences are raised.

The River Wall is owned by Chatham Maritime Trust. The Trust have advised that any permanent structures within 15m of the wall should be avoided. Any works within 15m of the wall will require the Trusts' approval and sign off. A 15m easement is required along the landside of the River Wall for the benefit of the Trust along with vehicular access through the site. The Trust may consider a reduced width of easement along the slipway walls. The easement is to enable the Trust to access the wall for inspections, repairs and rebuilding. Landscaping and surface parking maybe allowed in the easement area subject to full access being available for the Trust from time to time, to carry out its works.

5.6 Mitigation Works and Requirements

In discussion with the Environment Agency and Medway Council, applicants are required to consider effective mitigation works for the Riverside site which may include the following:

- An increase of the defence levels in order to protect the site against tidal flooding. EA recommendations indicate that the defence levels should increase to match flood levels for the 0.5% AEP and a 2115 climate change scenario (approximately 6.0mAOD) plus 300mm of freeboard or to reach the maximum levels along the northern stretch of the site.
- Land raising of the site could minimise the residual risk from tidal flooding and mitigate the big difference in levels. This would have the added benefit of improving the aesthetic value of the site given its prominent riverside location and enhanced public realm opportunities. Any proposal for land raising will need to fully take into the account the load pressure on the Riverside Wall and will require engagement with the Chatham Maritime Trust. With the site's important historical context in mind, any land raising proposals will also need to be approved by Historic England and Medway Council's Conservation Officer. Proposals should also be discussed with Chatham Historic Dockyard Trust to ensure the setting of surrounding Monuments and views from The Historic Dockyard are not impacted.

5.7 Design Response and Requirements

In discussion with the Environment Agency and Medway Council, applicants are required to consider an effective design layout for both sites with special attention to mitigation measures such as finished floor levels and access and egress for residential and less vulnerable uses as follows:



Riverside Site

- Finished floor levels (FFL) for more vulnerable uses (i.e. residential) that lie within the 0.5% AEP and 2115 climate change scenario should be set at the flood levels for that scenario plus 600mm of freeboard. Ground floor levels below that criteria would be acceptable for less vulnerable uses. Therefore, ground floor levels on the Riverside site will be unsuitable for residential uses. Surface parking and/or commercial uses are likely to be acceptable.
- Safe access and egress for more vulnerable uses will be required. Good practice would be to provide safe access onto Western Avenue and Dock Road allowing emergency services into the site. Safe refuge for residents above the flood levels at upper floors may be considered acceptable particularly given the flood risk is tidal (i.e. levels will reduce as the tide goes out). Safe access and egress solutions should be discussed with Medway Council.
- Surface water drainage proposals should consider use of the Sustainable Urban Drainage Systems (SuDS) to be established (where feasible) as part of the overall design layout for the site to enable effective drainage of surface water and will require connections to surface water drains. Drainage arrangements should be discussed with Historic England to ensure preservation of below ground assets.

Brunel Site

- Finished floor levels (FFL) for more vulnerable uses (i.e. residential) that lie within 0.5% AEP and 2115 climate change scenario event should be set at the flood level for that scenario plus 600mm of freeboard. Ground floor levels below that criteria would be acceptable for less vulnerable uses. It should be noted that the majority of the site that falls within this area is the subject of a restrictive covenant limiting uses to car parking, which is an appropriate use. The small area outwith the parking area and within the flood zone may need land raising.
- For the parts of the site that lie outside the 0.5% AEP and 2115 climate change scenario event no recommendations on the finished floor levels would be required provided the proposals do not lower existing levels.
- Safe access and egress for more vulnerable uses will be required. Given that the eastern part of the site, including the main access to the site, lies outside the flood zone, it is not envisaged that providing safe access and egress will be an issue.
- Surface water drainage proposals should consider use of the Sustainable Urban Drainage Systems (SuDS) (where feasible) as part of the overall design layout for the site to enable effective drainage of surface water and will require connections to surface water drains. Drainage arrangements should be discussed with Historic England to ensure preservation of below ground assets.

5.8 Application Requirements

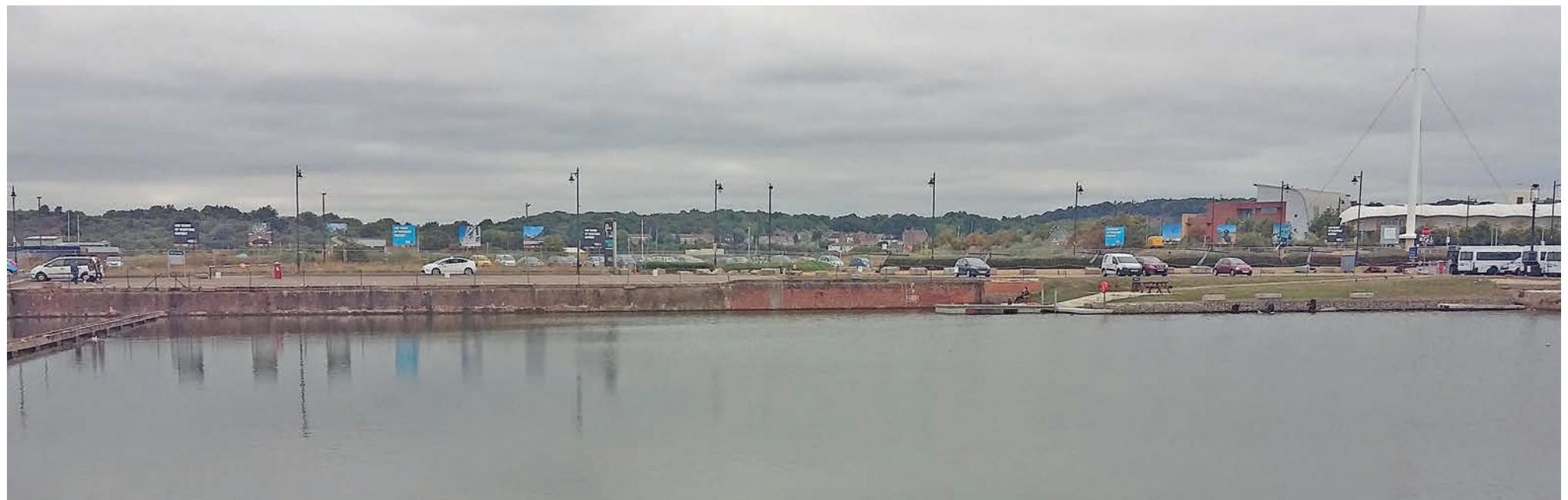
Pre-application discussions should be held with the Environment Agency and Medway Council Flood Risk Officer to agree the scope and nature of any assessment work, and proposed measures to mitigate against flood risk. Approval of any works to the wall should be agreed with Chatham Maritime Trust.

Applicants are also advised to seek the view of Historic England and Chatham Historic Dockyard Trust in relation to any impacts on historical assets.

A Flood risk and drainage assessment will be required (for both sites). To address Exception Test requirements, a site-specific flood risk assessment must demonstrate that the development will be safe for its lifetime taking account of the vulnerability of its users, without increasing flood risk elsewhere, and, where possible, will reduce flood risk overall.

5.9 Relevant Documents

- National Planning Policy Framework: Section 10 – Meeting the challenge of climate change, flooding and coastal change.
- Medway Local Plan (2003) – Policy CF13.
- Product 4 data and background flood risk reports.
- North Kent Coast Modelling and Mapping Study.
- Medway Estuary and Swale Shoreline Management Plan.



6 Ecology

6.1 Introduction

Important ecological designations exist alongside the Interface Land sites within the River Medway. Appropriate ecological survey work will be required to support a planning application.

6.2 Overarching Principles

- Due regard and consideration will be required to ensure the development does not impact significantly on adjacent ecological sites of interest
- Protected species may exist on site. Appropriate ecological survey work will be required prior to application submission with suitable mitigation response.
- Proposals should seek to encourage habitat creation through a considered approach to scheme design.



6.3 Ecological Context and Requirements

Both Sites

- The Medway Estuary and Marshes Marine Conservation Zone (MCZ), Ramsar, SSSI and SPA lie immediately to the west of the Riverside site within the River Medway and its banks
- Natural England require all new residential units within 6km of the SPA to mitigate against recreational disturbance with a developer contribution of £223.58 per unit (at time of writing). Both the Riverside and Brunel site will therefore be required to contribute per residential unit.
- Appropriate survey work will be required prior to application submission.



Riverside Site

- The Riverside site contains a large area of improved grassland and bare ground, and areas of hardstanding including the slipway. There are no statutory ecological designations within the site boundary.
- In 2016 there were no records of bat activity or reptiles, amphibians or invertebrates on or close to the western site. There were however records of foraging, nesting, overwintering birds. It is for the applicant to undertake appropriate ecology surveys and determine ecological presence or absence on site.
- On-site ecology is unlikely to restrict development potential.
- Habitats are unlikely to support rare species due to managed nature and proximity to urbanised areas.
- Impacts of adjacent Medway Estuary and Marshes will need to be considered during construction stage.

Brunel Site

- Police House building and dominant habitats of semi-improved grassland, scattered trees, scrub and broadleaved woodland exist on site. There are no statutory ecological designations within the site boundary.
- In 2016 there were no records of bat activity or reptiles, amphibians or invertebrates on or close to the Brunel site. It is for the applicant to undertake appropriate ecology surveys and determine ecological presence or absence on site.
- Habitats are unlikely to support rare species due to managed nature and proximity to urbanised areas. On-site ecology unlikely to restrict development potential subject to bat survey findings.
- Bat potential close to Police House and trees.
- Trees are the subject of Tree Protection Orders due to the site's location within a Conservation Area. Historic England has confirmed the trees are not historically significant and have self-seeded over several decades. Removal of any trees will require approval from the Planning Authority and appropriate ecological and aboricultural survey work.

6.4 Application Requirements

- Phase 1 Extended Habitat Survey and subsequent requirement for ecological surveys.
- Aboricultural assessment.
- Proposals should respond to the results of any survey work and show an appropriate design response to support and enhance on-site ecology.



7

Land Quality, Air Quality and Noise

7.1 Land Quality

Both sites were historically part of the Dockyard operations. The Landowner has recently undertaken investigations to assess ground conditions which has confirmed the presence of contamination and land stability issues on both sites.

Overarching Principles

- Ensuring applicants undertake appropriate ground investigation works and respond sufficiently to remediation, foundation, and construction design advice ensuring a safe environment without risk to human health during the construction process and for end users.

Applicants will be expected to submit a detailed Ground Investigation and establish an appropriate development solution to mitigate against contamination and any land stability concerns. Details of site investigations and a remediation strategy will be required. Ground investigations in land known to or suspected of containing buried archaeological remains will require actions to avoid or minimise harm to these. Mitigation of unavoidable harm through archaeological recording may be necessary.

Local Plan Policy BNE23 Contaminated Land applies:

Development on land known or likely to be contaminated or affected by adjacent or related contamination must be accompanied by the findings of a detailed site examination to identify contaminants and the risks that these might present to human health and the wider environment. Appropriate measures to reduce, or eliminate, risk to building structures, services and occupiers of the site and of adjoining sites must be agreed. Such remedial measures must be satisfactorily implemented before the development is occupied.

Consultation with Medway Council's Environmental Health Officer (EHO) is advised.

Application Requirements

- Phase1 Geo-Environmental Assessment and Phase 2 Ground Investigation Report.
- Development response which accords with the findings of the Ground Investigation.

7.2 Air Quality and Noise

The sites lie close to the Central Medway Air Quality Management Area on Dock Road. Applicants should consider the proposals' impacts on the Air Quality Management Area and should discuss the need for, and scope of, an Air Quality Assessment at early pre-application stage.

The Riverside site lies adjacent to the Slip Buildings which have been in heavy industrial shipyard and boatyard use for centuries and remain so today. Equally the Brunel site lies adjacent to the Brunel Sawmill which also has an ongoing industrial use. The uses within the Slip Buildings and the Sawmill are safeguarded and the commercial viability of businesses should not be negatively impacted as a result of future residential use and potential noise complaints. Proposals must seek to address noise through careful planning, layout and design mitigation measures to ensure the noise impact for future users is made acceptable. Applicants should discuss the need for, and scope of, a Noise Assessment at early pre-application stage.

8 Stand Offs and Services

8.1 Introduction

This section summarises a number of stand offs or development set back requirements (some of which have been previously identified in the preceding chapters). Information and advice is also provided in relation to drainage and utilities.

8.2 Overarching Principles

- Ensuring development respects and responds to existing stand offs/ easements and restrictions.
- Ensuring development can be served by the necessary infrastructure with any capacity issues mitigated in advance.

8.3 Stand Offs, Restrictions and Service Routes

The following information establishes required setbacks and restrictions:

Riverside Site

- 50m no build zone from Medway Tunnel in the north west portion of the site. This amounts to approximately 20m on the site.
- 15m easement along river wall for access and maintenance.
- Appropriate set back required from slip buildings to be agreed in discussion with Historic England and Medway Council.
- Northern mast pond drains to the River. No build on top of the drain. Appropriate easements.

Brunel Site

- No build on top of Brunel Sawmill and Canal route Scheduled Monument designation.
- Setting of Police House and Brunel Sawmill to be respected.
- Car park strip fronting Northern Mast Pond is subject to a restrictive covenant restricting car park or landscape use only.
- Foul water sewer traverses the northern portion of the site and may need diverting or an easement may be required should diversion not be possible.

8.4 Utilities and Drainage

A utilities assessment and drainage assessment should be prepared and submitted as part of the planning application and will need to prove that the development can connect into and be served by the relevant utilities providers. This requirement includes electricity, gas, fibre optic broadband, potable water, foul drainage, and surface water drainage.

8.5 Application Requirements

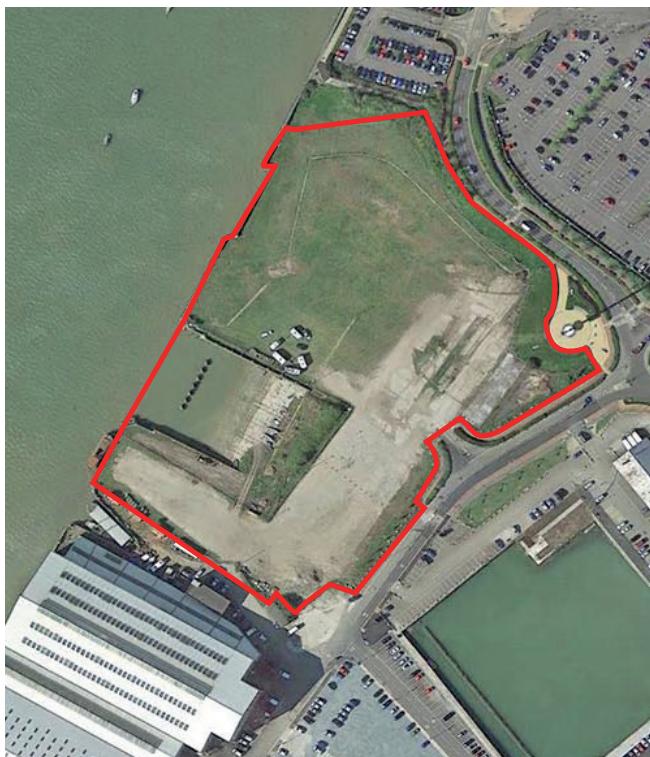
- Utilities and drainage assessment.

9 Developable Areas

Riverside Site

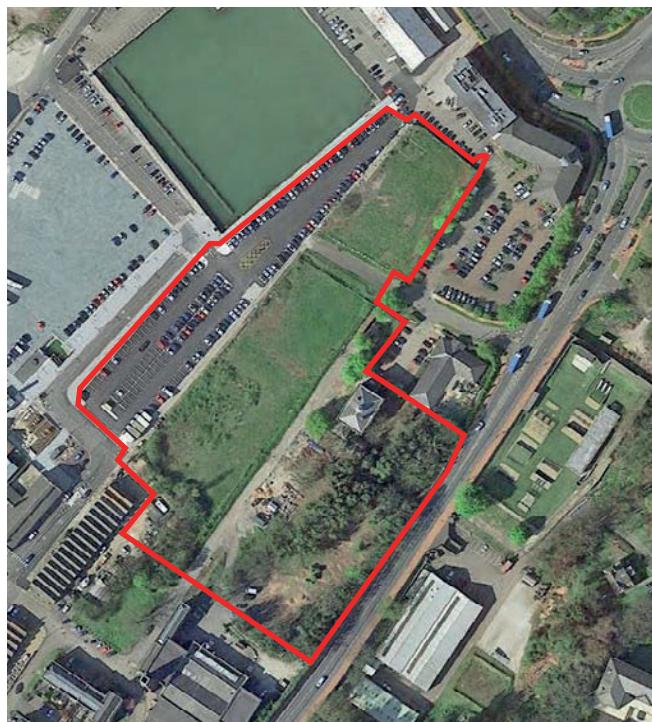
Indicative Developable Areas

Factoring in all known required setbacks, restrictions and easements, the indicative residual developable areas are shown below for illustrative purposes only.



Brunel Site Indicative Developable Areas

Factoring in all known required setbacks, restrictions and easements, the indicative residual developable areas are shown below for illustrative purposes only.



10 Other Considerations

Other matters for consideration include:

10.1 Consultation

Applicants are strongly advised to consult early and regularly with key stakeholders. Including:

- Pre-application discussions with Planning and Highways Authority through formal pre-application meetings.
- Discussions with technical Council officers including flood risk, conservation, design, environmental health, housing, and other officers.
- Statutory consultees – Historic England and the Environment Agency should be consulted prior to application submission.
- Early and regular discussions with Chatham Historic Dockyard Trust to discuss emerging proposals.
- Consultation with other neighbouring stakeholders – Chatham Maritime Trust, Busy Bees Nursery, Travelodge, The Observatory office premises.
- The Universities at Medway.
- Local Residents Associations.

10.2 Validation Requirements

Applicants should refer to guidance relating to both National requirements for submitting planning applications and also Medway Council's local requirements. A full planning application and conservation area consent application will be required due to the national significance of heritage assets on site and the sites' location within a Conservation Area. Scheduled Monument Consent will be required for works to or affecting Police House and the Brunel Sawmill and the route of the Canal.

The Development Brief has set out a number of technical assessment requirements, applicants should however clarify the extent of documentation and plans required through pre-application discussions to ensure applications are comprehensively prepared and validation is assured.

10.3 Developer Contributions

A Section 106 agreement will be required to provide necessary on-site and off-site infrastructure requirements. Discussions and negotiations regarding S106 provision should be undertaken at pre-application stage.

Applicants should refer to the Medway Council Developer Contributions SPD (2014).

